

Wandsworth Borough Council

# Urban design study - Executive Summary

Characterisation, development capacity and design guidance

LBW/274165-00/001/v1.3  
23/12/21





# Executive summary

This Urban Design Study has been commissioned by the London Borough of Wandsworth to provide a townscape character assessment, alongside other necessary evidence, to enable the Council to deliver a design led approach to meeting its housing targets through the emerging Local Plan. The study brings together the values, character and sensitivity of different parts of the borough with the reality of future development pressures.

The quality of much of Wandsworth's townscape together with the close proximity to central London, abundance of green space, range of popular schools and a number of thriving high streets makes it one of the most sought-after places to live and work in London, and at the same time puts huge pressure on housing availability.

Wandsworth is one of the largest inner-London boroughs, stretching from Vauxhall to the edge of Richmond Park, with one of the longest riparian frontages to the River Thames. The borough's five town centres: Balham, Clapham Junction, Putney, Tooting and Wandsworth Town, and nine local centres, give focal points, vibrancy and identity to the diverse range of communities that make up the borough.

Residential areas are characterised by leafy streets of Victorian terraced housing punctuated by landmarks such as schools, churches and historic pubs. There is also a legacy of post-war public housing estates, which in their time were revolutionary social housing achievements.

Open spaces are a particularly distinctive feature, in particular the four Commons (Clapham, Tooting, Putney Heath/Wimbledon and Wandsworth); whilst the high quality townscape is reflected in around half the borough designated as conservation areas.

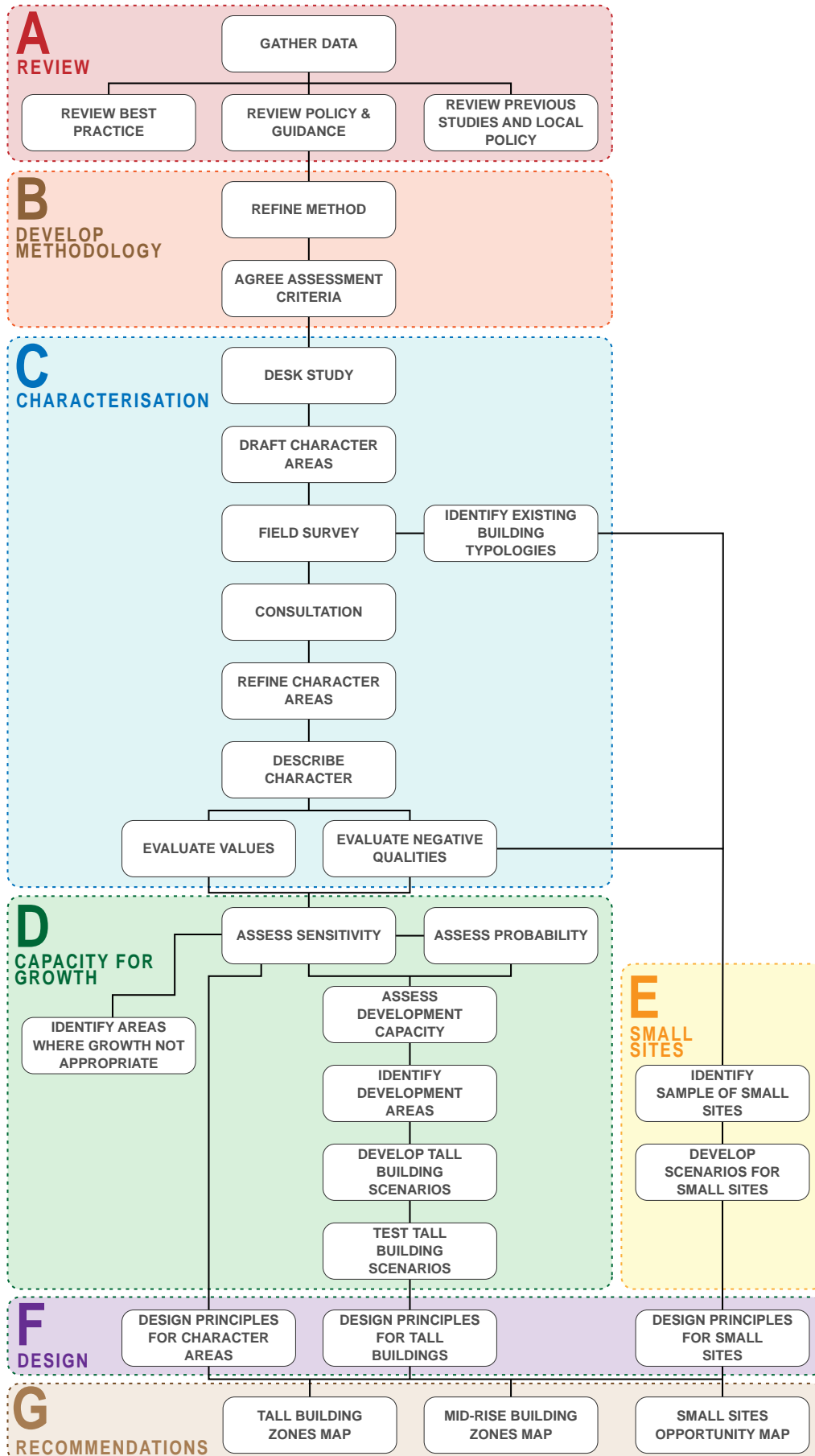
In recent years, high-rise mixed use developments have become increasingly a feature of the Thames riverside in areas previously occupied by heavy industry such as in the Nine Elms Opportunity Area around Battersea Power Station.

Wandsworth is an ambitious and proactive borough that desires to drive positive change with a focus on appropriate, well-planned delivery. This commitment to change includes maximising the supply of housing for the Borough. This is only achievable through ensuring the Local Plan policies and site allocations remain up to date, fit for purpose and are supported by the best possible evidence in order to be effective and robust.

Nationally there is a focus on the central role of design in planning policy, reflected in revisions of the [National Planning Policy Framework](#) (NPPF) and the publication of the [National Design Guide](#). Likewise the emerging London Plan focuses on a design-led approach, with greater consideration of character, design, accessibility and existing as well as proposed infrastructure. Within this context, many areas of Wandsworth are undergoing change, and it is important to plan for good growth in a way that optimises capacity, delivers high quality streetscapes, open spaces and where development contributes to the quality of life of communities.



Wandsworth has historically developed around its river frontages, with Battersea Power Station on the River Thames now the centre of one of the largest regeneration projects in Europe



Methodology overview

# Methodology

## *Review*

This stage establishes a comprehensive understanding of the context and policy background, in national planning policy and design guidance, and in Wandsworth's current policies and evidence base studies.

## *Develop methodology*

The characterisation process is informed by industry guidance set out by the Landscape Institute, Natural England and the Greater London Authority (GLA). The methodology is refined to suit the particular characteristics and constraints of Wandsworth.

## *Characterisation*

The baseline characteristics of the borough as a whole are reviewed, including consideration of physical, cultural, perceptual and social qualities.

The borough is then divided into character areas, each of which are described and evaluated to draw out valued features and negative aspects for enhancement. The key characteristics and qualities of character areas are verified on site, in discussion with stakeholders and through community engagement.

## *Capacity for growth*

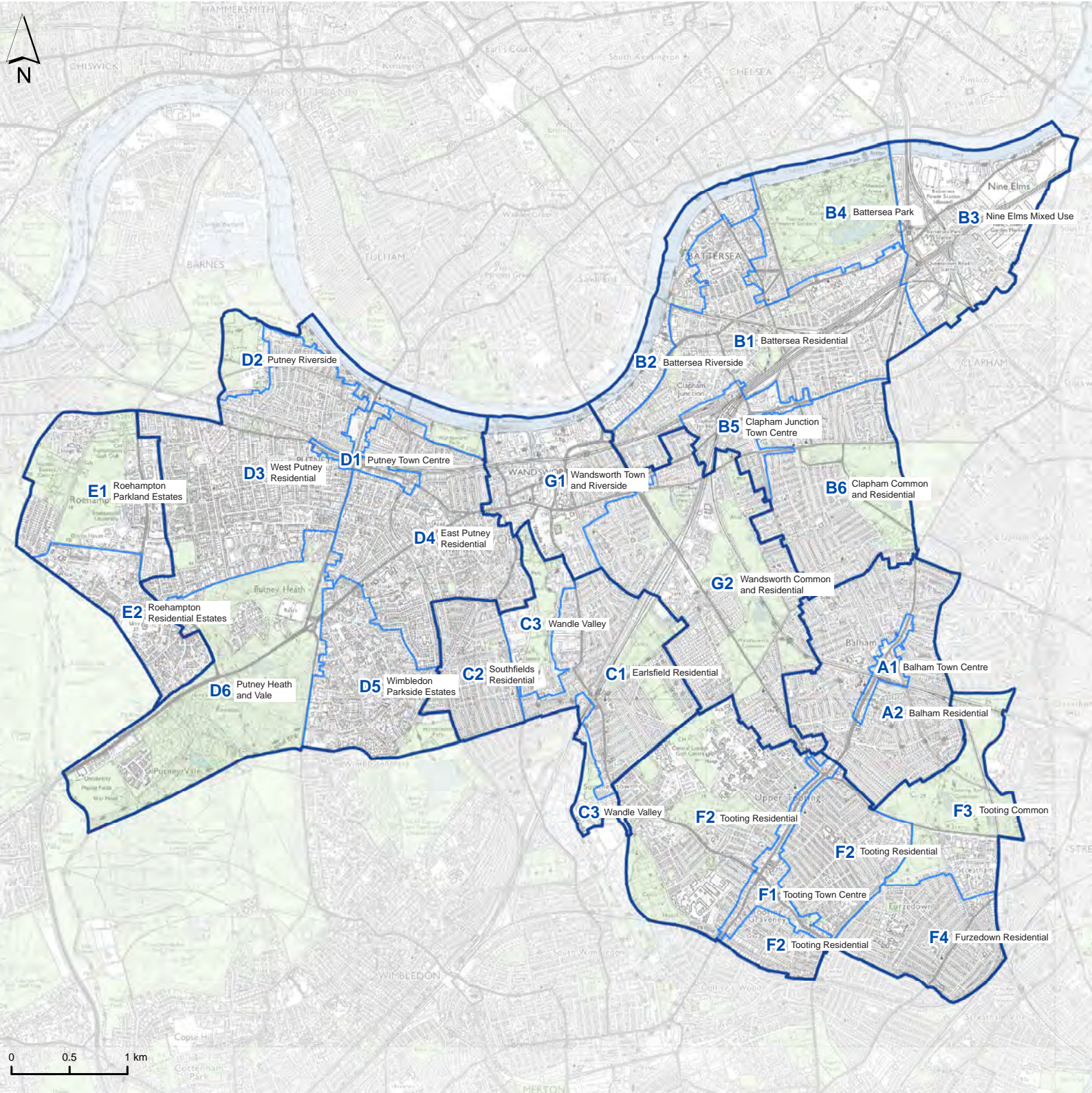
The capacity for growth is determined by assessing the sensitivity of the character areas to establish high sensitivity areas unlikely to have capacity for development without adverse effects on the townscape; alongside areas of medium and low sensitivity with the potential for targeted or larger scale growth. Simultaneously, the 'probability' of change is assessed, analysing the borough in terms of aspects such as public transport accessibility, land availability and planning policies. Sensitivity and probability are considered together to understand the potential development capacity of the borough.

The development capacity map is used to establish areas which may be able to accommodate tall and mid-rise buildings, tested against hypothetical scenarios.

## *Small sites, design and recommendations*

The outcome of this process is design principles and recommendations indicating where in the borough tall and mid-rise building development is likely to be appropriate, alongside borough-wide mapping for small site opportunities.





**Overview of character areas**

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### *Characterisation study*

The Borough-wide townscape character assessment divides the borough into seven 'places' and 25 character areas. Each 'place' and its constituent character areas are presented in Section 3 as a series of character area profiles. The character area profiles describe and evaluate the borough. The characterisation can be used as a standalone evidence base for future planning and policy making, as well as the latter parts of the urban design study.

The information contained within the character profiles includes:

- a description of the key characteristics of the character area;
- explanation of the area's valued features and negative qualities;
- an overview of the area's sensitivity (specifically in relation to tall buildings), highlighting 'higher sensitivity' areas where extensive change is unlikely to be appropriate and where the design of any new development should be especially sensitive or 'lower' sensitivity areas whose character may be enhanced through positive change;
- an overall strategy for the area, in relation to whether, as a whole, the character of the area should be conserved, restored, improved or transformed; and
- design guidance for new development or planning and management, taking into account the area's character, valued and negative qualities and overall strategy.



## The seven Places of Wandsworth

Wandsworth has been divided into seven 'Places' which reflect a sense of place and align with areas recognised by local people. These are further divided into 25 character areas which provide a finer grain of detail to draw out the distinctive qualities important to character.

### *Balham*

Balham developed as a small town centre following the opening of the railway in 1856, becoming known as the 'gateway to the south'. The area is characterised by 2-3 storey Victorian and Edwardian buildings in the main, although extensive damage in WWII means there is frequent evidence of post-war infill development.



Period 2-3 storey terraces are a key feature across Balham

### *Battersea*

Before the industrial revolution much of the Battersea area was farmland known as 'Battersea Fields'. The flat, fertile soils of the Thames floodplain were cultivated for market gardening. The area stretches along the River Thames, with the 83ha listed Battersea Park at its



Battersea's mixed riverside frontage

centre. The area is now also home to one of the largest regeneration projects in the country - focused around the grade II\* listed Battersea Power Station. The character, which includes the town centre of Clapham Junction and the area around Clapham Common, is typified by a Victorian and Edwardian townscape with a large number of important listed buildings.

### *Earlsfield and Southfields*

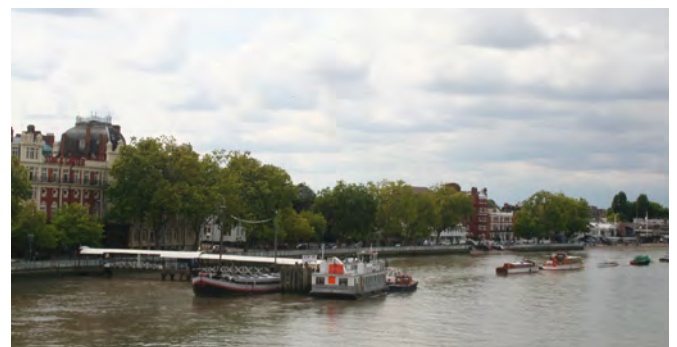
Earlsfield came into existence as a place name around 1870 following construction of the railway and later station, overtaking the former hamlet of Garratt further south. Southfields, originally part of Earl Spencer's Estate sold off in the 1870s developed with uniform 2 storey terraces with some commercial / industrial premises since converted into residential flats.



Tree lined streets with post-war development abutting period terraces

### *Putney*

Putney is characterised by its river-front location, including Putney Bridge and the Putney Embankment Conservation Area, reflecting its historic importance as a river crossing. It is also characterised by its residential tree-lined streets, many of which are recognised



Putney riverside



for their special character through designation as conservation areas. The land rises gradually away from the Thames up Putney High Street towards Putney Heath. Putney is one of the oldest settlements in Wandsworth, with the bridge one of the earliest crossing points over the Thames.

### ***Roehampton***

Roehampton is a mixed area, with some remnant large Georgian and Victorian mansions, 20<sup>th</sup> century housing estates, many of which were pioneering at the time, and the university campus. The 17<sup>th</sup> century village around Roehampton High Street is within Putney 'place' due to its relationship with Putney Heath.

The character of the area changed significantly in the mid to late 1950s with the introduction of the Alton



The listed concrete estates of the post war era

Estate, said to be the finest examples in London of the post WWII idealism of buildings within a cherished landscape setting.

### ***Tooting***

Tooting has pre-Saxon origins and was a focus for trade with its position on the Roman Road Stane



Many of Tooting's homes are quite large and have a suburban feel

Street. Furzedown, to the south of Tooting Common was developed by local builders in 1900 within the extensive grounds of Furzedown House with a grid pattern of streets. The 89 hectare Tooting Bec and Tooting Graveney Commons represent the remaining open space that once stretched to Mitcham.

Today, the demographics of Tooting are shifting towards young professionals with a vibrant town centre.

### ***Wandsworth Town and Common***

The Manor of Wandsworth appears in the Domesday book. The town's original centre was at All Saints Church on the High Street, and industry at the confluence of the River Thames and Wandle. Houses and industry developed together in a tight knit format. Wandsworth Common has been recorded as such since



Wandsworth Common

1741, although eroded over time by development and the construction of the railways.

Significant redevelopment of Wandsworth occurred in the 1970s and 80s, including the Arndale shopping centre and Swandon Way.

## Capacity for growth

An assessment was undertaken of the borough's capacity for tall buildings and small site development, using the characterisation study as an evidence base. The assessment considers sensitivity and 'probability' of change together, as set out in the methodology.

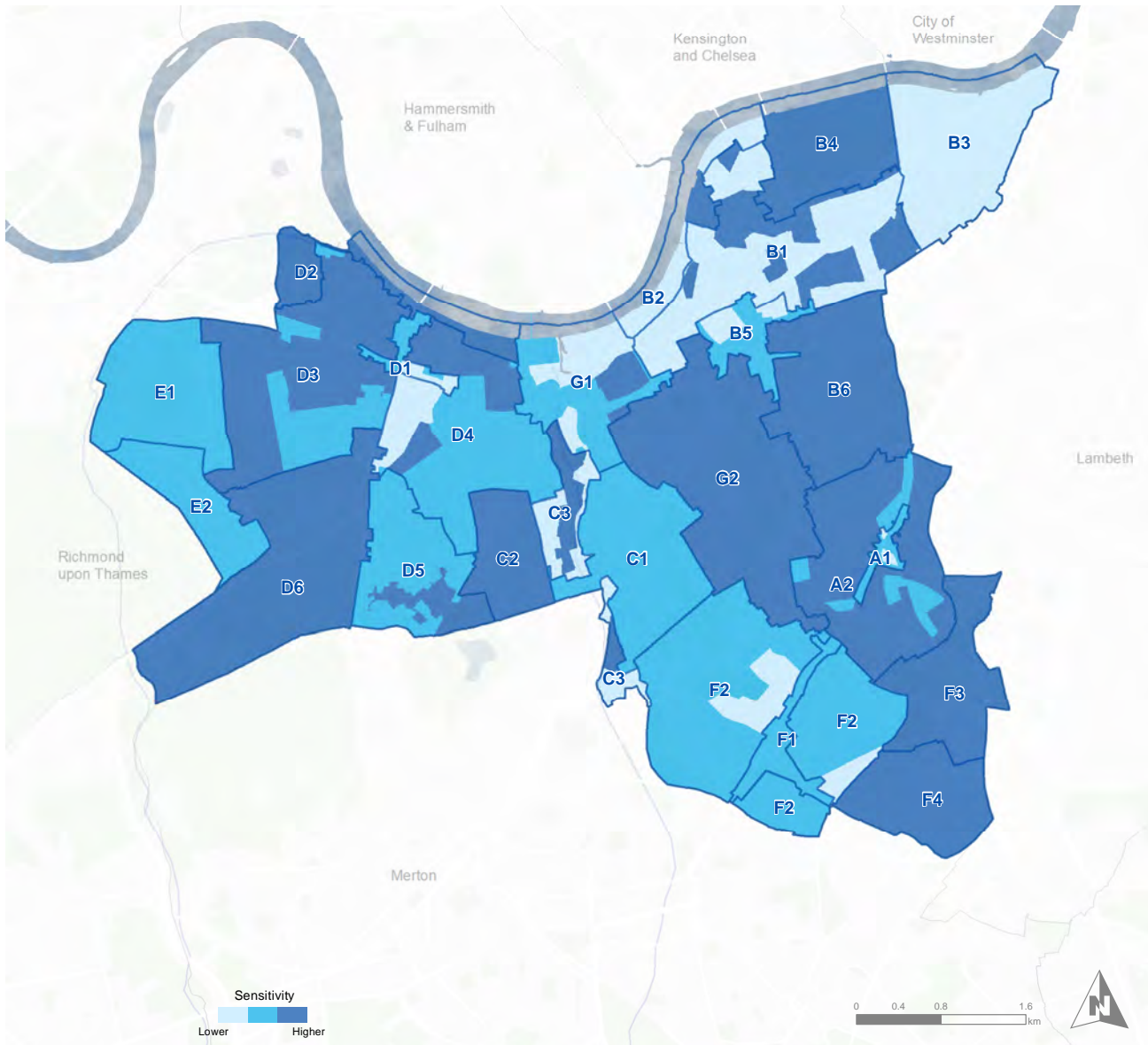
### Sensitivity

Some of the most sensitive parts of the borough include its large open spaces, such as Wimbledon, Wandsworth, Clapham and Tooting Commons; and Wimbledon, Wandsworth, King George's and Battersea Parks.

Other notably high sensitivity areas include conservation areas across much of West Putney Residential and Putney Riverside with extensive intact

period properties and strong cultural associations; Southfields Residential and Furzedown Residential with their highly consistent period terraces; period residences around Wandsworth Common; and residential streets around much of the Battersea and Balham Residential character areas.

Areas with a lower sensitivity include estates within East Putney and Battersea; parts of Upper Richmond Road within Putney Town Centre; stretches of Wandsworth Riverside and the Wandle Valley; areas around St George's Hospital near Tooting; modern estates around Church Lane in Tooting; the supermarkets and car parking within Balham Town Centre; and the Nine Elms Opportunity Area.



Sensitivity plan

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## Probability of change

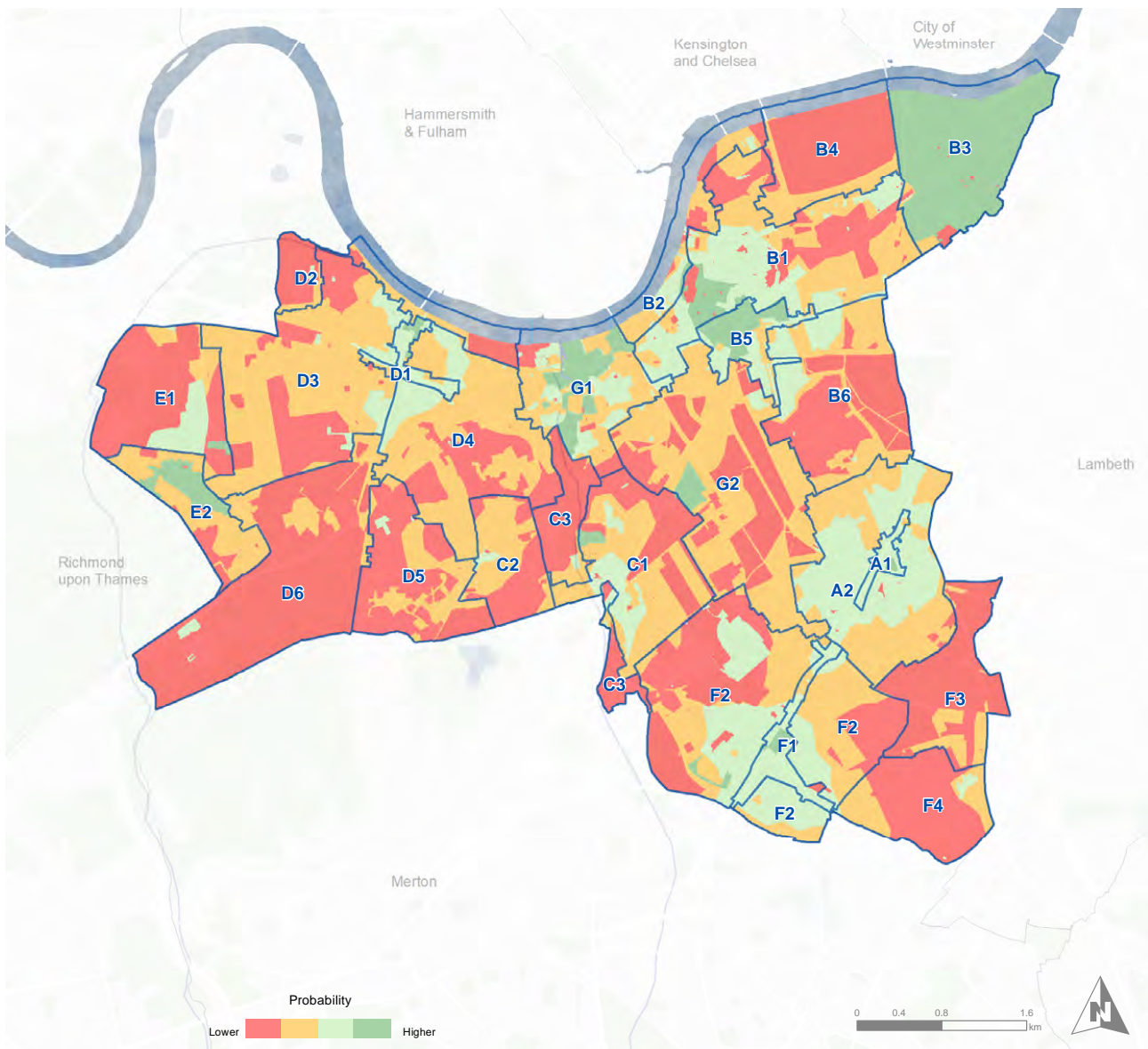
'Probability' of change (also known as 'likelihood' of change) is an assessment of how likely it is for different areas to come forward for development. Factors which give rise to a **higher probability of change** include areas which are already designated for development, including areas with likely forthcoming masterplans and major planning applications in Balham, Clapham Junction, Nine Elms, Putney, Roehampton, Tooting and Wandsworth Town. Areas with high levels of accessibility (i.e those with a high public transport accessibility level, or PTAL) also have a higher probability of change, which generally covers much of the northern and south eastern extents of the borough.

Factors which give rise to a **lower probability of change** include areas which are designated in their

existing use (such as Strategic Industrial Locations), areas of open space and areas with a low PTAL score.

Again, this is not to suggest that no change will occur in these areas; however, as a whole the likelihood of change is lower.

On a site-by-site basis, there will be a number of factors that influence probability of change, including: existing use; quality, fitness-for-purpose and vacancy; site size and configuration; and ownership.



Probability of change plan

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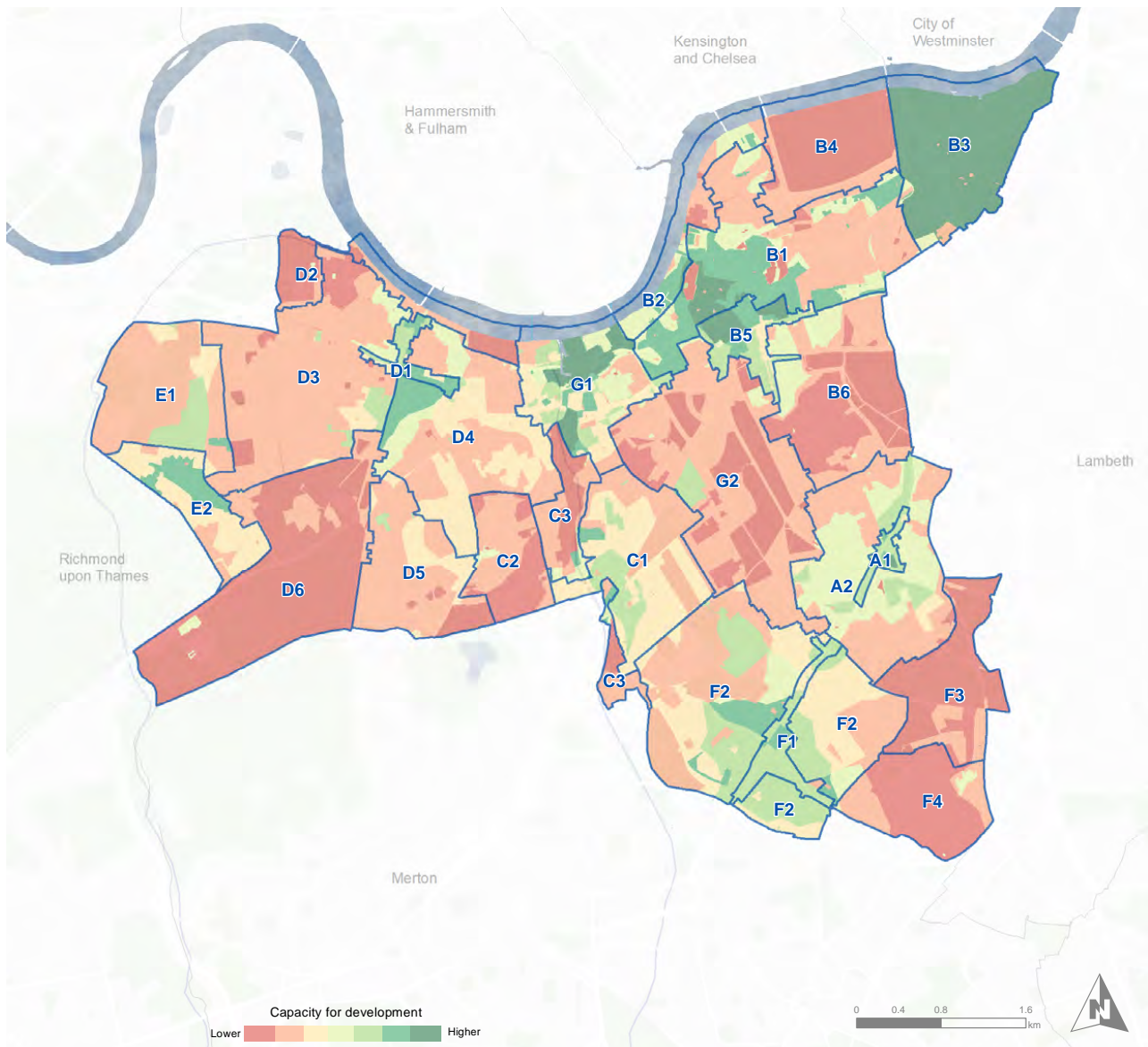
## Development capacity

The development capacity of different parts of the borough for tall buildings is assessed by combining sensitivity and probability of change together.

Areas with the least likely development capacity (shown in red on the plan below) include Wimbledon Common and Putney Heath; the westernmost parts of West Putney Residential; Barnes Common; Wimbledon, Wandsworth and King George's parks; Wandsworth, Clapham and Tooting Commons; and Battersea Park.

Orange areas have either a lower sensitivity but still a low probability of change (e.g. development areas with a low PTAL score), or a high sensitivity but only a medium probability of change.

Green and pale yellow areas generally have a high probability of change or a lower sensitivity to change. Dark green areas are the most likely to have the greatest development capacity, with a high probability of change compared against a low sensitivity, including areas such as some estates in East Putney Residential, Battersea Residential and Tooting Residential; Wandsworth Town Centre; some industrial plots within the Wandle Valley; parts of Clapham Junction Town Centre and Balham Town Centre; and Nine Elms Opportunity Area.



Development capacity plan

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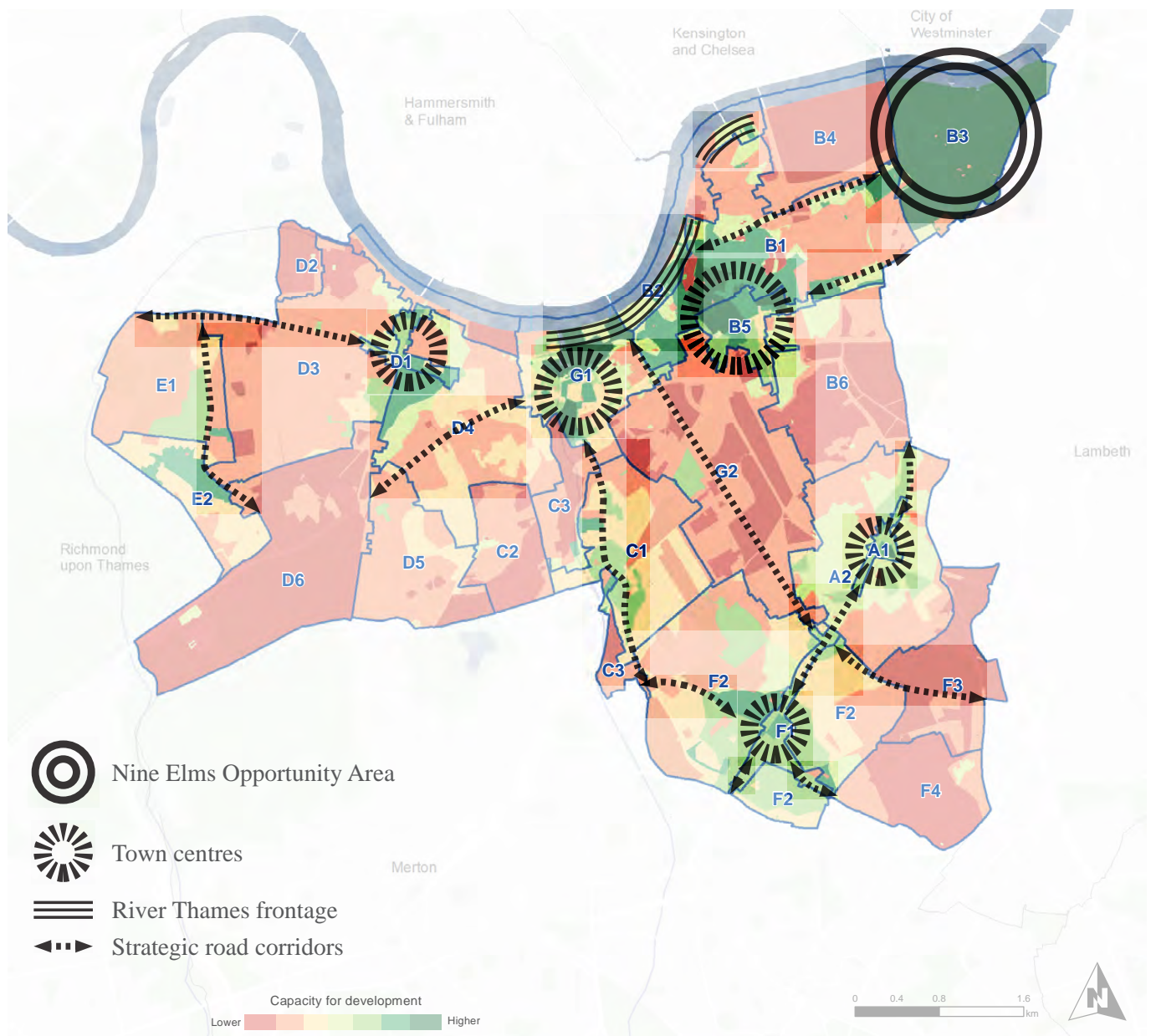
## Overall development strategy

The development capacity, existing tall buildings and consented tall buildings mapping helps to inform an overall strategy for development of increased height across the borough.

The plan below identifies the broad areas considered in more detail in the following sections on tall and mid-rise buildings. Much of LB Wandsworth is characterised by highly sensitive areas with a consistently low height residential scale. Therefore these are scoped out of further consideration, with the strategy for tall buildings focusing on the Nine Elms Opportunity Area, the five town centres (Balham, Clapham Junction, Putney, Tooting and Wandsworth),

the River Thames frontage through Wandsworth and Battersea and known areas with emerging masterplans or major planning applications.

The study further reviews the potential for increases in height through mid-rise buildings along strategic road corridors including some existing areas with a less consistent built pattern or higher existing building heights which typically extend from these routes.



Strategy for mid-rise and tall building development across the borough  
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## Tall building capacity

Wandsworth has a hugely varied character, from the major tall developments of the Nine Elms Opportunity Area in the east through to the more suburban character of Roehampton and Putney Heath in the west. This variety, and associated differences in sensitivity, as well as the variation in 'probability' of change, means the capacity for tall buildings varies across the borough. The differences in character and sensitivity also mean that the height of a building for it to be considered "tall" varies. For the purposes of this study we have defined a tall building as:

**Buildings which are 7 storeys or over, or 21m or more from the ground level to the top of the building, whichever is lower.**

Using this definition, the opportunity map establishes, for each character area (and where relevant sub-areas), the prevailing existing building height and the specific tall building height. The accompanying criteria for each character area/sub-area which must be considered throughout the development of any tall building proposals.

Overall, Wandsworth has capacity for tall buildings in a number of strategic and more local locations. Opportunities for tall buildings are generally concentrated within three different types of area:

1. Along the **River Thames frontage** including within the **Nine Elms Opportunity Area**: Here, until recently, commercial and industrial uses have dominated. There are opportunities for tall buildings to respond to the large scale and width of the riverside. However, the impact of riverside development goes well beyond the borough boundaries and therefore must continue to be carefully planned to protect the character of both banks and the overall historic and cultural importance of the River Thames as a globally recognised characteristic of London. Nine Elms is located within London's Central Activities Zone and is the site of significant tall building development in recent years. In particular any proposals need to assess and avoid impacts on the Palace of Westminster and Westminster Abbey including St Margaret's Church World Heritage Site in the nearby City of Westminster on the northern bank of the Thames. Tall building applications in the Wandsworth riverside area would need to have particular regard to historically sensitive sites on the north bank such

as Fulham Palace Scheduled Monument. A policy on character and design in relation to the River Thames and the Thames Path (or a sub-policy contained within the general policy) would be beneficial considering its importance shown through some of the character area assessments.

2. Within **town centres**: There are opportunities for tall buildings within the town centres of Putney (including along Upper Richmond Road), Wandsworth, Clapham Junction and, to a lesser extent, Balham.
3. Within or adjacent to **existing estates and emerging major regeneration masterplans**: This includes opportunities within Roehampton, Battersea, the southern end of the River Wandle (near the Wimbledon AFC development in neighbouring LB Merton) and at St George's Hospital in Tooting.

In all cases, the acceptability of individual plots will depend on specific characteristics of the site. Tall building guidance provided is intended to help steer selection of appropriate sites and development of suitable building proposals, including in the context of consideration of the cumulative impact of tall buildings.

An area being designated as a tall building zone does not mean it has capacity to receive tall buildings within the appropriate range across the whole extent. Every new development will need to consider the specific context of the plot, existing buildings surrounding the plot and any other development proposals in the area including those going through planning, consented schemes and buildings under construction.

## Mid-rise buildings capacity

Separate from the London Plan definition of tall, the borough of Wandsworth recognises that there may be mid-rise buildings in the borough which, whilst not defined as 'tall' in line with the London Plan, are of an increased height to their surroundings. Such buildings have the potential to result in significant impacts to the roofscape, skyline, or townscape character.

Therefore, such buildings have been described as 'mid-rise' buildings within this study. A mid-rise building is defined as:

**"Buildings of 5-6 storeys or 15-18m from the ground level to the top of the building, whichever is lower."**

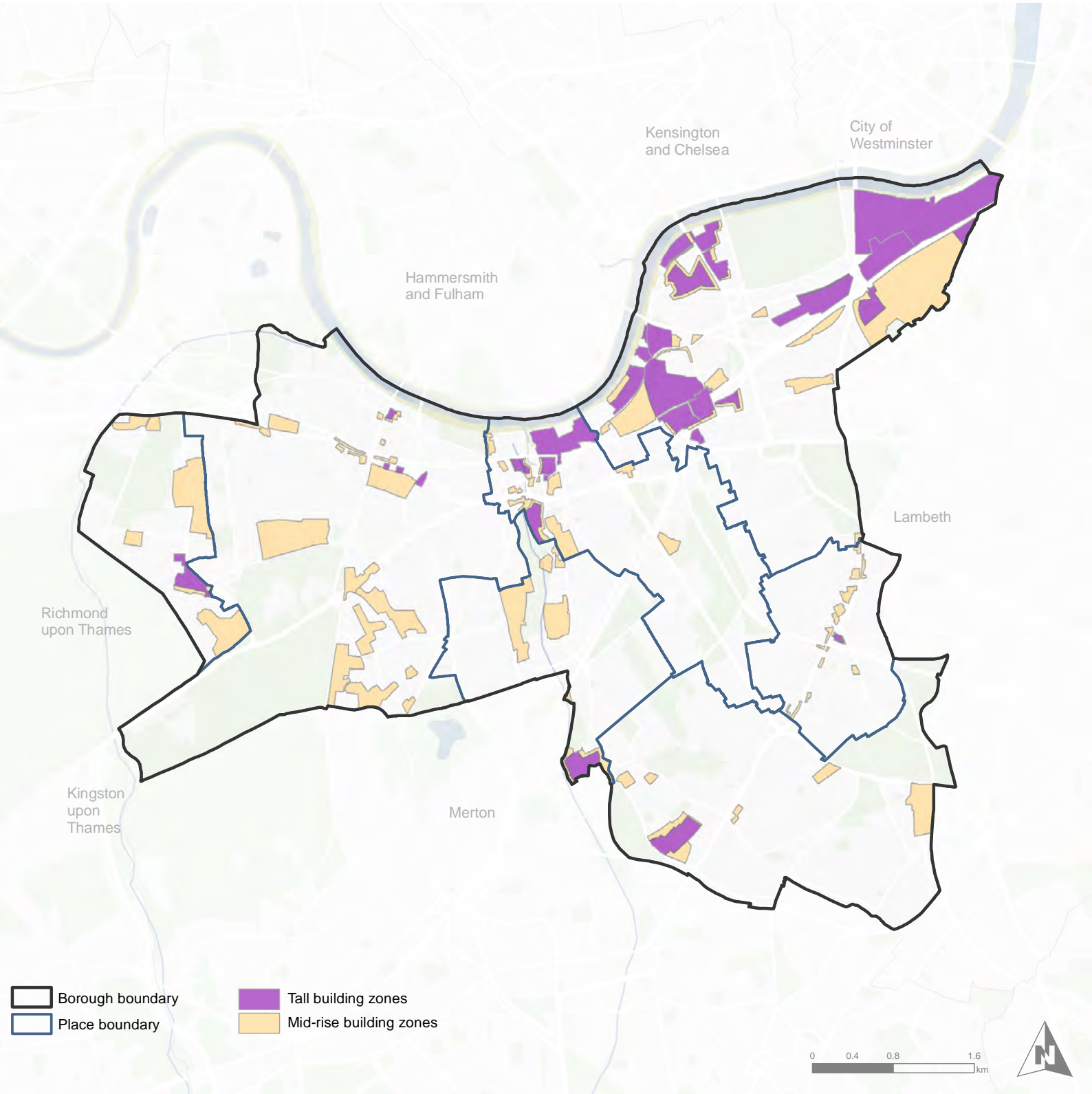
This definition is based on the prevailing building heights in the borough, determined through the site survey. Many areas in Wandsworth borough contain 2-3 storey buildings. In such cases, a new development of 5-7 storeys would be of an increased height to its surroundings, although would not be classified as a 'tall building'. The borough therefore classifies such buildings as 'mid-rise'. Mid-rise buildings zones are not required for the London Plan. However, this study has defined these in the same way as the tall building zones, in order to direct appropriate growth in the right locations in the borough.

Opportunities for mid-rise buildings are generally concentrated within five different types of area:

1. As **transition areas** to tall building zones: The majority of tall building zones have mid-rise building transition zones around them to provide appropriate stepping and integration to surrounding small scale built form.
2. Along **strategic road corridors**: The width of some roads provides the capacity for increased height, particularly at key junctions. This includes Upper Richmond Road in Putney, Balham High Road, Tooting High Street and West Hill.
3. Within **town centres**: There are opportunities for mid-rise buildings within the town centres of Putney, Clapham Junction and Balham (beyond the tall building zones), and within Tooting.
4. Within or adjacent to **existing estates**: This includes opportunities within Roehampton, Wimbledon Parkside, West Putney, East Putney, Tooting, Battersea and Nine Elms (south of the railway line).
5. Alongside **large scale open spaces**: Where

mid-rise buildings would be seen in proportion to the scale of the open space, and visibility filtered by mature boundary trees. This includes locations like Roehampton Golf Club, Putney Heath, Wimbledon Park, King George's Park and Wandsworth Common.

In all cases, the acceptability of individual plots will depend on specific characteristics of the site. The tall building guidance also applies to mid-rise buildings within the borough.



Tall and mid-rise building zones borough overview map  
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## Small sites

The study has also used the information gathered to provide a high level overview of opportunity for small sites across the borough.

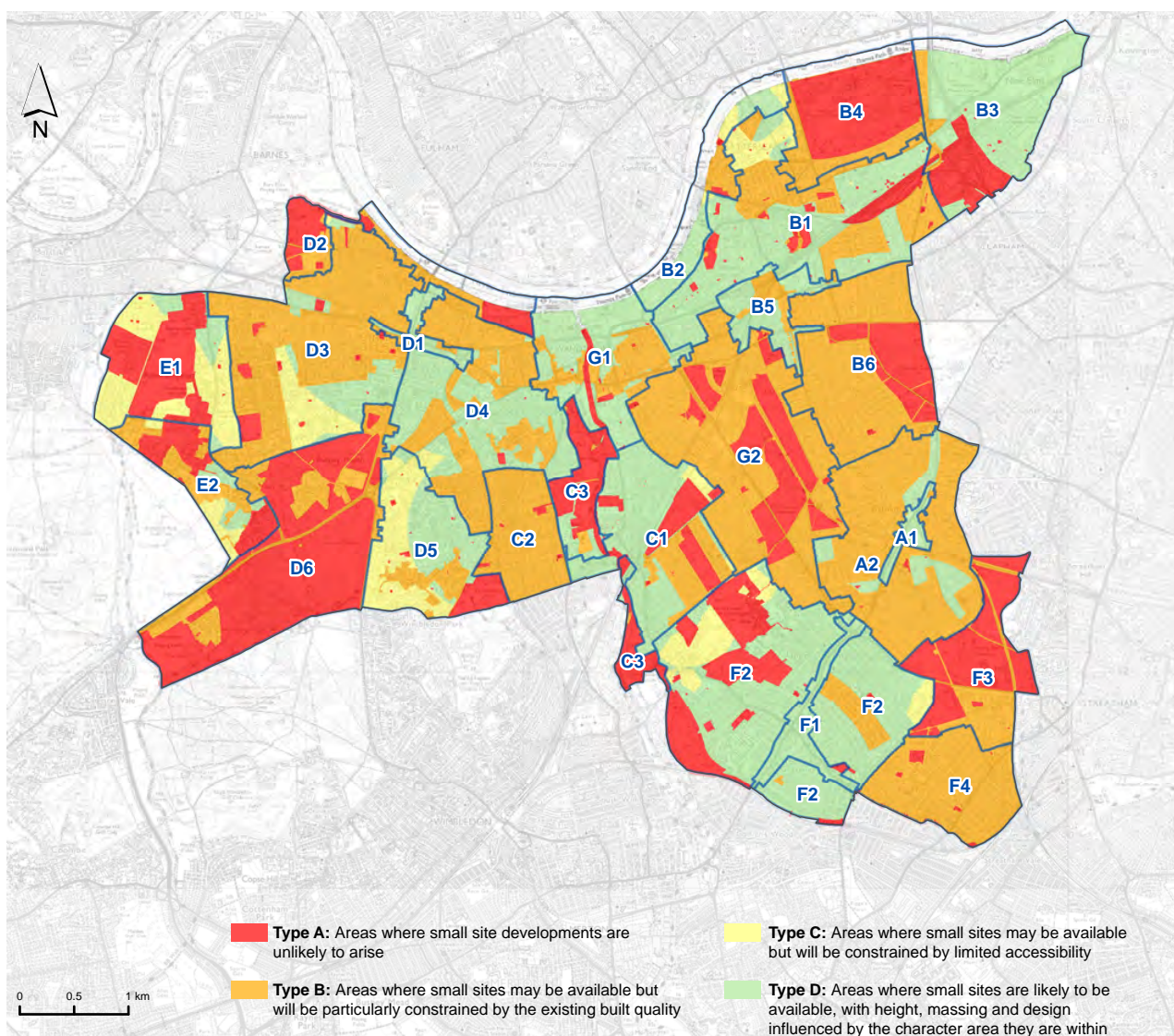
Proposals for small sites should accord with the general, criteria-based policy on design and character suggested in relation to tall building development. There would be value in a separate policy on small sites to cover the design guidance provided in the Urban Design Study. This should also reference the need to optimise the small site density (balanced against character, design, policy and other material considerations), to make best use of available sites.

With the exception of public open spaces, there is potential for some form of small site development across different parts of the borough. In some areas, the

realisation of a small site will be heavily constrained by the character of the area, and any development should refer to the character profiles.

A similar principle applies in areas of the borough with a low PTAL score, where, without further investment in public transport, increased density of housing is unlikely to be suitable. Therefore, small sites development may not contribute to increased density in these areas, but again may still help fulfil housing numbers in vacant or very low density plots.

The remainder of the borough has potential for small sites development, with the density and housing numbers that may be achievable influenced by the character of each area.



Small sites opportunity map

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Wandsworth Borough Council

# Urban design study

Characterisation, development capacity and design guidance

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23/12/21



## Revision history

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1.1	23/06/2021	Amendments following consultation	Various	Kate Anderson	Mark Job
1.2	09/07/2021	Amendments following client comments	Various	Kate Anderson	Mark Job
1.3	23/12/2021	Amendments following client comments	Various	Kate Anderson	Mark Job

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# Section 1

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## Introduction

# Section 1

## Introduction

### 1.1 Background

This Urban Design Study has been commissioned by the London Borough of Wandsworth to provide a townscape character assessment, alongside other necessary evidence, to enable the Council to deliver a design led approach to meeting its housing targets through the emerging local plan.

Wandsworth is one of the largest inner-London boroughs, stretching from Vauxhall to the edge of Richmond Park, with one of the longest riparian frontages to the River Thames. Home to over 320,000 people it includes a diverse range of communities and many distinct districts. A third of the borough is occupied by residential properties, many within one of the 46 conservation areas. A quarter of the borough is open space, much of which is in the form of large heaths and commons. The five town centres (Balham, Clapham Junction, Putney, Tooting and Wandsworth), the Nine Elms Opportunity Area and nine local centres give focal points and identity to the communities that make up the borough. The quality of much of the townscape together with the close proximity to central London, abundance of green space, range of popular schools and a number of thriving high streets has shaped Wandsworth into one of the most sought-after places to live and work in London, placing huge pressure on housing availability.

Wandsworth is an ambitious and proactive borough that aims to drive positive change with a focus on appropriate, well-planned delivery. This commitment to change includes maximising the supply of housing. This is only achievable through ensuring the Local Plan policies and site allocations remain up to date, fit for purpose and are supported by the best possible evidence in order to be effective and robust.

Over the last decade there has been a fundamental shift in policy regarding density, design and characterisation. Nationally there is a focus on the central role of design which is reflected in revisions of the [National Planning Policy Framework](#) (NPPF) and the publication of the [National Design Guide](#) which outlines ten characteristics of well-designed places to help illustrate

how this can be achieved. This is reinforced by the emerging position of the [Planning White Paper](#) which places emphasis on placemaking and design as a process to delivering good sustainable growth. Regionally, the [London Plan](#) (2021) also focuses on a design-led approach, with greater consideration of character, design, accessibility and existing as well as proposed infrastructure.

Within this context, many areas of Wandsworth are undergoing change, and it is important to plan for good growth in a way that optimises capacity, delivers high quality streetscapes, open spaces and where development contributes to the quality of life of communities. This approach is described in more detail in the [Council's smart growth agenda](#).

This Urban Design Study brings together a deep understanding of the values, character and sensitivity of different parts of the borough with the reality of development pressures. It provides a robust evidence base to inform future planning and assess the potential for delivering more housing on large redevelopment sites and small individual plots. In this way, the study will help make the best use of the land available and provide creative solutions for how new schemes can enhance their surroundings and protect existing cherished features, resulting in sustainable development with the community at its heart.

The London Plan sets a target for Wandsworth of 19,500 additional homes between 2019/20 to 2028/29. This represents an annualised average target of 1,950 new homes per year, which is a 7.5% increase over the 2015 London Plan target of 1,812 and a 70% increase over the 2011 London Plan target of 1,145. The London Plan also requires 4,140 of these units (414 per year) to be delivered from small sites to maximise regeneration of empty or poorly developed plots and protect other valued areas such as open spaces and particularly Metropolitan Open Land.



## 1.2 Purpose of this document

This study brings together a deep understanding of the values, character and sensitivity of different parts of the borough with the reality of development pressures.

It provides a robust evidence base to inform future planning and assess the potential for delivering more housing on large and small sites. In this way, the study will help make the best use of the land available while enabling new schemes to enhance their surroundings and protect existing cherished features. The study has also been used to inform the Council's housing capacity for the Local Plan through informing density calculations.

The study provides an overview at a borough scale, creating the framework for further work and future site-specific design guides, briefs and codes.

The work complements other Local Plan evidence base documents as is intended for use by Council Officers as a material consideration in determining planning applications. The study is also likely to be of interest to:

- developers wishing to understand the distinctive characteristics of the borough, and they type and scale of development the Council is likely to consider appropriate; and
- residents hoping to gain an insight into areas identified for growth alongside areas where the Council will be creating further guidance.

## 1.3 Structure of the document

This document describes and analyses the existing character of Wandsworth to help inform how and where housing targets should be met. The document is structured as follows:

**Section 1:** introduces the report and provides an overview of the methodology, which is detailed in [Appendix C](#).

**Section 2:** provides an overview of the Borough-wide context with reference to physical, cultural, perceptual and social qualities. It also includes an overview of the

national, regional and local policy drivers, which are described in more depth in [Appendix D](#).

**Section 3:** describes each of the 25 character areas located within the seven "Places" of:

- Balham;
- Battersea;
- Earlsfield and Southfields;
- Putney;
- Roehampton;
- Tooting; and
- Wandsworth Town and Common.

Building types identified across the borough are summarised and illustrated in [Appendix E](#). Site survey pro-formas are included in [Appendix F](#). Consultation undertaken to inform the characterisation study is summarised in [Appendix G](#).

**Section 4:** draws out the analysis of this characterisation work to understand the capacity for growth in different parts of the borough. This section summarises the sensitivity analysis undertaken in Section 3 and compares this against probability of change to establish the development capacity of the borough. From this work, the capacity for tall buildings, mid-rise buildings and small sites development has been assessed to inform future development in the borough. The tall buildings map has been developed to be in compliance with London Plan Policy D9 and informs where development in excess of 7 storeys or 21m can occur. Further guidance and protection is provided by the mid-rise buildings opportunity map (5-6 storeys or 15-18m).

The justification for each of the tall building zones, including a series of scenarios developed to test the capacity of certain areas, is provided in [Appendix A](#), along with design guidance for tall buildings. Scenarios have also been tested for some small sites across the borough, to inform design guidance, presented in [Appendix B](#).

Where text is highlighted [as shown](#) this provides a link to the relevant section of the report or an external information source.

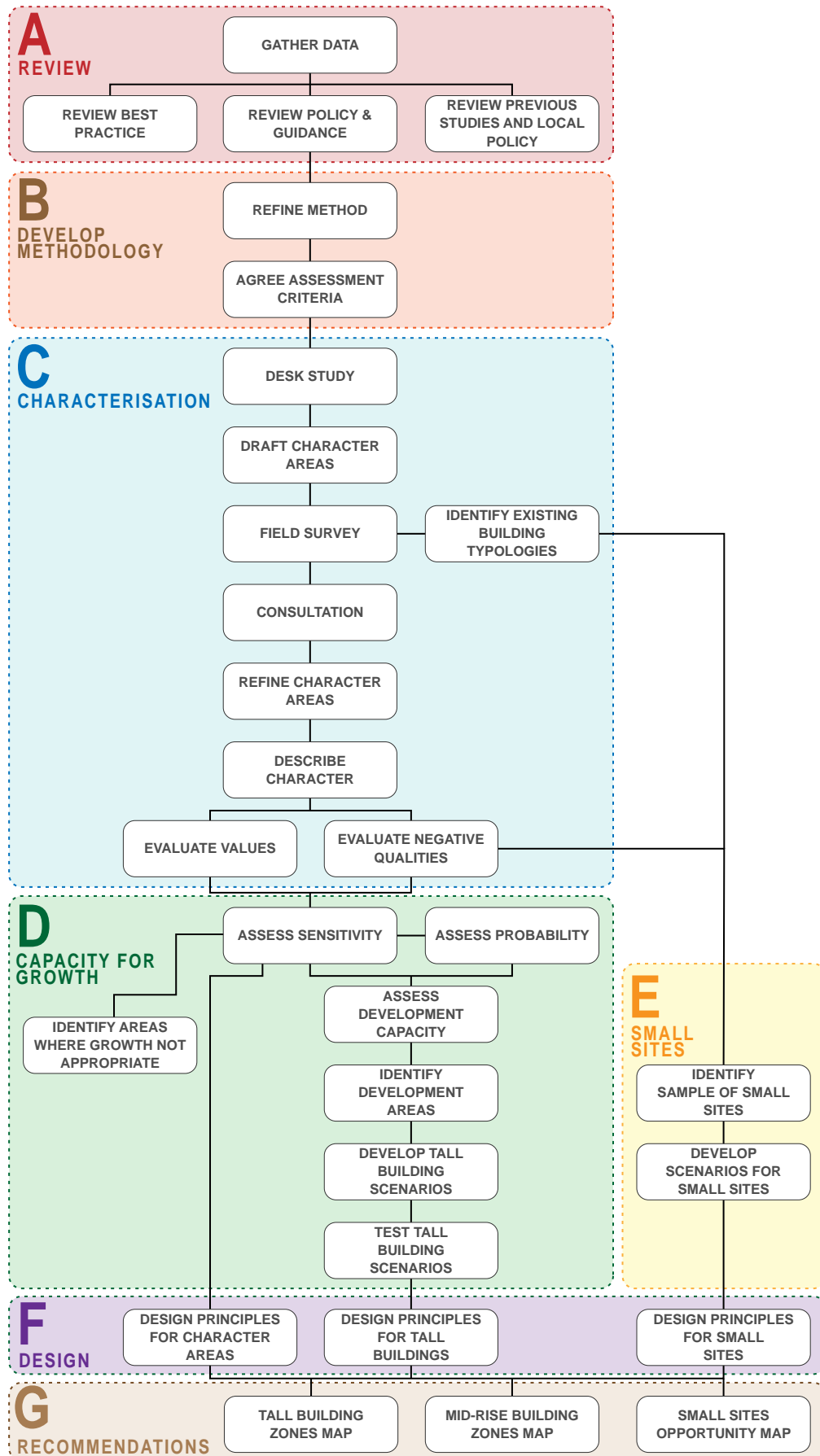


Fig. 1: Methodology overview

## 1.4 Methodology overview

The method for this study comprises stages A-G as shown in [Fig. 1](#). A detailed methodology is provided in [Appendix C](#).

### *Review*

The first stage of the methodology establishes a comprehensive understanding of the context and policy background, in national planning policy and design guidance, and in Wandsworth's current policies and evidence base studies.

### *Develop methodology*

The characterisation process is informed by industry guidance set out by the Landscape Institute, Natural England and the Greater London Authority (GLA).

### *Characterisation*

The borough is divided into character areas, each of which are described and evaluated to draw out valued features and negative aspects for enhancement. The key characteristics and qualities of character areas are verified on site, in discussion with stakeholders and through community engagement.

### *Capacity for growth*

The capacity for growth is determined by assessing the sensitivity of the character areas, by considering their value and susceptibility to change, using Landscape Institute guidance. This process establishes high sensitivity areas unlikely to have capacity for development without adverse effects on the townscape; alongside areas of medium and low sensitivity with the potential for targeted or larger scale growth, where development may provide a positive contribution. Simultaneously, the 'probability' of change is assessed, analysing the borough in terms of aspects such as public transport accessibility, land availability and planning policies. The sensitivity and probability are then overlaid to understand the potential development capacity of the borough.

The development capacity map is assessed alongside existing and consented developments to establish areas which may be able to accommodate tall and mid-rise buildings. These are further tested through hypothetical scenarios. The testing considers likely development capacity potential impacts and concludes with appropriate height ranges for specific areas.

### *Small sites, design and recommendations*

The outcome of this process is design principles and recommendations indicating where in the borough tall and mid-rise building development is likely to be appropriate, alongside borough-wide mapping for small site opportunities.



Candy Land  
Oil Licenses - Grocery - Household - Gift - Food

SUETTE  
COPPER  
KIDNEY  
HEALTH

WELLS  
LONDON  
1870



## Section 2

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### Borough Overview

# Section 2

## Borough overview

### 2.1 Introduction

This section provides an overview of the borough, beginning with the policy context and then describing its physical, cultural, perceptual and social qualities.

Wandsworth is one of the largest inner London boroughs. It is located in south-west London, stretching from Vauxhall to the edge of Richmond Park.

The River Thames is an important aspect of its character both historically and today. The growth of Wandsworth Town was fuelled by industry which developed around the River Wandle's confluence with the Thames. Eight bridges link the borough to the north side of the river. Some of these crossing points, such as Putney Bridge, were historic catalysts of growth for towns. Accessibility to London led to the development of country estates and villas in Roehampton in the Georgian era.

The five town centres of Balham, Clapham Junction, Putney, Tooting and Wandsworth Town, alongside nine local centres, give focal points, vibrancy and identity to the diverse range of local communities. A third of the borough is occupied by dwellings: residential areas are characterised by leafy streets of Victorian terraced housing punctuated by landmarks such as schools, churches and historic pubs. There is a legacy of post-war public housing estates, which in their time

were revolutionary social housing achievements, and many buildings (such as at Alton East) are now listed. In recent years, high-rise mixed use developments have become a key feature of the Thames riverside in areas previously occupied by heavy industry. This is exemplified by the extensive regeneration in Nine Elms around the iconic Battersea Power Station.

The open spaces are a particularly distinctive feature of Wandsworth, occupying a quarter of the borough's area. The four Commons (Clapham, Tooting, Putney Heath and Wandsworth) are particularly special features. There are also four major parks including Battersea Park and King George's Park, and numerous large cemeteries.

The industrial revolution and development of the railways are another key chapter in Wandsworth's history. Today, Clapham Junction is the busiest station in the country.

The high quality townscape is reflected in around 50% of the borough being designated as conservation areas. This, together with its proximity to central London and good public transport accessibility, makes Wandsworth an attractive place to live.



**Fig. 2:** Battersea Power Station has inspired artists and writers  
© Paul Carstairs



**Fig. 3:** Tooting Common, one of the many popular public open spaces for recreation, relaxing and gathering



Hammersmith & Fulham

Kensington and Chelsea

City of Westminster

Battersea Power Station

Battersea Park

Clapham Junction

Putney

Clapham Common

Wandsworth Common

Lambeth

Richmond upon Thames

Putney Heath

Balham

Tooting Commons

Merton

## 2.2 Policy summary

This section outlines the reviews undertaken of relevant national, London and borough-wide policies. Further details are provided in [Appendix D](#).

### 2.2.1 National policy

Chapter 12 of the [National Planning Policy Framework \(NPPF\)](#) (2019) covers design, reflecting that high quality buildings and places are key to what planning should seek to achieve. It states that plans should:

*‘...set out a clear design vision and expectations, so that applicants have as much curtailed as possible about what is likely to be acceptable. Design policies should be developed with local communities so they reflect local aspirations, and are grounded in an understanding and evaluation of each area’s defining characteristics.’* (Paragraph 125)

Particularly relevant to this study, the NPPF states that planning policies should ensure that developments are sympathetic to local character and history, including the surrounding built environment and landscape setting – but also not preventing or discouraging appropriate innovation and change (including increasing densities). Development should establish or maintain a strong sense of place, and optimise the potential of sites.

The NPPF also includes a number of policies around making effective use of land in meeting the need for homes and other uses – including building at appropriate densities and avoiding low density development where there is an existing/anticipated shortfall in land. The strategic direction established in the NPPF is supported by further details in Planning Practice Guidance and the [National Design Guide](#).

The study has also been informed by [Historic England Advice Note 4: Tall Buildings](#) which guides sustainable and successful tall building design and planning within historic environments. Note that this document is due to be updated in 2021/2022.

### 2.2.2 London-wide policy

The [London Plan](#) was adopted in March 2021 and is the new Spatial Development Strategy for Greater London. It sets out a framework for how London will develop over the next 20-25 years and the Mayor’s vision for Good Growth.

Chapter 3 of the London Plan includes policies on design, character and capacity for growth. A summary

of the policies and supporting text is provided in [Table 12](#) within [Appendix D.2](#).

There are a number of other relevant London-wide policies including (and covered in full in [Appendix D.2](#)):

- [Draft Good Quality Homes for all Londoners: Housing SPG](#);
- [Shaping Neighbourhoods: Character and Context SPG](#);
- [London View Management Framework SPG](#); and
- [Industrial Intensification and Co-location Through Plan-led and Masterplan Approaches Practice Note](#).

### 2.2.3 Borough-wide policy

Wandsworth’s current adopted local development plan consists of the following: Core Strategy (adopted March 2016); Development Management Policies Document (adopted March 2016); Site Specific Allocations Document (adopted March 2016); Policies Map (adopted March 2016 and subsequently superseded by a 2018 version); and the London Plan.

The local development plan includes a number of policies relevant to this study, including a policy on local distinctiveness and a criteria-based policy for tall building location and design (see [Appendix D](#)).

### 2.2.4 Other London boroughs

The study has also reviewed policy approaches in other London boroughs. These boroughs (Hammersmith & Fulham, Sutton, Richmond upon Thames and Tower Hamlets) have all adopted policies in the last three years and share similarities (in terms of geography, development pressure etc.) with Wandsworth. The review has considered policies relating to:

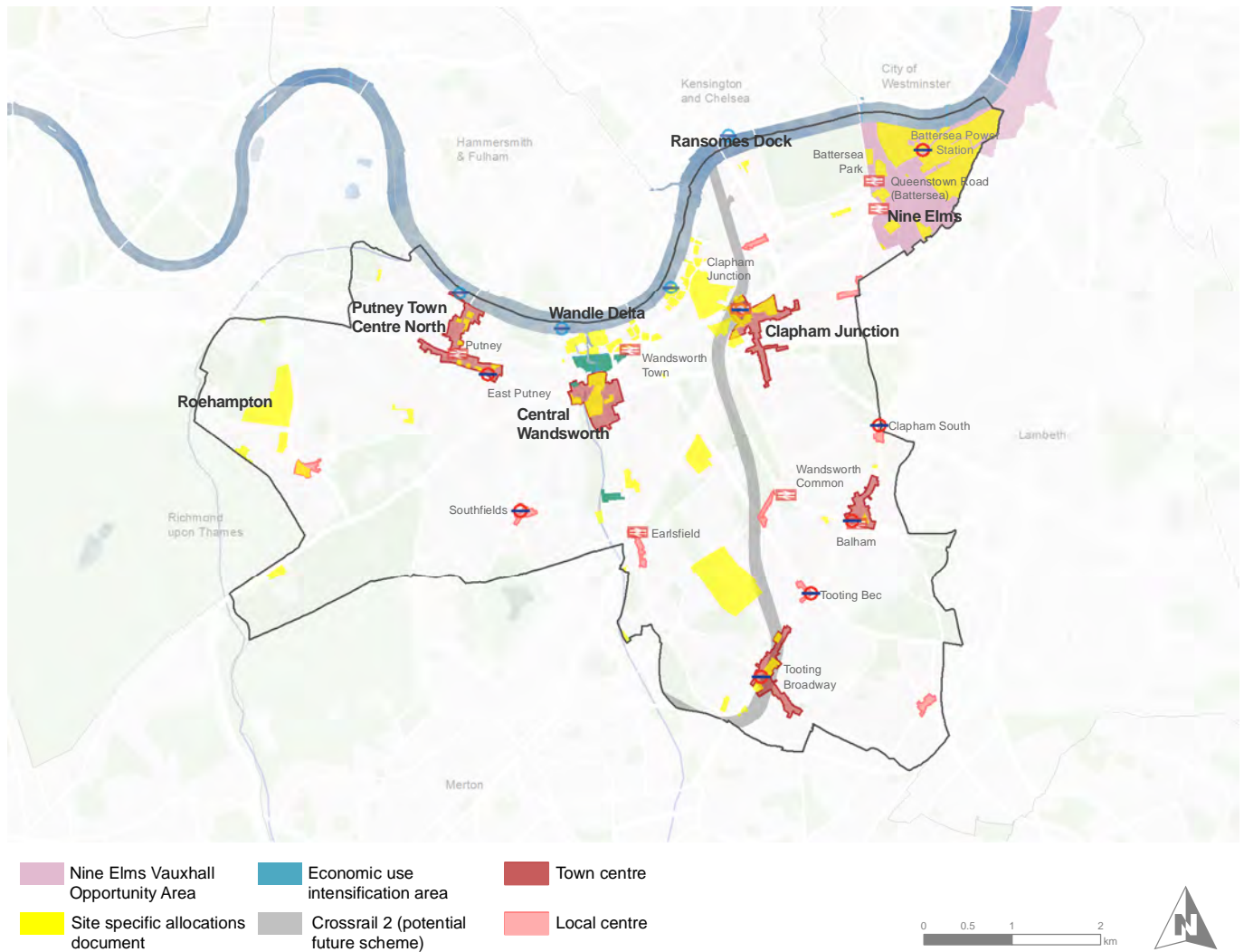
- approach to urban design and character;
- mapping of character areas;
- definition of a ‘tall building’;
- approach to tall buildings; and
- approach to urban design on small sites.

The full review and comparison with other boroughs is provided in [Appendix D](#). Whilst these are not necessarily ‘best practice’ and, depending on their timing, may not be aligned with the London Plan 2021, there are a number of useful conclusions that can be drawn for application in Wandsworth.

Note that references throughout to the Local Plan refer to the March 2016 version.



## Areas of planned and potential change



**Fig. 4:** Areas of planned and potential change

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There are a number of areas of planned and potential change within the borough:

- **Opportunity Area:** Development in London in the 21<sup>st</sup> century has focussed on regenerating redundant riverside industrial areas, including Nine Elms, designated part of the GLA's Central Activities Zone. The regeneration in this area includes restoration of Battersea Power Station, a new United States Embassy, a new high street and new public park. The Northern line is being extended from Kennington to Battersea Power Station with an intermediate station at Nine Elms.
- **Potential increased accessibility:** Crossrail 2 potential future scheme would include stations at Clapham Junction and at Balham or Tooting Broadway, as well as at Wimbledon in Merton. Increased transport accessibility would be expected

to increase development opportunities and typologies around these centres. A new pedestrian and cycle bridge between Nine Elms and Pimlico in Westminster is proposed and a new river pier is now open. The recently opened Northern Line extension to Nine Elms and Battersea has significantly increased the accessibility of this part of the borough.

- **Town and local centres:** There is an emphasis in both national and London-wide policy to support development (including increased housing) within and near town centres.
- **Site Specific Allocations:** There are a number of sites allocated for development in Wandsworth's Local Plan.

## 2.3 Borough-wide baseline

### 2.3.1 Physical qualities

#### *Topography*



**Fig. 5: Topography plan**

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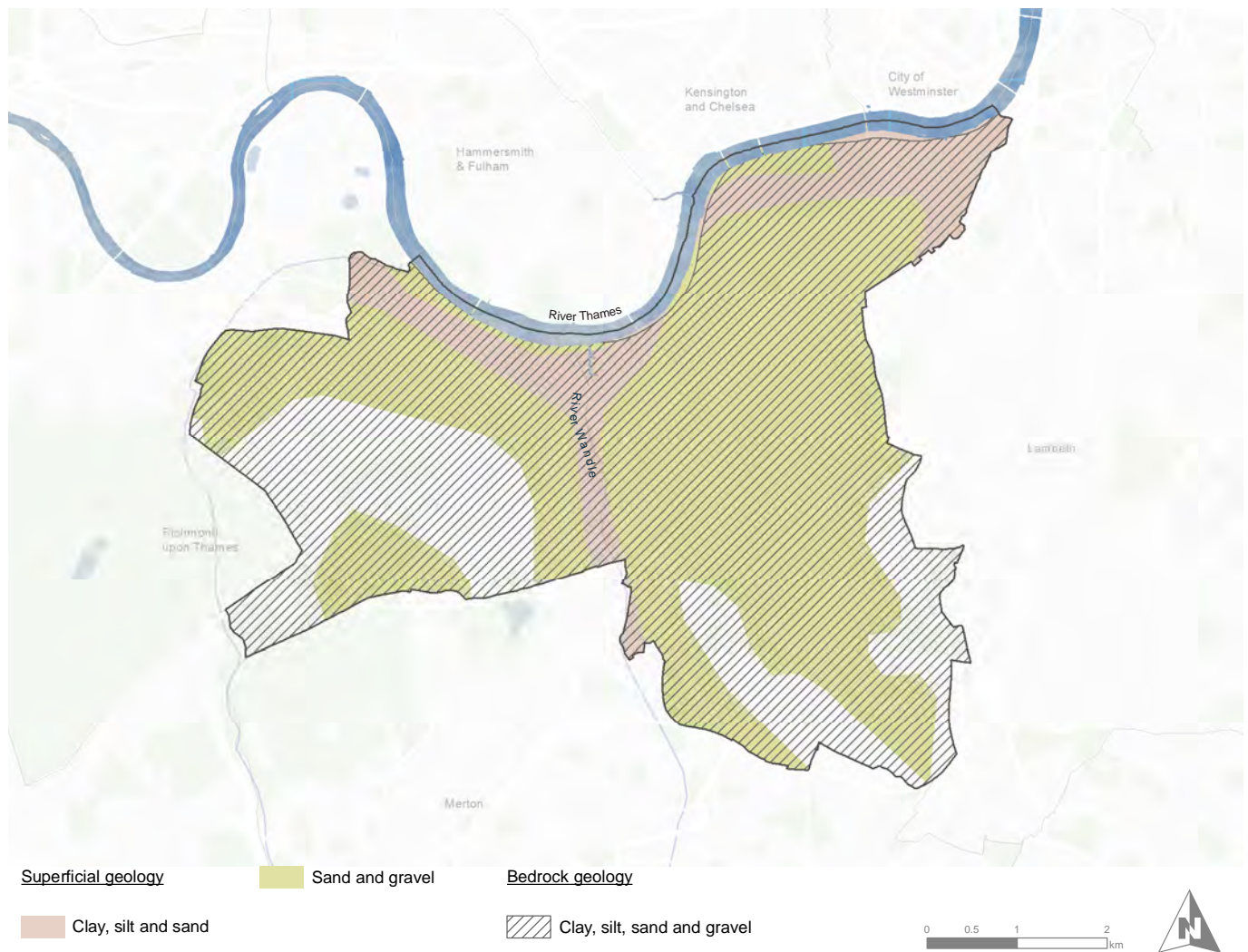
The topography of Wandsworth gently rises from the floodplain of the River Thames and the River Wandle which cuts through the centre of the borough. The highest land in the borough is a large area of former common land at the intersection between Wimbledon, Wandsworth, Putney and Roehampton.

Wandsworth is characterised by the natural topography of the three valleys of the River Wandle, Beverley Brook and Falcon Brook, meandering to the River Thames. The River Wandle cuts a broad north-south landform through the centre of the borough. Either side of the Wandle Valley are ridges of higher land: at Putney Heath and Clapham Junction. The Falcon Brook (now culverted) cuts a valley through the ridge

at Clapham Junction along the route of Northcote Road and St John's Road.

At Clapham Junction, St John's Hill/Lavender Hill gives way to a flat valley floor to the north, covering most of Battersea.

## Geology



**Fig. 6: Geology plan**  
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The geology of Wandsworth has a direct relationship with its topography.

The borough lies upon a bedrock geology of clay, silt and gravel and is overlain by superficial deposits of sand and gravel for most of the borough, with linear deposits of clay, silt and sand along the northern boundary, and concentrating in the north east corner at Battersea Park.

More specifically, the bedrock geology of the borough comprises a chalk aquifer, overlain by the Thanet Sand Formation (fine grained sand), Lambeth Group (clay with beds of sand), Harwich Formation, London Clay Formation (clay and silt) and the Claygate Member (sand, fine grained silt and clay).

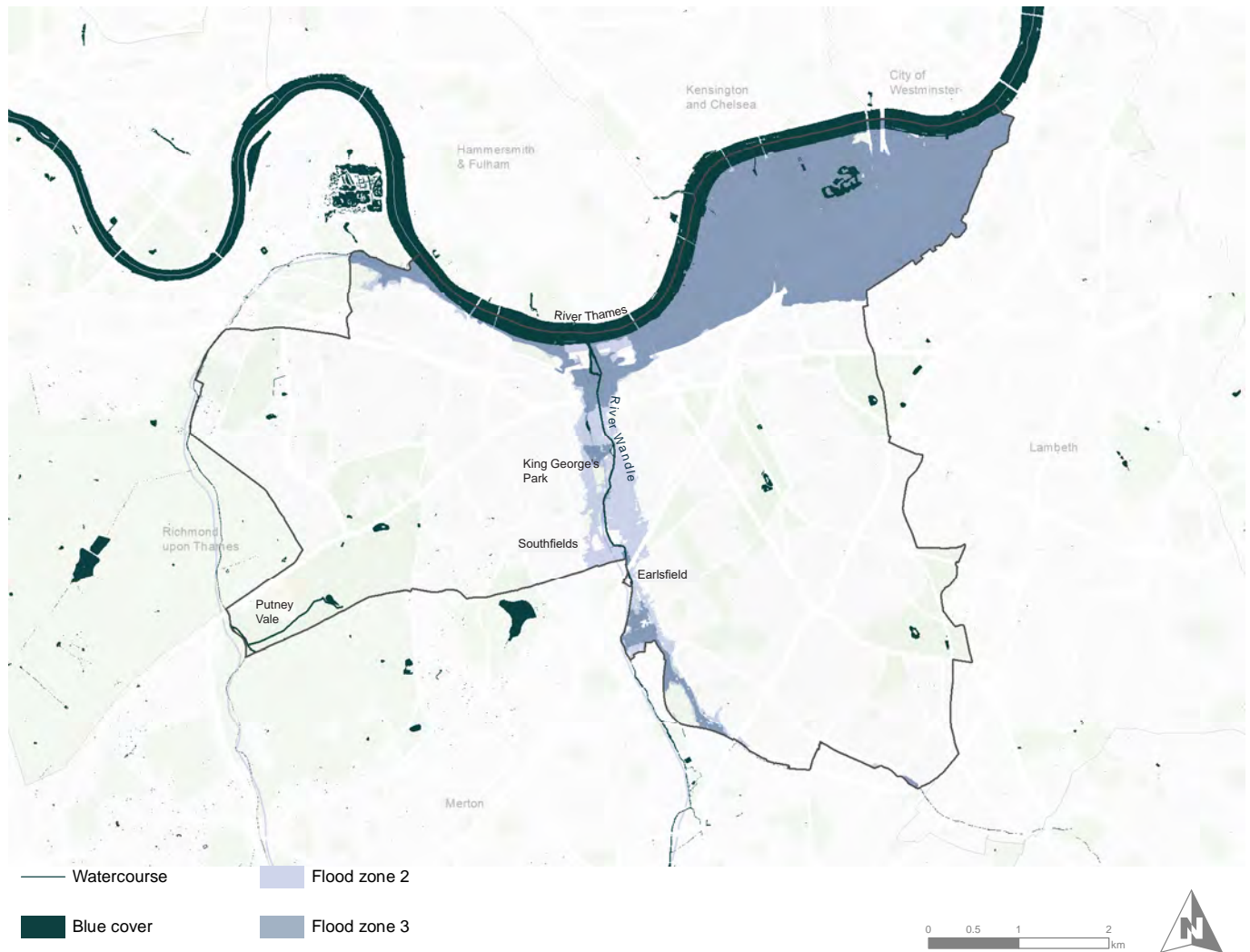
Superficial deposits are present in the majority of the borough and consist of various river terrace deposits,

Head, Langley Silt Member and alluvium.

The river terrace deposits are subdivided into a number of units that are differentiated on the basis of altitude but are geographically similar. These are gravel, sandy and clayey. In the northern and central low lying areas, close to the River Thames and the River Wandle there are significant deposits of various gravel formations. Member deposits are overlain on the bedrock at the Wimbledon and Wimbledon Common area as well as at Clapham, Wandsworth Common and Tooting Common.

Significant deposits of Head (clay, silt, sand and gravel) are distributed across much of the wider area and ribbons of alluvium (mainly sand, silt and clay) are associated with the rivers.

## Hydrology



**Fig. 7: Hydrology plan**

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Parts of Wandsworth are at risk of flooding from the rivers Wandle, Graveney, Thames and the Beverley Brook.

Water levels in the River Thames are tidally influenced and a large area of Battersea and Clapham Junction with smaller areas to the north of Wandsworth and Putney are in Flood Zone 3. However, these areas benefit from flood defences as part of the Thames Tidal defences. The presence of the flood zone indicates the historic extent of the river valley, which has shifted, cut and re-cut a series of channels along its course, influenced by glacial activity half a million years ago.

The River Wandle enters Wandsworth from the south and discharges into the River Thames. The river splits in two near Wandle Recreation Ground and is culverted in sections. With this, Wandsworth Town,

King George's Park, Southfields, Earlsfield and Summerstown have areas at risk of flooding.

The River Graveney is a tributary of the River Wandle which defines the south eastern boundary of the borough and joins the River Wandle to the south of Wandsworth. It is culverted in several sections and divides in the vicinity of Tooting, where the northern boundary runs along the Wandsworth boundary before joining the River Wandle. Areas to the south of Tooting Graveney are in flood zone 2.

The Beverley Brook flows from south to north through Putney Vale, to the west of Roehampton and Putney Lower Common before joining the River Thames. However, there is very little area in the borough affected by the floodplain of the Beverley Brook.

## Green infrastructure



**Fig. 8:** Green infrastructure plan

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Wandsworth is comparatively rich in green spaces compared to other inner London boroughs with approximately 1,060 ha of open space, according to the Open Space Survey (2020). The avenues of trees in West Hill, Streathbourne Road and within Battersea Park are very distinctive, serving to reinforce legibility and sense of place.

Open spaces within the borough form important parts of wider strategic green networks. Strategic networks that pass through the borough include the Thames Path, Wandle Trail and Capital Ring Footpaths. These provide recreational opportunities for residents and visitors, and improve active travel accessibility between Wandsworth and surrounding boroughs.

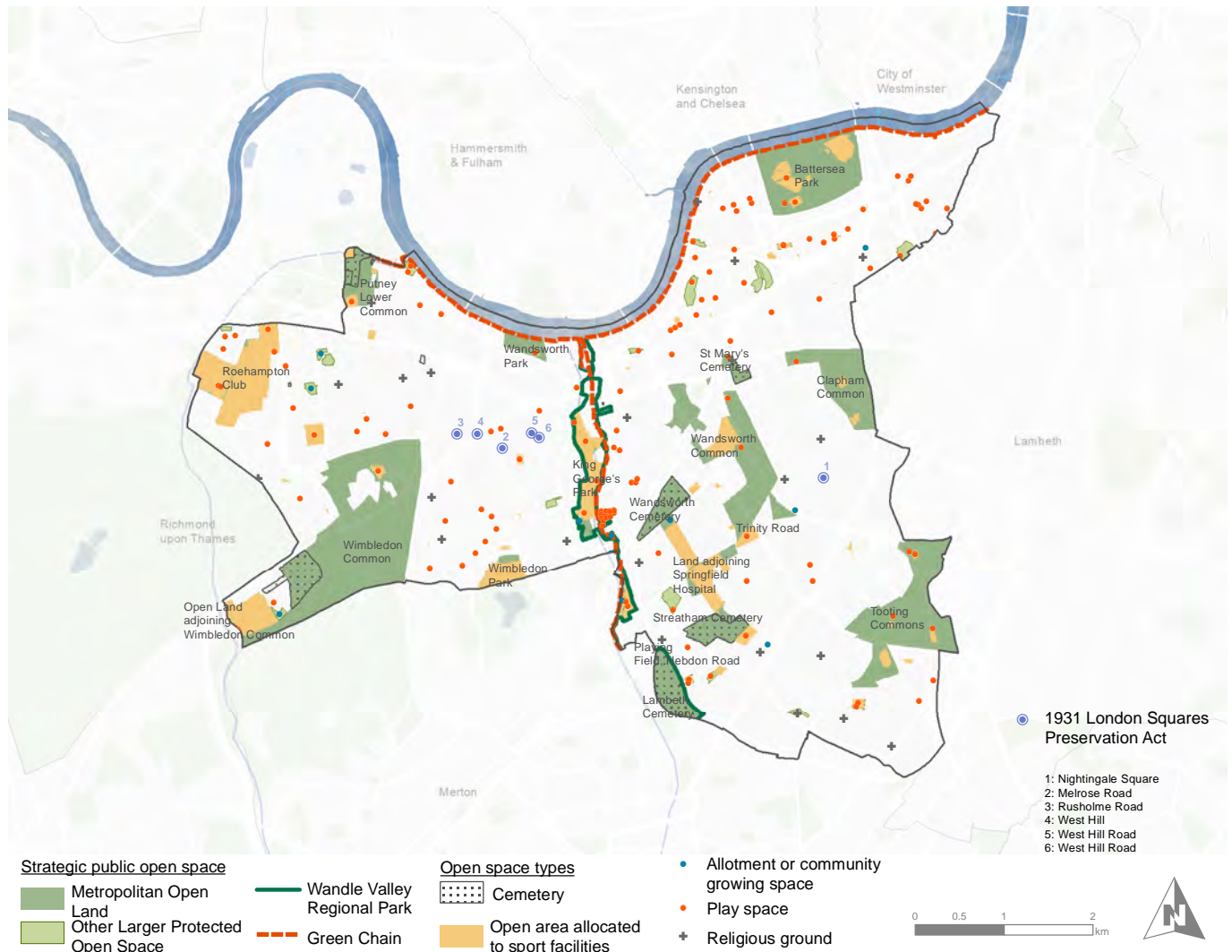
The River Thames is an important natural asset of strategic importance in terms of recreation, visual

amenity and natural habitat. Green chains have been identified, which extend along the Wandle Valley, for example linking Wandsworth and Tooting Commons with other open spaces in neighbouring boroughs.

Commons providing a rich natural and semi-natural biodiversity resource, as well as recreational and amenity value for local communities. Wimbledon Common, Putney Heath and Putney Lower Common provide the largest extent of heathland in London.

Additionally, large areas of the borough hold biological importance, with the majority of the common areas and river corridors designated as Sites of Importance for Nature Conservation. Putney Heath is designated as a Special Area of Conservation and parts of Battersea Park are designated as Local Nature Reserves.

## Public open space



**Fig. 9: Open space plan**  
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The large open spaces in the borough represent a distinctive element of the landscape character. The four commons: Clapham, Tooting, Putney Lower/Wimbledon/Putney Heath, and Wandsworth are important open spaces with recreational and amenity value. They are designated Metropolitan Open Land (MOL) and protected by the Wimbledon and Putney Commons Act of 1871 from being enclosed or built on.

There are four major parks, also designated MOL: Battersea, King George's, Wandsworth and Wimbledon and Richmond Park (a grade I Registered Park and Garden), adjoins the borough. MOL plays an important structural role in London, Wandsworth's open spaces are recognisable and clearly distinguishable from the built up area, for example Tooting and Wandsworth Commons particularly when viewed from transport corridors passing through these spaces.

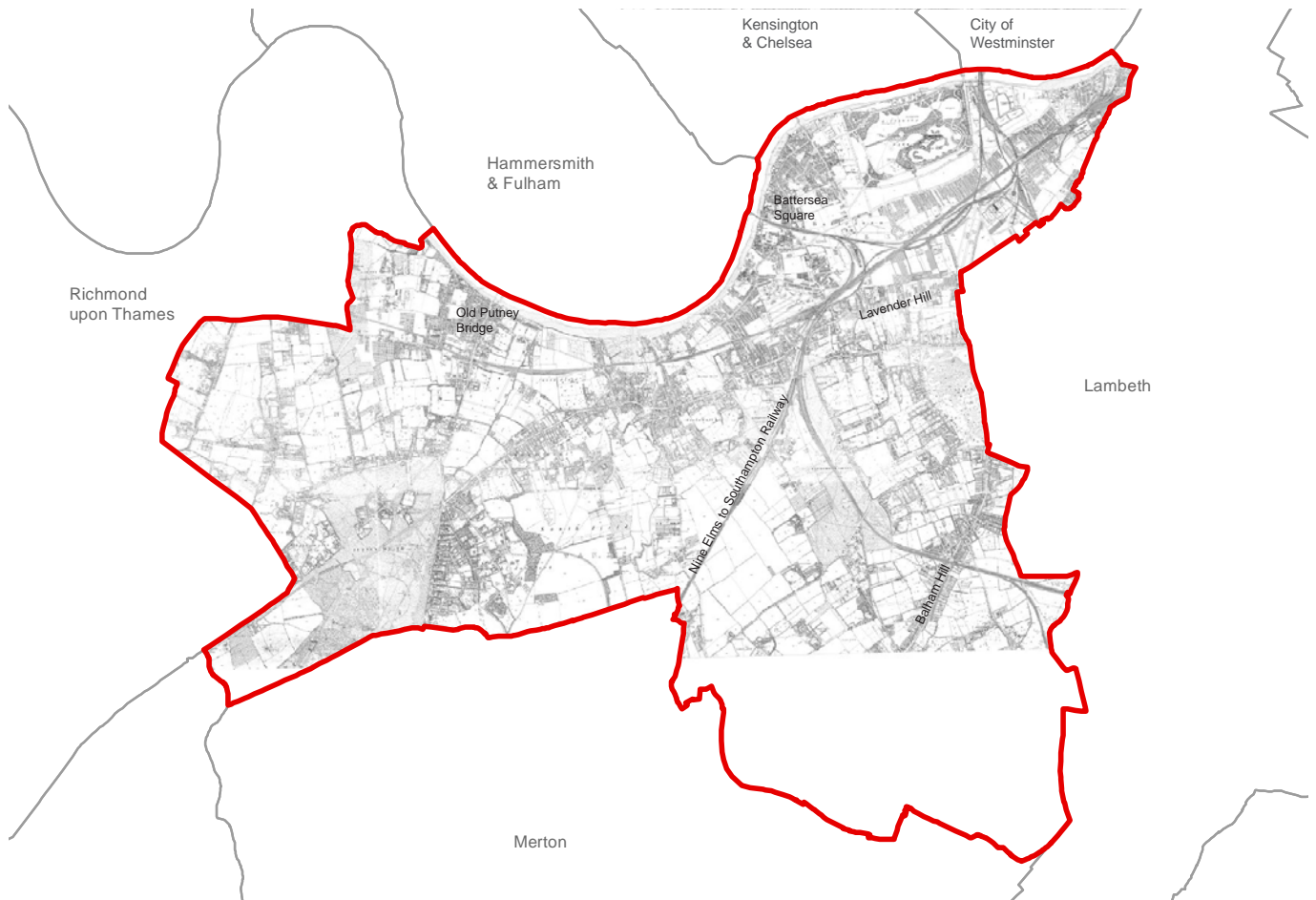
Open spaces in the borough are not just green spaces but include small urban squares, spaces and piazzas that contribute to the character and enjoyment of an area. Six open spaces in the borough are protected by the 1931 London Squares Preservation Act. These are Nightingale Square, Melrose Road, Rusholme Road, West Hill, and two on West Hill Road.

The borough also contains a number of private clubs and open spaces, including The Roehampton Club and the Bank of England Sports Ground. Whilst these do not necessarily benefit the general public, they do provide high quality sports facilities that play a strategic role in organised sports provision attracting users from outside of the borough. A number of small local parks and pocket parks throughout the borough provide children's play space, however, this is deficient in some areas, particularly between the Commons.



## 2.3.2 Cultural qualities

### Historic evolution



**Fig. 10:** Historic map 1869-1874

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Settlement in the borough dates back to the pre-historic and Iron Age eras, although became more developed in the Roman period. Balham Hill/High Road was part of the Roman route from Chichester to London. By the medieval period early villages had become established, such as Battersea Square around St Mary's Church. Much of the area was being farmed, and later more specialist market gardening developed particularly around Lavender Hill.

The rivers provided a resource for the growth of industry, including ship building, brewing and craft industries. The Huguenots influenced the development of the latter, in the late 17<sup>th</sup> and early 18<sup>th</sup> centuries.

The industrial revolution and arrival of the railways in 1838 (Nine Elms to Southampton line) brought significant transformation: the landscape of medieval field patterns began to give rise to the development of estates and a rapid increase in population.



**Fig. 11:** View upstream along the river bank from Old Putney Bridge, 1796

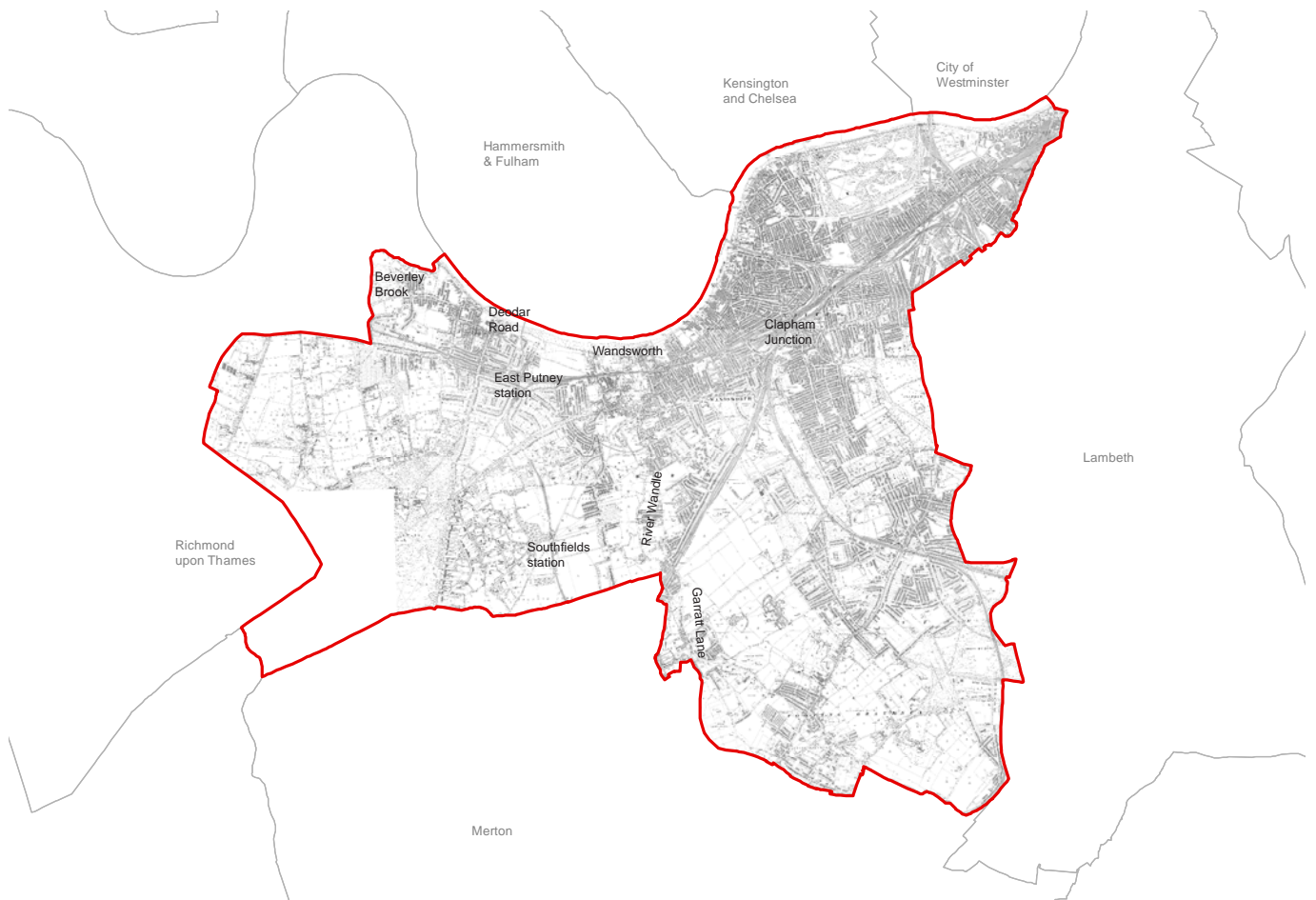
© Wandsworth Heritage Service



**Fig. 12:** Battersea Square, 1868

© Wandsworth Heritage Service





**Fig. 13: Historic map 1896**  
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Into the late 19<sup>th</sup> century the rivers at Wandsworth provided a context for the growth of industry, particularly brewing, flour milling and craft industries. North-south through the centre of the borough, the River Wandle cuts a wide valley, flowing north into the Thames. The Falcon Brook (now culverted) flows through Clapham Junction to join the Thames at Battersea and Beverley Brook flows north through Richmond Park and out to the River Thames.

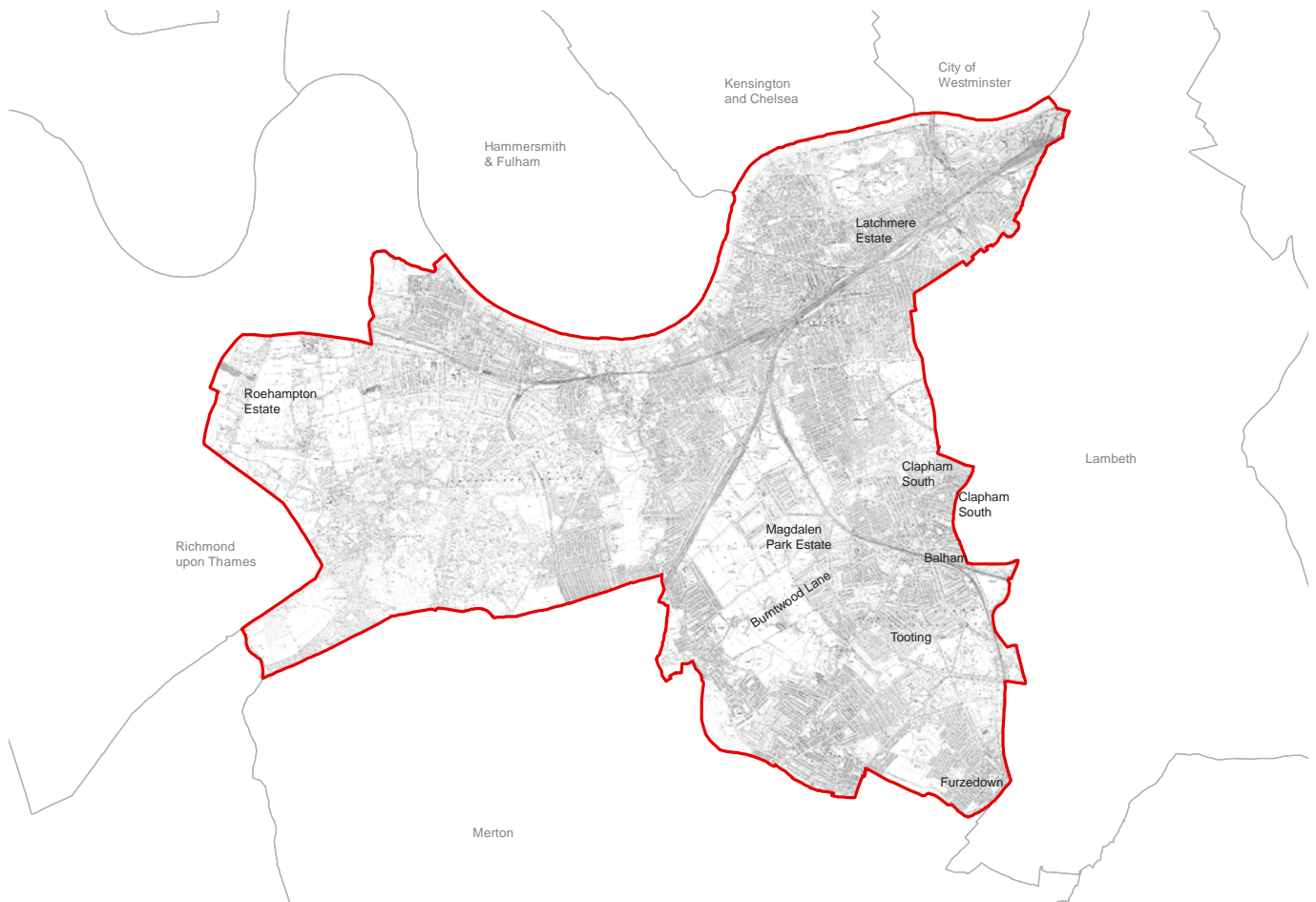
The underground railway first came to Wandsworth in 1890 with the opening of stations at East Putney and Southfields. These brought further growth to these areas, continuing the move away from a largely agricultural landscape.



**Fig. 14: View south west along Garratt Lane towards Earlsfield station, 1895**  
 © Wandsworth Heritage Service



**Fig. 15: Deodar Road viewed from the river, 1895**  
 © Wandsworth Heritage Service



**Fig. 16:** Historic map 1916-19  
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In the 20<sup>th</sup> century, the former London County Council developed the Latchmere Estate in Battersea. In 1903 this was the first municipal housing in England developed using the Council's own workforce. The most striking feature of the Estate's appearance is the uniformity of the two storey terraces of yellow stock brick with red brick detailing. This was followed by the Roehampton Estate in Dover House Road in the 1920s.

The introduction of an electric tram service in 1903 brought more growth to places such as Clapham South and Balham. The Northern Line extended to Clapham South, Balham and Tooting in the 1920s, bringing further urban growth and development.

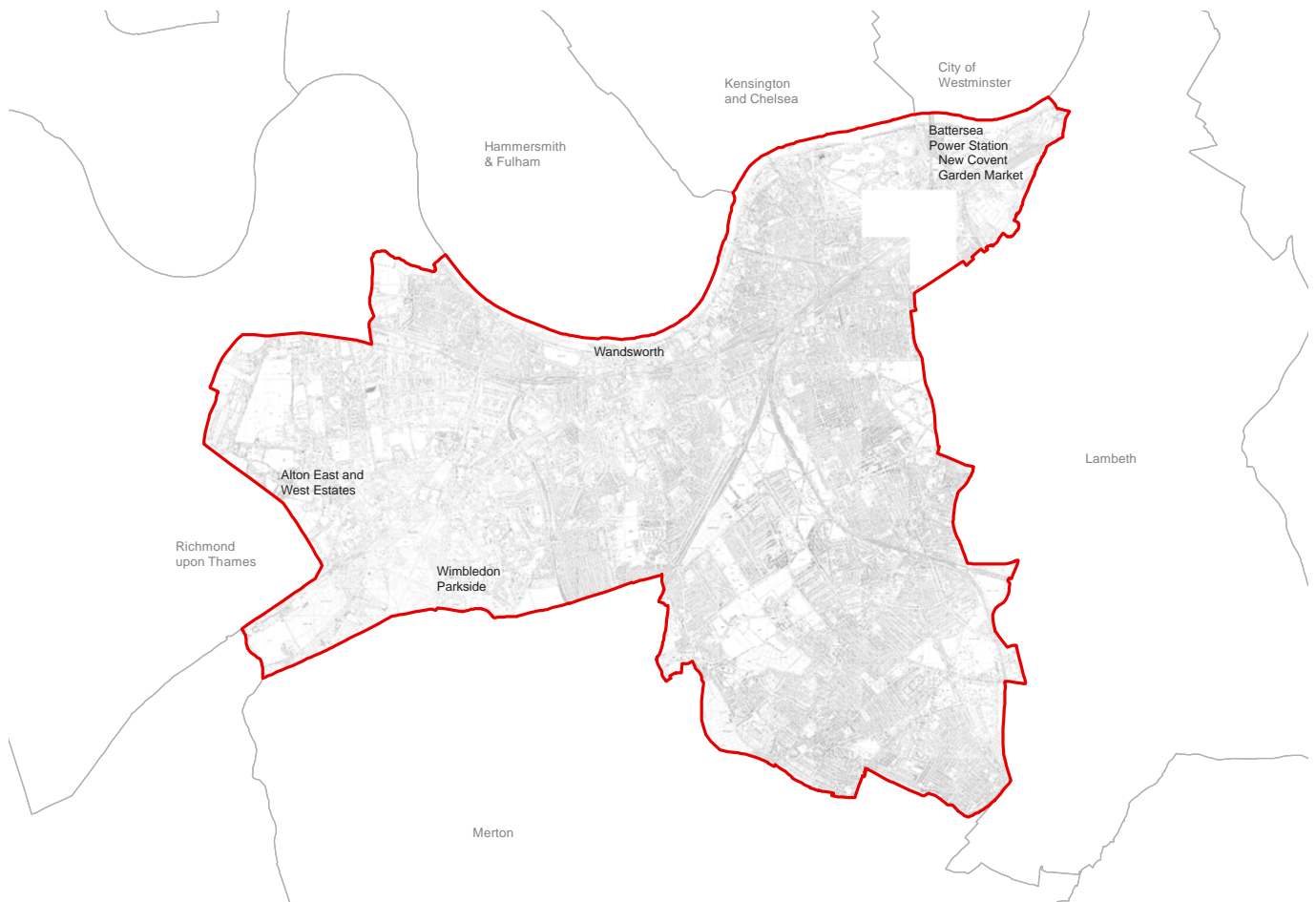
The extensive grounds of Furzedown House were developed by local builders after 1900, with a distinctive grid pattern of streets still visible today. At the same time there was extensive development of Earlsfield and Southfields, including the Magdalen Park Estate.



**Fig. 17:** Balham High Road, 1915  
 © Wandsworth Heritage Service



**Fig. 18:** Burntwood Lane in Earlsfield, 1915  
 © Wandsworth Heritage Service



**Fig. 19:** Historic map 1947-1952  
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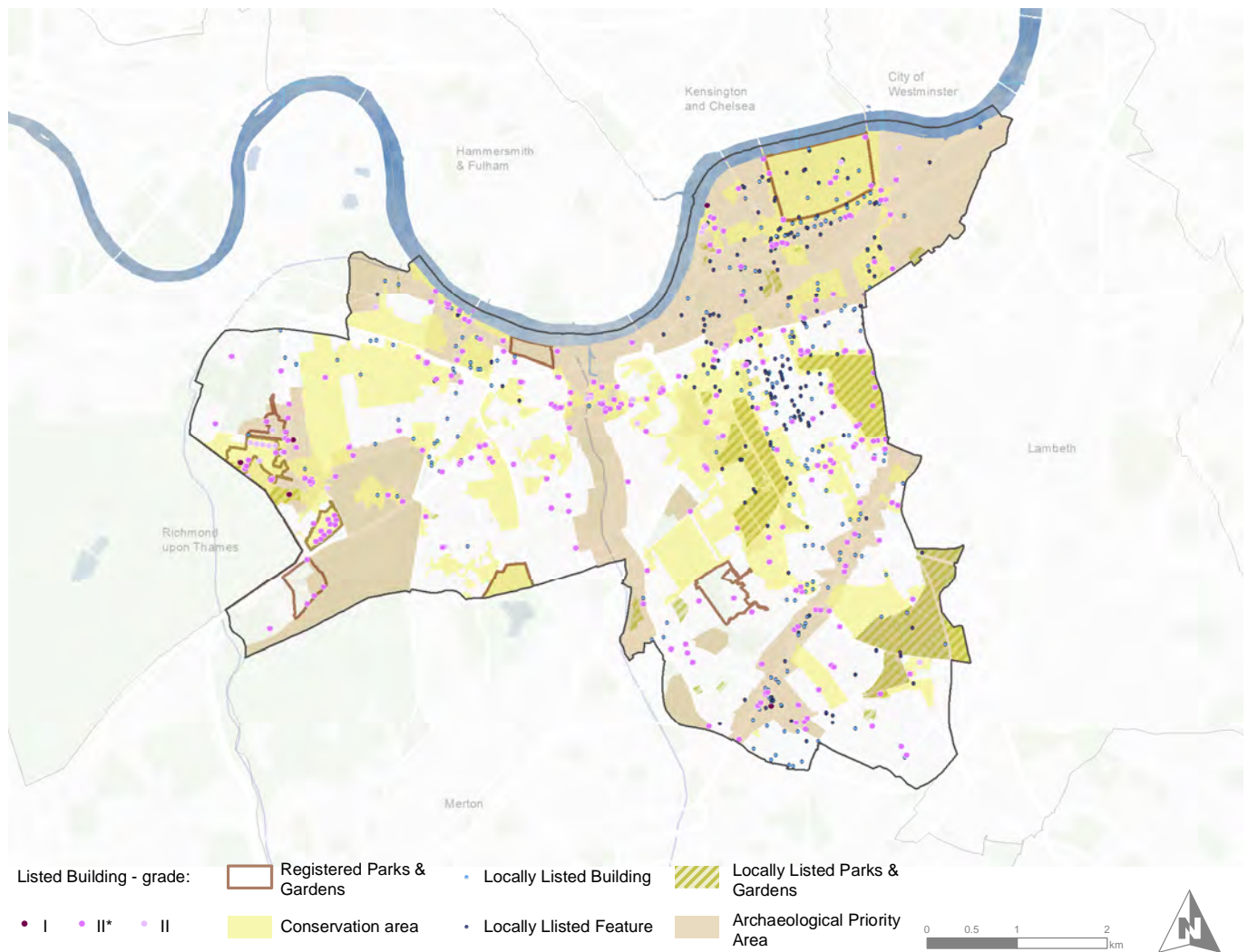
After WWI Wandsworth continued to see further urban development and transformation of some areas. This included the construction of railway marshalling yards and goods sheds across large areas/ The iconic Battersea Power Station was built between 1929 and 1935, with subsequent phases in 1937-41 and 1955. This attracted further industrial development all the way through to the 1970s with large footprint buildings such as the New Covent Garden Market. The 1970s also saw a major redevelopment of Wandsworth Town Centre, including the construction of the Arndale shopping centre, which was the largest of its kind in Europe at the time.

The Wimbledon Parkside area was home to substantial developments from the 1930s through to the 1960s, including the Ackroyden Estate. Similarly, Roehampton saw the arrival of the Alton East and West estates between 1952 and 1959 dramatically changing its character.



**Fig.20:** Alton East Estate, 1957  
 © Wandsworth Heritage Service

## Heritage assets



**Fig. 21: Heritage assets**

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Wandsworth has a rich historical heritage which is not always readily appreciated by people moving into the borough and tends to be overshadowed by the wide range of world class museums and heritage sites in central London. There are 46 conservation areas covering around 50% of the borough's area.

The borough has six Registered Parks and Gardens: Battersea Park (grade II\*); Grove House (grade II); Putney Vale Cemetery (grade II); Springfield Hospital (grade II); Wandsworth Park (grade II); and Wimbledon Park (grade II\* - partly in Merton).

Putney, Cremorne, Battersea, Albert and Chelsea bridges are all listed as buildings of special architectural or historic interest. Putney railway bridge is listed of local architectural interest.

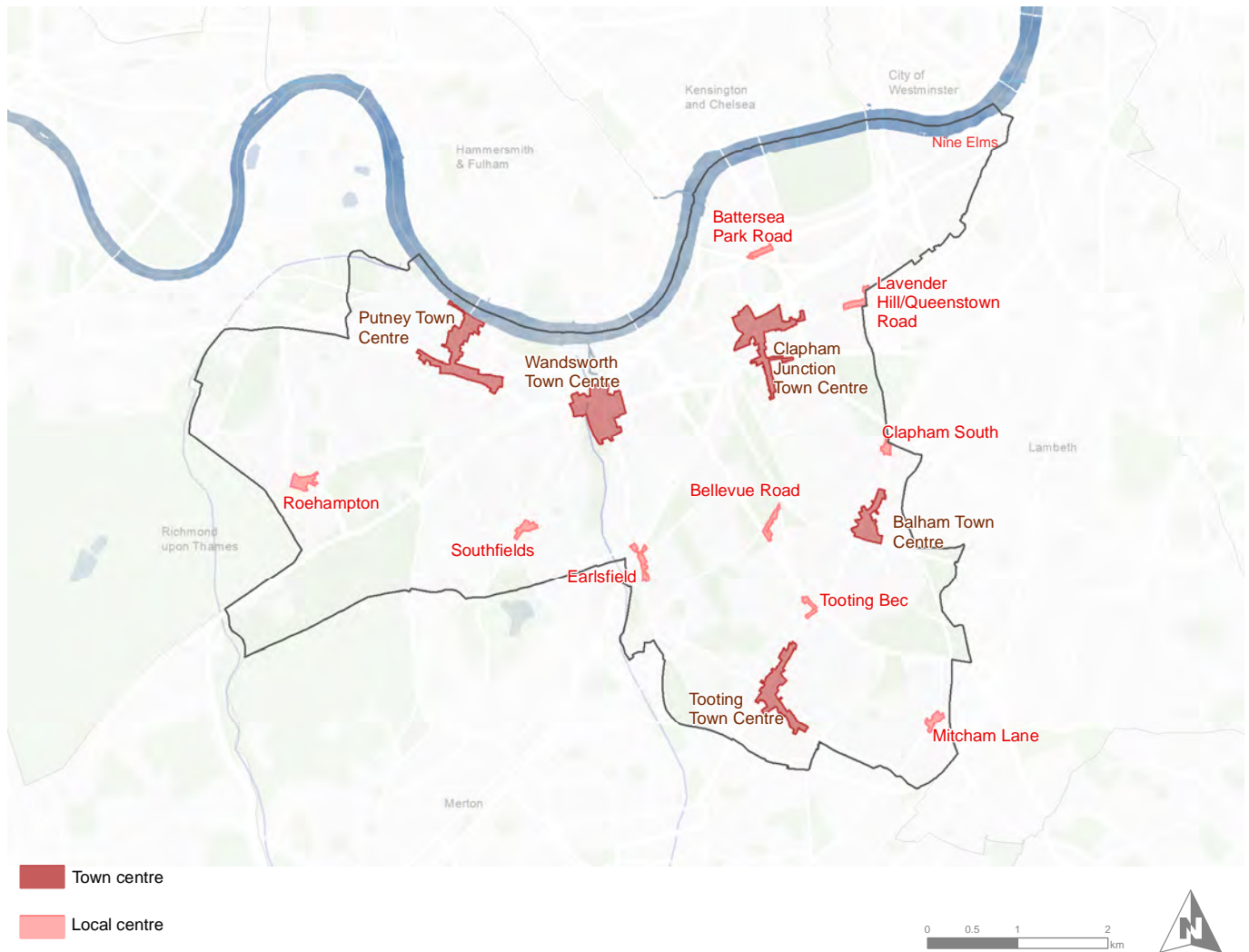
Over 500 buildings are listed as being of special

architectural or historic interest. There are five grade I buildings, designated for their exceptional interest. They are: Church of St Mary, Battersea; Mount Clare, Minstead Gardens; Granada Cinema (Gala Bingo), Mitcham Lane; Parkstead (Manresa) House, Roehampton Lane; and Roehampton House, Roehampton Lane.

There are a number of areas designated as Archaeological Priority Areas. These are concentrated along the River Thames and its floodplain, the Wandle Valley and its floodplain, Putney Heath/Wimbledon Common, Putney Riverside and Lower Common, the historic settlement of Roehampton, Balham High Road, and eight cemeteries.

There are around 520 buildings on the local list of historic interest, and 73 parks and gardens of interest, compiled by the London Parks and Gardens Trust.

## Urban centres



**Fig. 22: Town and local centres**

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Wandsworth includes a diverse range of communities and many distinct districts including Clapham Junction, Battersea and Nine Elms, Balham, Tooting, Wandsworth, Earlsfield, Southfields, Putney and Roehampton.

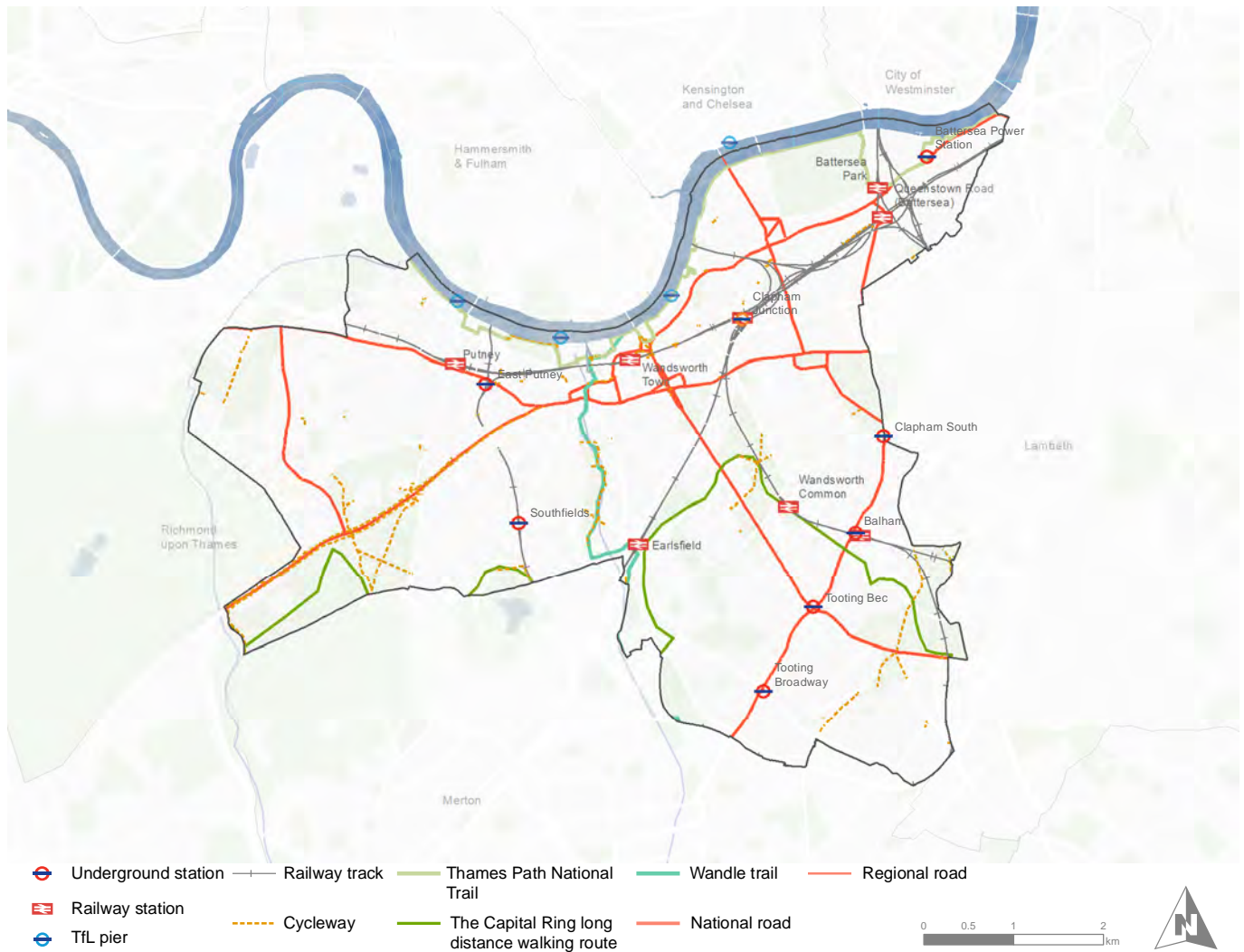
Wandsworth does not currently have one strategic retail centre. It has five town centres serving distinct areas of the borough, backed by nine local centres. These centres give focal points and identity to the communities that make up the borough and demonstrate that it is a collection of varied neighbourhoods rather than a homogeneous or coherent whole. The town centres provide a range of services including retail, leisure, community facilities, and business floorspace. The borough is made up of 20 Wards.

Wandsworth is bordered across the Thames to the north

by LB Hammersmith & Fulham, RB Kensington & Chelsea, and the City of Westminster. The borough therefore shares characteristics with central and west London.

However, in line with the rest of south east England, demand and development pressure for new housing is a major issue in the borough, exacerbated by the increased popularity of Wandsworth as a place to live. There has been considerable redevelopment over the past 30 years on brownfield sites and opportunities remain particularly on the Thames riverside, in the town centres, particularly Putney, Wandsworth and Clapham Junction, and in Nine Elms with the emerging Central Activities Zone retail centre. Significant redevelopment is taking place in Wandsworth Town.

## Transport network



**Fig. 23: Transport network**

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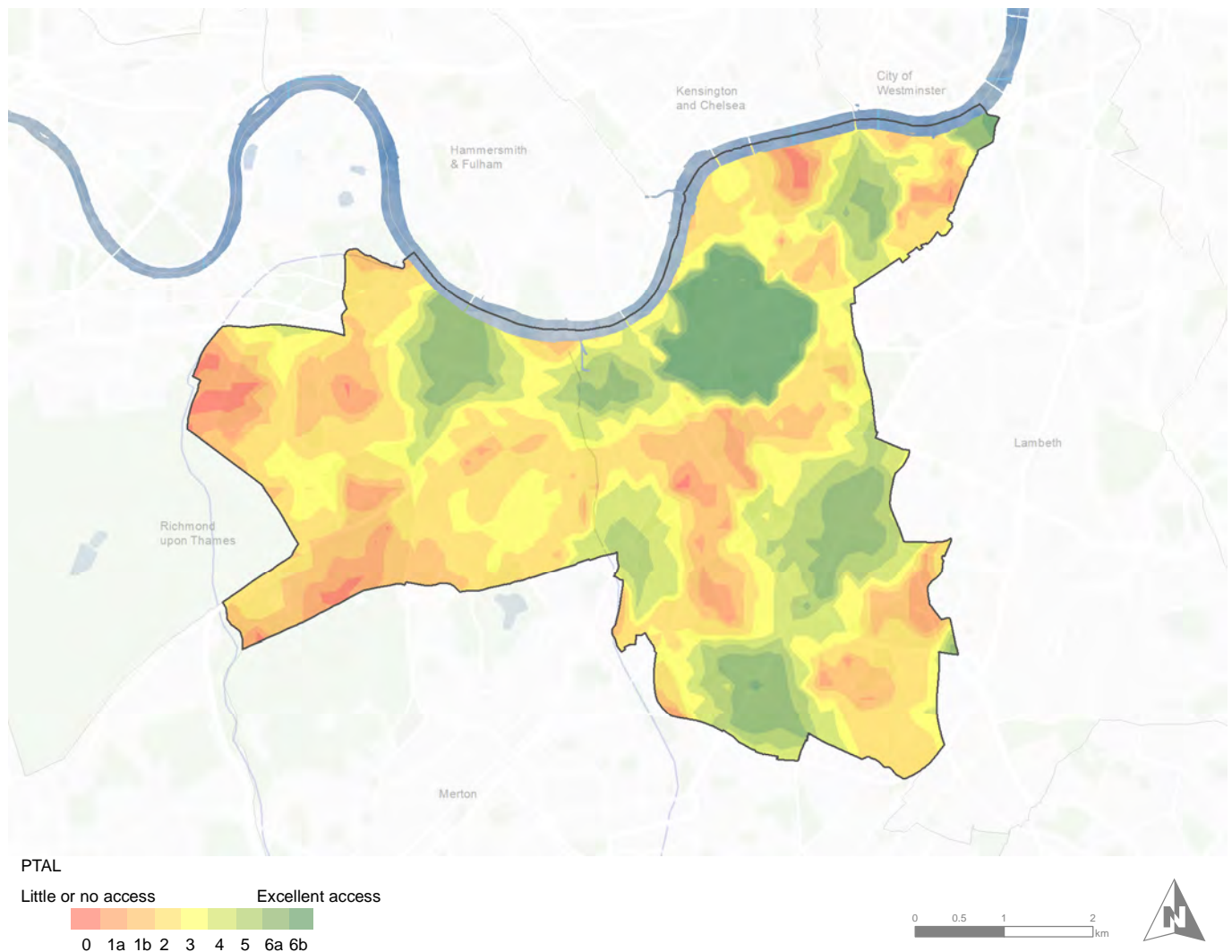
The arrival of the railways in 1838 with the opening of the Nine Elms to Southampton line brought rapid transformation: acting as a catalyst for development of industrial and residential areas. The underground railway first came to Wandsworth in 1890 with the opening of stations at East Putney and Southfields. The Northern Line extended to Clapham South, Balham and Tooting in the 1920s.

Today, Wandsworth benefits from good rail and tube links, with six London Underground Stations and eight overground stations. The Northern Line extension, with new stations at Nine Elms and Battersea Power Station, is scheduled to open in 2021 and will provide improved London Underground links in the north east of the borough. In the longer term, Crossrail 2 has the potential to ease crowding on some local routes, and improve connectivity both into central and north east London as well as south and east/west.

There are approximately 440 km of national and regional roads in the borough with 33.5 km of highway designated as part of the Transport for London road network including the South Circular. Eight Thames bridges link the borough to the north side of the river. The transportation opportunities offer good accessibility, particularly to central London, with orbital movement facilitated by the completion of the 2012 London Overground Orbital route.

The borough also offers an extensive network of public footpaths utilising green corridors. Two of the six London strategic walking routes, the Thames Path and the Capital Ring, pass through the borough. Other traffic-free walking opportunities include the Wandle Trail and routes passing through the borough's parks and commons.

## Public transport accessibility



**Fig. 24:** Public transport accessibility levels  
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Public transport accessibility levels vary across the borough. Areas around Clapham Junction, Balham, north of Tooting, Putney and Wandsworth Town are well linked with efficient access to multiple modes of public transport. This is created by proximity to the main overground and underground rail lines into central London.

However, areas of Putney Heath and Roehampton suffer from particularly poor public transport, where, in some of the more deprived areas, there is little or no access. Despite this, 71% of all trips by Wandsworth residents are already made by walking cycling or public transport. A main aim of the Wandsworth third Local Implementation Plan is to increase this to 82% by 2041. It is recognised that public transport in the borough needs upgrading to be able to achieve this.

Despite the upcoming Northern Line extension and potential Crossrail 2 route, areas of the borough that already have relatively low levels of public transport provision, are not likely to benefit from these upgrades. Across the borough, rail and tube services will need to offer more capacity and better reliability. For many people, bus services will be the most affordable and most convenient travel option, but to attract more passengers improvements to the network will be required.

In the medium to long-term there are a number of significant, but currently unfunded, investments identified which will be required to ensure the economic and social vitality of the borough. These include improvements to walking and cycling facilities and new stations to encourage modal shift to public transport.

## Urban grain



**Fig. 25: Urban grain plan**

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The predominant form in the borough is terraced houses fronting a grid network of streets. The first municipal housing began to be built pre-war, when the former London County Council developed the Latchmere Estate in Battersea in 1903, the first municipal housing in England using the Council's own workforce. This was followed by the Roehampton Estate in Dover House Road in the 1920s, both estates based on the principles of the early Garden Cities and suburbs. The Alton East and Alton West estates in Roehampton were developed in the 1950s, their design based on pioneering work on 'slab' and 'point blocks'.

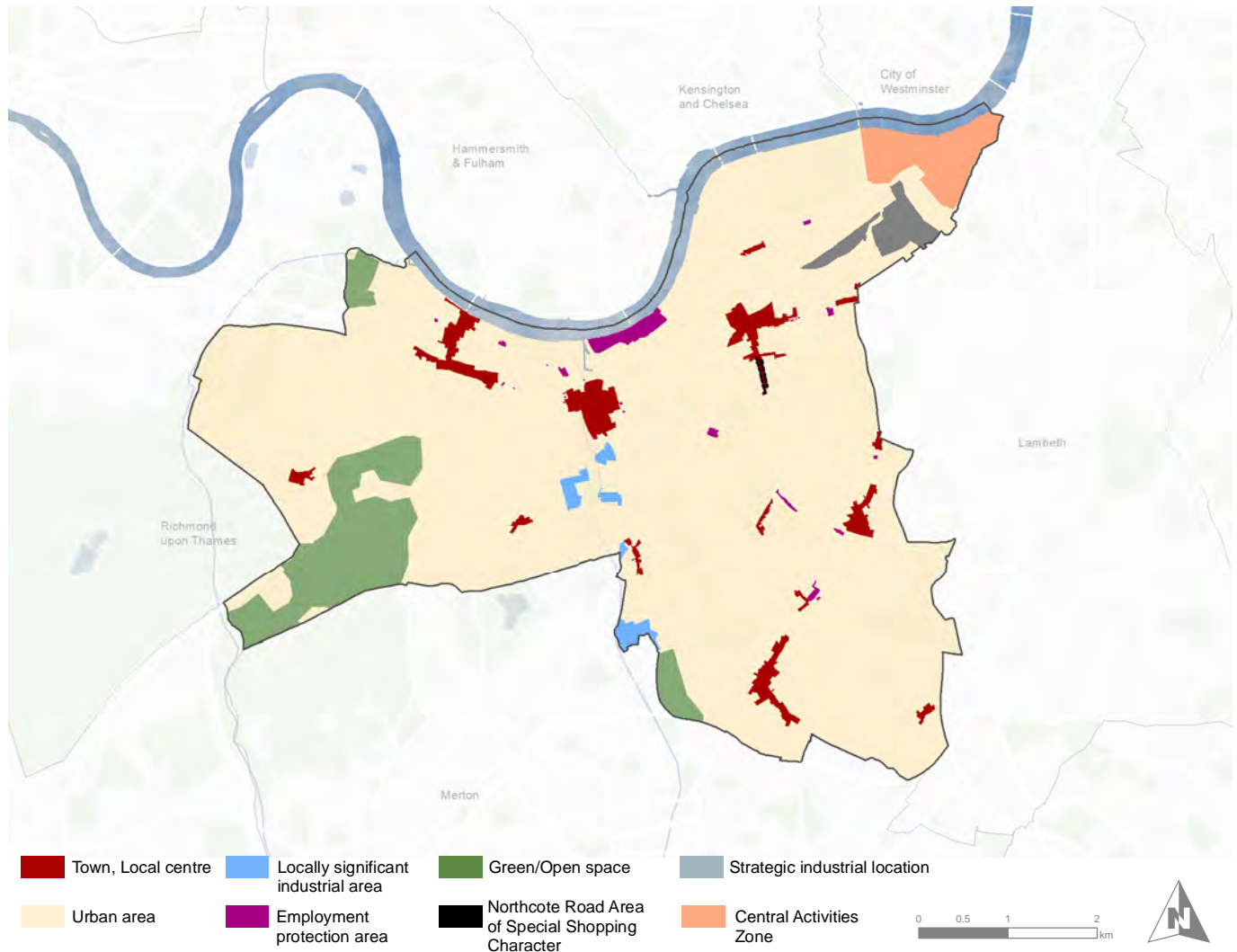
Building density varies across the borough and is at its most dense in the south and the east, in parts of Tooting, Balham, Battersea and Wandsworth Town. Conversely, much of Roehampton and Putney are considerably less dense in terms of urban form.

There are important areas of public realm including the triangular space at the centre of Battersea Square which has been a gathering place for centuries and provides a distinctive sense of place. Kersley Mews and Ernshaw Place are unique mews streets, retaining their pedestrian character with granite setts. The spatial character of the streets in the Westmead Conservation Area is formed by the brick and random rubble footways with granite kerbs and channels.

A number of conservation areas across the borough protect the special architectural and historic interest of valued places. These include the remains of small villages such as Battersea Square or even large areas of 20<sup>th</sup> century social housing like the Alton Estate.



## Land use



**Fig. 26:** Land use plan (note designations shown are from the adopted Local Plan at the time of writing)  
 Contains OS data © Crown copyright and database right 2021

A third of the borough is occupied by dwellings, many of which are within one of 46 conservation areas. A quarter of the area of the borough is occupied by open space, much of this in the form of heath and commons as well as ornamental parks and playing fields. The rest of the land consists of industrial and commercial uses, hospitals, schools and other public services as well as transport infrastructure.

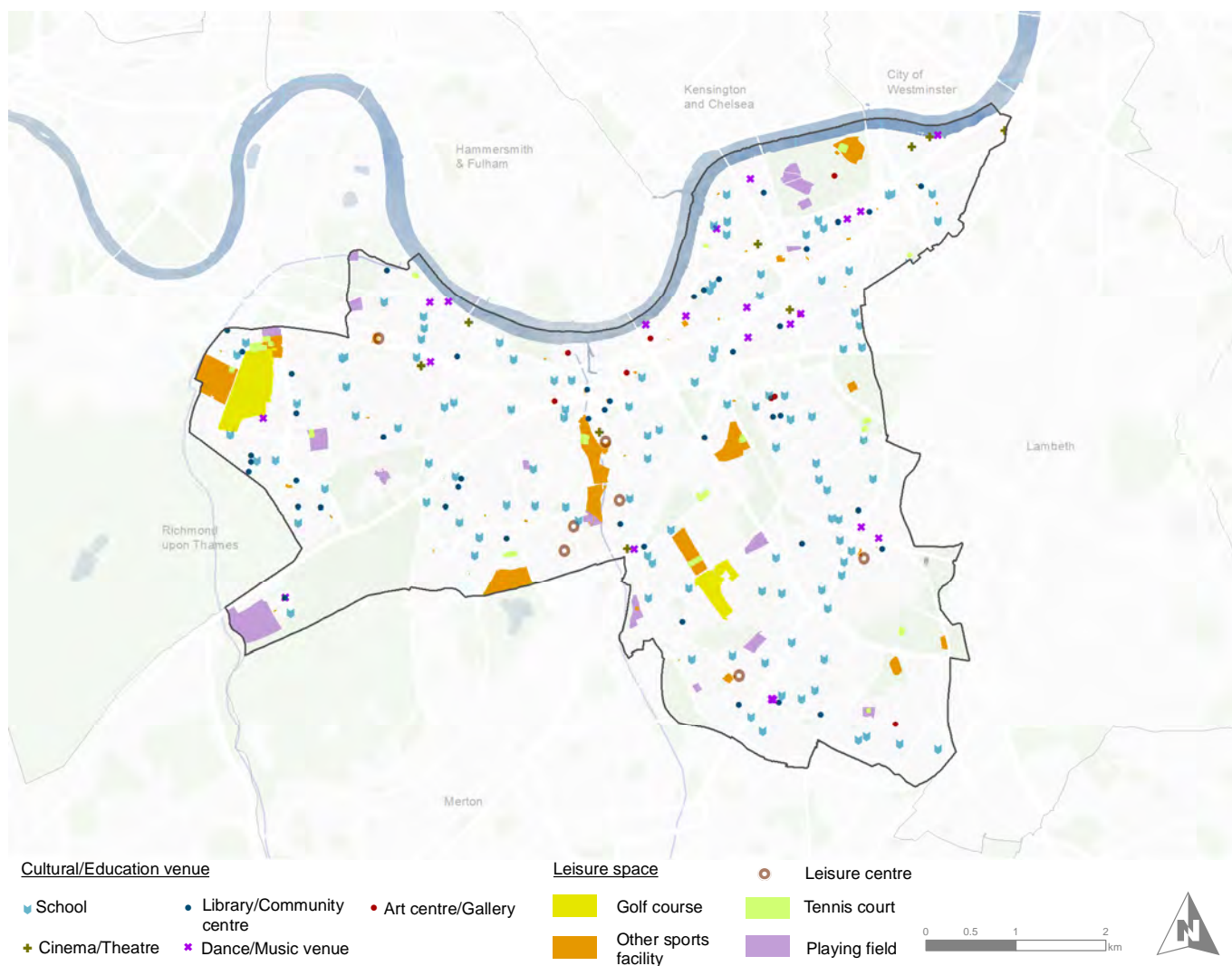
The strategic industrial location at Queenstown Road/Battersea consists of 42.8 ha of land and is home to the majority of industrial uses in the borough. Other industrial and distribution uses are scattered across the borough in locally significant industrial areas. The borough is categorised in the emerging London Plan as a restricted transfer borough. This designation encourages boroughs to adopt a more restrictive approach to the transfer of industrial sites to other uses

in protected industrial areas. This reflects the limited quantity of industrial land in the borough.

There are also a number of employment protection areas across the borough within which offices are situated, particularly serving local markets and providing floorspace for small and medium enterprises, that have good public transport accessibility or are located close to town centres and local centres.

There are five main retail centres in the borough; Clapham Junction, Putney, Tooting, Wandsworth and Balham. In addition to these, Northcote Road is an area in Battersea which is considered to have its own distinctive character, as a specialist retail area.

## Culture, leisure and education



**Fig. 27: Cultural, leisure and educational facilities**  
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Wandsworth benefits from a significant supply of cultural, leisure and education facilities. Access to the huge variety of venues and performances in central London, many of which are amongst the best in the world, is perhaps the most important opportunity for many people living in Wandsworth to engage with the performing and visual arts. Events and venues in Wandsworth complement these, contributing to the rich mix of performing and visual art available in London, but more significantly provide local opportunities to participate in the arts.

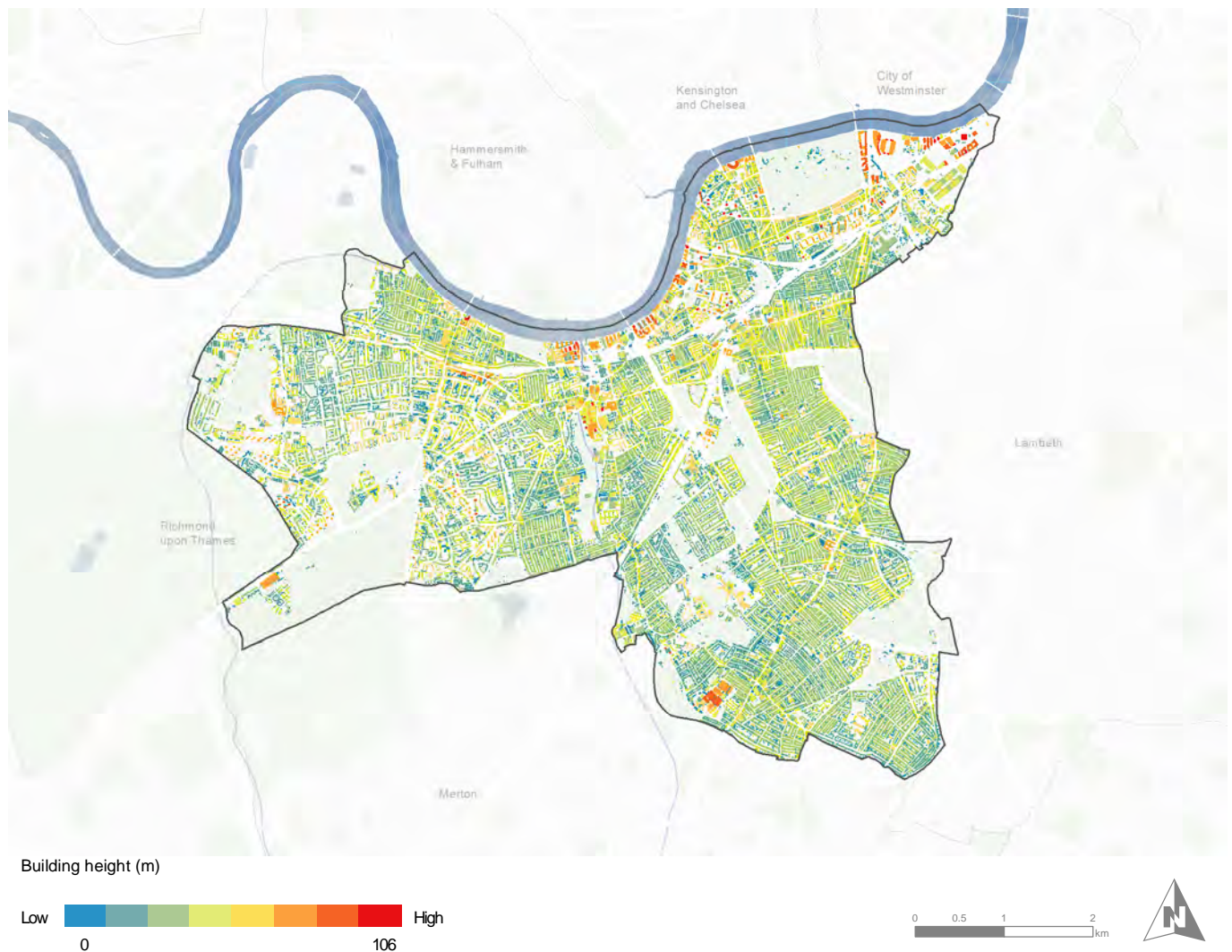
Performing and visual arts provision in Wandsworth includes Putney Arts Theatre which is a dedicated amateur theatre venue used by the resident company and others throughout the year and specialist provision for Youth Theatre; the National Opera Studio in Wandsworth which holds regular lunchtime recitals;

and more than 20 small professional music venues across the borough, such as the County Arms at Wandsworth Common and the Half Moon in Putney.

Additionally, the borough benefits from a number of training institutes including the Academy of Live and Recorded Arts at Wandsworth Common; the Royal College of Arts and the Royal Academy of Dance in Battersea - one of the largest and most influential dance education and training organisations in the world.

In terms of leisure facilities, there is a wide range of accessible and high quality facilities for sport, physical activity and recreation are available across the borough including four leisure centres and five sport and recreation centres, two athletics tracks, a boathouse and the unique Tooting Bec Lido. The borough additionally benefits from eight community centres.

## Building heights



**Fig. 28:** Existing building heights

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Tall buildings have always played a role in the townscape of Wandsworth. Buildings with community significance, dating back to the earliest times, such as churches, and in the Victorian and Edwardian times, schools, town halls and libraries have always been large and imposing, compared to the form of traditional residential dwellings. Many of these early buildings remain in the borough and many are now listed.

Building heights in the borough generally relate to their type of use. In the less deprived areas of the borough, residential streets are dominated by 2-3 storey terraced houses. Along main roads and within town centres buildings sometimes increase to 3-4 storeys. This pattern is interspersed with areas of mixed estates, including towers, which are largely a legacy of the post-war reconstruction in the 1950s-1970s. These high rise housing blocks are generally located in the more

deprived areas of the borough, particularly Battersea, where high rise towers were seen as a solution to social housing provision.

Tall buildings in Wandsworth are considered favourably as a solution to providing investment that is vital to support existing businesses and encourage new ones. Tall buildings can compliment the traditional lower-storey terraced streets in areas where growth is considered sustainable, but will not always be suitable. Therefore the character of the area will play a key role in determining applications.

### 2.3.3 Perceptual qualities

#### Views and landmarks

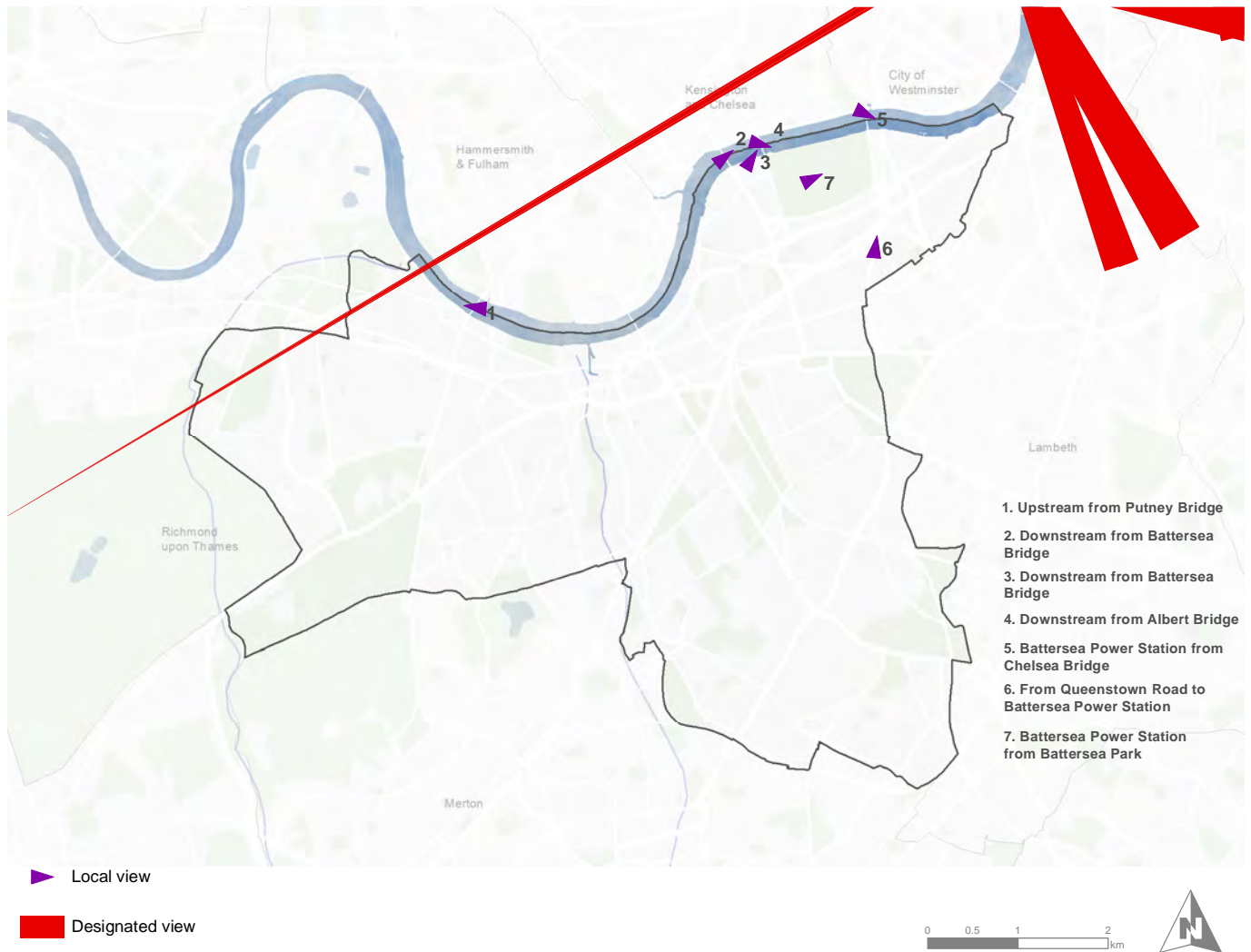


Fig. 29: Views and landmarks

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#### Landmarks

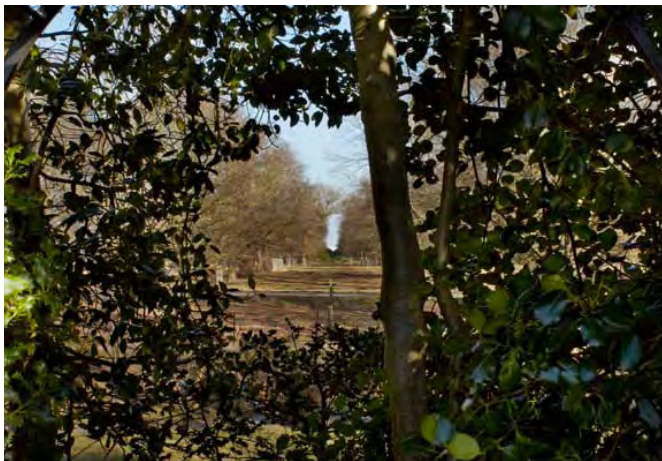
There are a number of significant landmarks in Wandsworth, including Albert Bridge and Chelsea Bridge with their towers and suspension cables which represent distinctive gateways into the borough from the north.

A number of the borough's buildings are considered strong landmarks due to their architectural design. These include the grade II\* listed Battersea Power Station, one of the most recognisable landmarks owing to its striking art deco design. The borough's public houses form local landmarks with significance both for their community value and architectural value, as recognised in the [Town Centres SPD](#).

#### Views

The River Thames provides a key focal point in the borough, which is an iconic feature of London's landscape. The higher land to the south of the borough presents wide-ranging views, including the protected from King Henry's Mound in Richmond Park (outside of the borough). There are six designated local views in the borough's [Local Views SPD](#), relating to views from bridges over the Thames and views of Battersea Power Station. Additional views and vistas of interest have been identified through this study and are noted in each of the character area profiles and listed in [Appendix I](#).

Riverside development will need to be particularly aware of important views from within and outside the borough, including from Fulham Palace Scheduled Monument and the Westminster World Heritage Site.



**Fig. 30:** LVMF protected view from King Henry VIII's Mound  
© London View Management Framework SPG



**Fig. 31:** St Paul's Cathedral when viewed from King Henry VIII's Mound  
© London View Management Framework SPG



**Fig. 32:** View upstream from Putney Bridge  
© Wandsworth Local Views SPD



**Fig. 34:** View downstream from Battersea Bridge  
© Wandsworth Local Views SPD

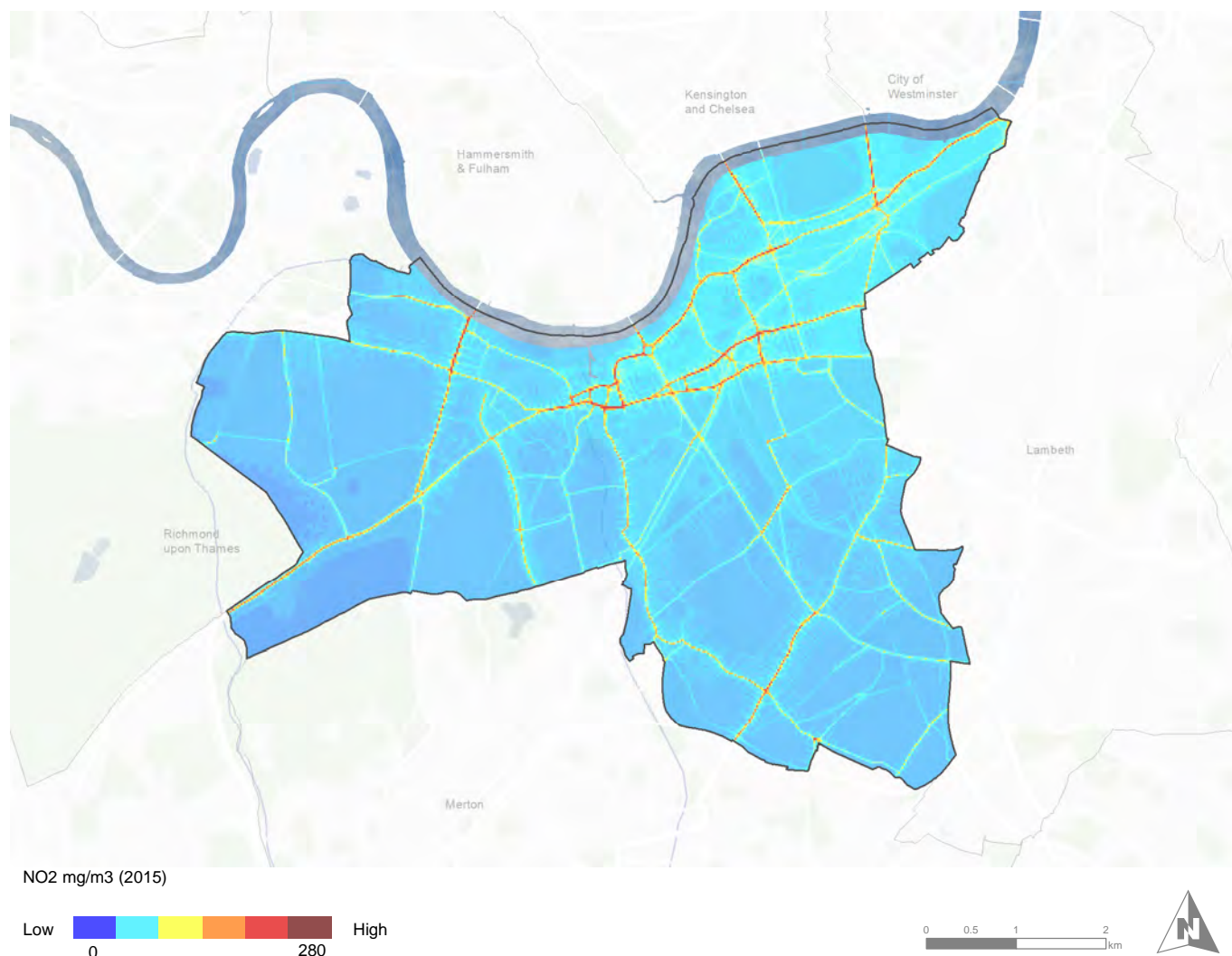


**Fig. 33:** View downstream from Albert Bridge  
© Wandsworth Local Views SPD



**Fig. 35:** Early masterplanning work on Battersea Power Station, viewed from Chelsea Bridge  
© Wandsworth Local Views SPD

## Air quality



**Fig. 36:** Air quality

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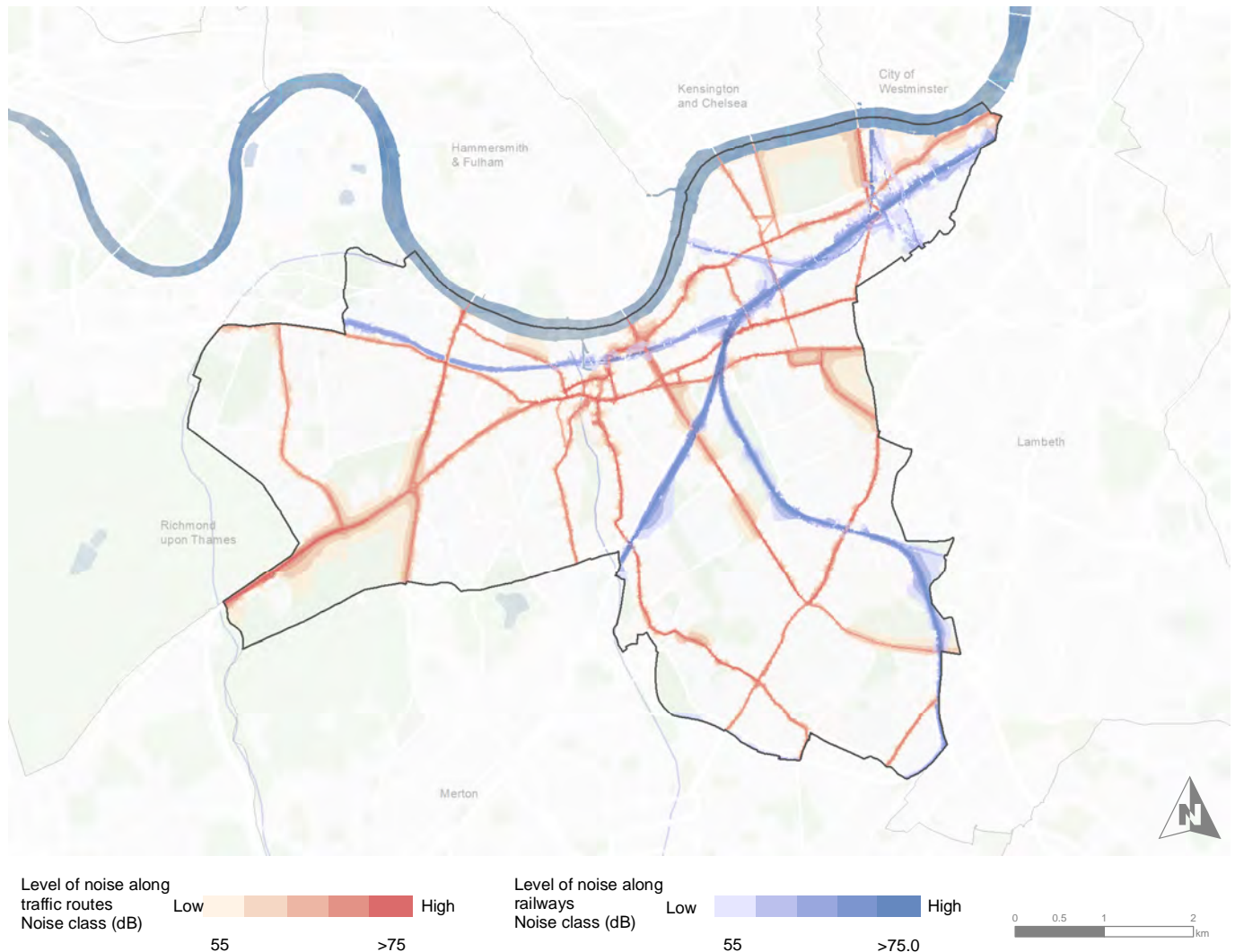
Wandsworth, as is the case for much of London, has areas of considerably poor air quality; owing predominantly to the volume of traffic on the strategic transport networks and around the areas of high industrial activity. The borough has been within an Air Quality Management Area (AQMA) since 2001 due to concentrations of pollutants exceeding national air quality standards. As a result the Council has produced an [Air Quality Action Plan](#) to achieve air quality improvements in the borough by reducing polluting emissions through measures such as reducing the need to travel by car, setting out criteria for sustainable design and construction and promoting sustainable construction in the demolition and construction process.

The latest monitoring results for 2019 confirm that air pollution in the borough still exceeds the Government Air Quality objectives, and therefore there is still a

need for Wandsworth to be designated as an AQMA and to pursue improvements in air quality. In 2019 Wandsworth operated seven automatic air quality monitoring sites and a diffusion tube network covering 32 locations around the borough. Despite this, some areas of the borough, such as at Roehampton and Putney have the lowest levels of air pollutants in the borough and this is concentrated particularly around Wimbledon and Putney Commons.

There are a number of measures set out for new developments to tackle the poor air quality conditions in the borough. These include requirement of an air quality assessment for major developments and may also be required where the development will have a significant impact on air quality or where the existing air quality environment will expose future occupiers to unacceptable pollutant concentrations.

## Noise levels



**Fig. 37: Noise levels**

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Noise pollution throughout the borough is primarily concentrated along main transport links. Due to the structure of the infrastructure network, noise pollution is concentrated to the north of the borough into Battersea and more centrally at Wandsworth Town where the central London road links converge.

Additionally, the north of the borough including Putney, Wandsworth and Battersea currently does experience some noise from aircraft approaching Heathrow Airport.

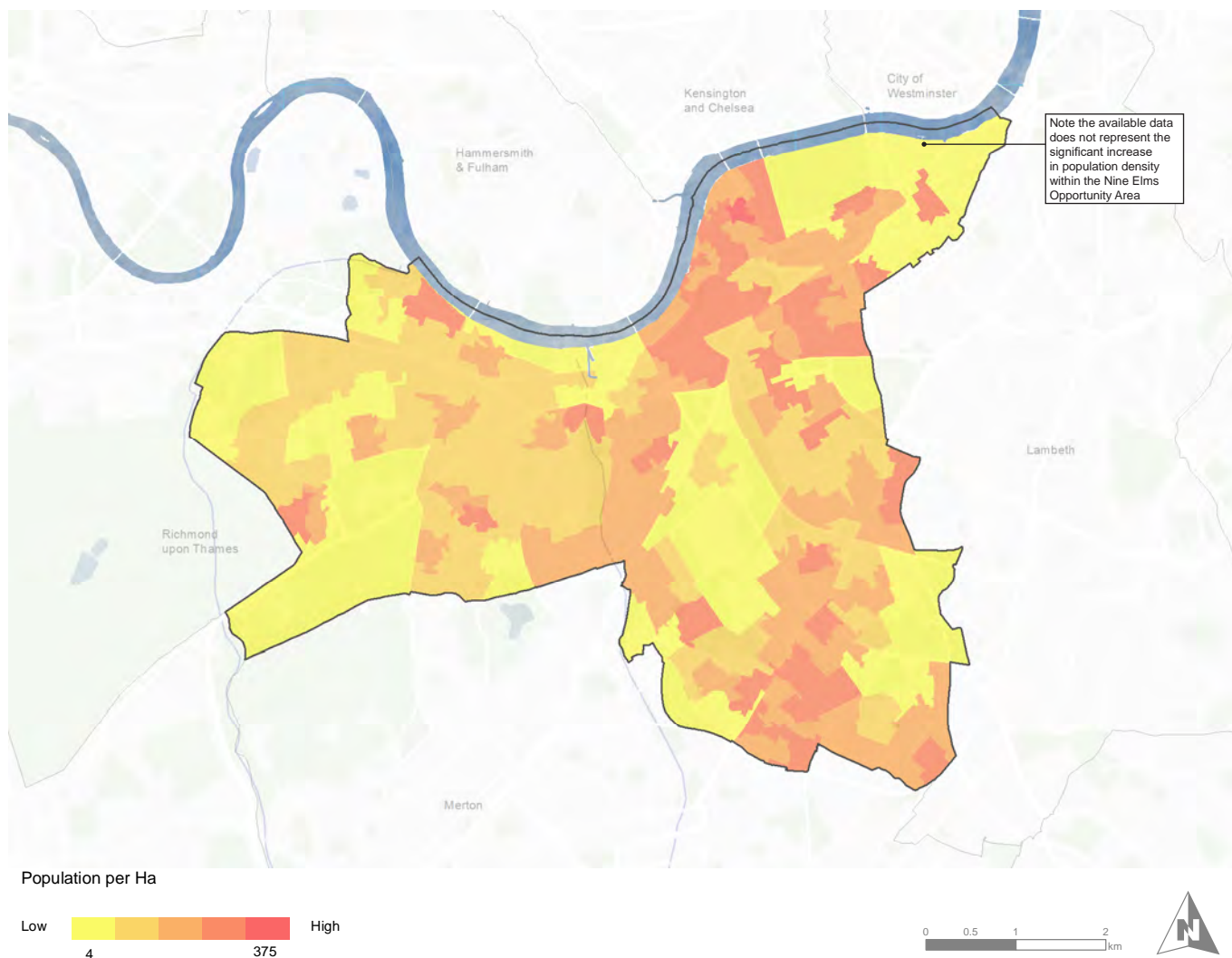
Noise pollution is concentrated in corridors along these transport networks. Locations within the borough that are not within close proximity to road and rail networks do not suffer any significant noise level other than the ambient noise of town centres and industrial areas.

According to research conducted by Campaign to Protect Rural England, South London parks are quieter than in other London locations. Most south London boroughs have a figure below the median for percentage of parks severely impacted by noise.

Wandsworth's framework to mitigate against some of the impacts of elevated noise pollution is to manage land uses by identifying areas for business, mixed-use, waste management, industry, and town centres where ambient noise levels tend to be higher and which tend to be located further away from family housing.

## 2.3.4 Social qualities

### Population density



**Fig. 38:** Population density

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Wandsworth has an average population density of 9,600 people per sq. km. Population density across the borough varies significantly with some parts of the borough, particularly the Roehampton and Queenstown wards, having low densities. However, the only areas of Wandsworth that are below the gross London-wide density figure are those which include large parks and commons such as Putney Vale, Battersea Park and Tooting Common.

Conversely, the areas with the highest densities are located in the Battersea, Tooting and Putney neighbourhoods, and other output areas dotted around the wards. Many of these are adjacent to some of the largest open spaces such as Wandsworth Common and Putney Vale. The riverside does not – apart from parts of Battersea - have particularly high population

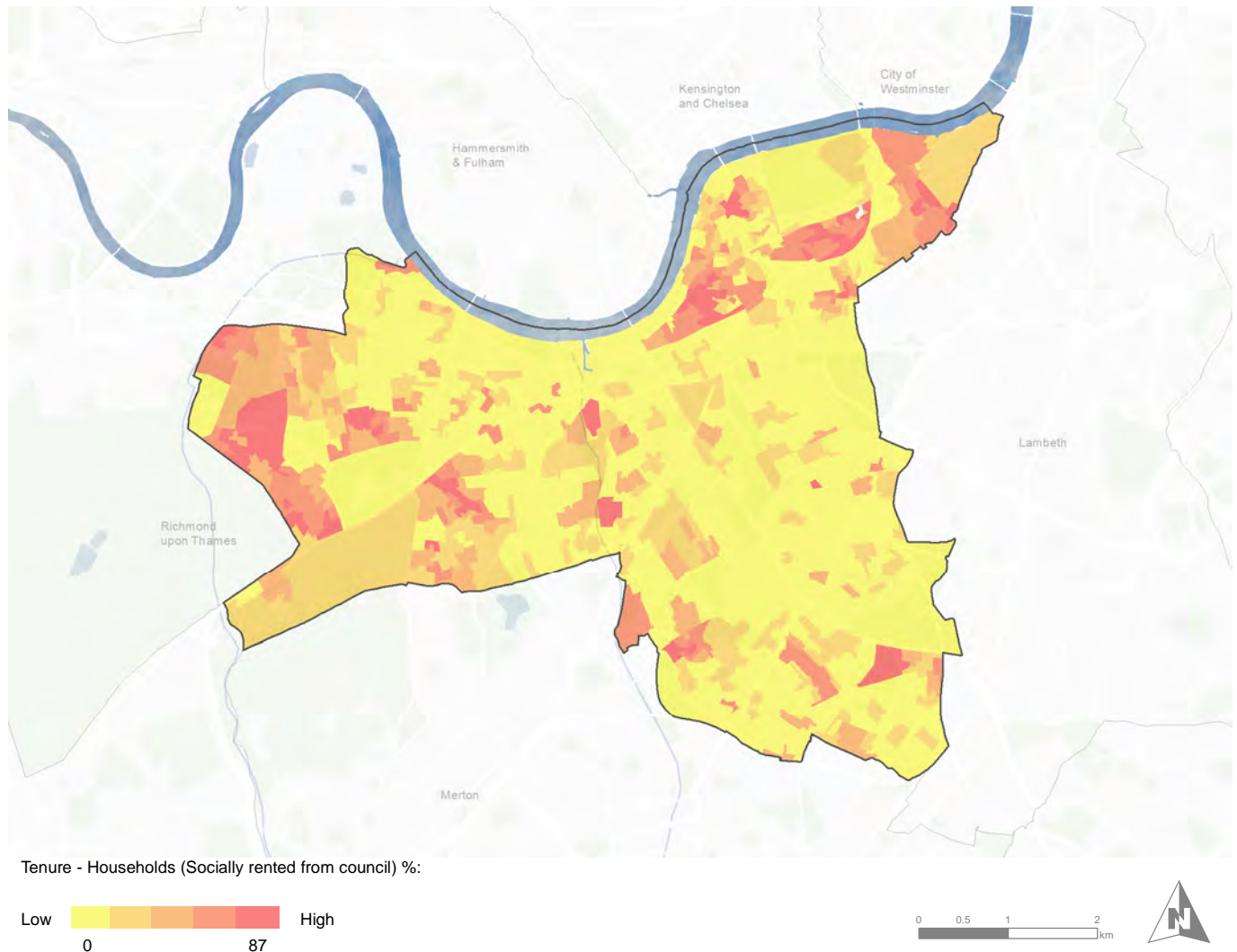
densities compared to the rest of the borough.

The higher population densities are concentrated not only around the Commons but also the more developed areas where transport links into London are good. This distribution of population density brings wider issues, including exacerbation of deprivation and population inequalities as well as access to services as the population continues to grow. In the less densely populated areas of Wandsworth, there are different issues including both isolation of elderly people as well as poor transport links and access to services when compared to the more densely populated areas.

The Queenstown ward is now becoming significantly denser as part of the major redevelopment of the Nine Elms Opportunity Area.



## Housing tenure



**Fig. 39: Housing tenure**

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Almost a third (32%) of residents rent privately, which is higher than the inner London average (29%). The median monthly rent of around £1,650 is similar to other inner London boroughs (average £1,699), but higher than the London average (£1,495) and more than double the England average (£675). This is estimated to be approximately 40% of the median household income, which makes a lot of private sector housing unaffordable to people on lower incomes.

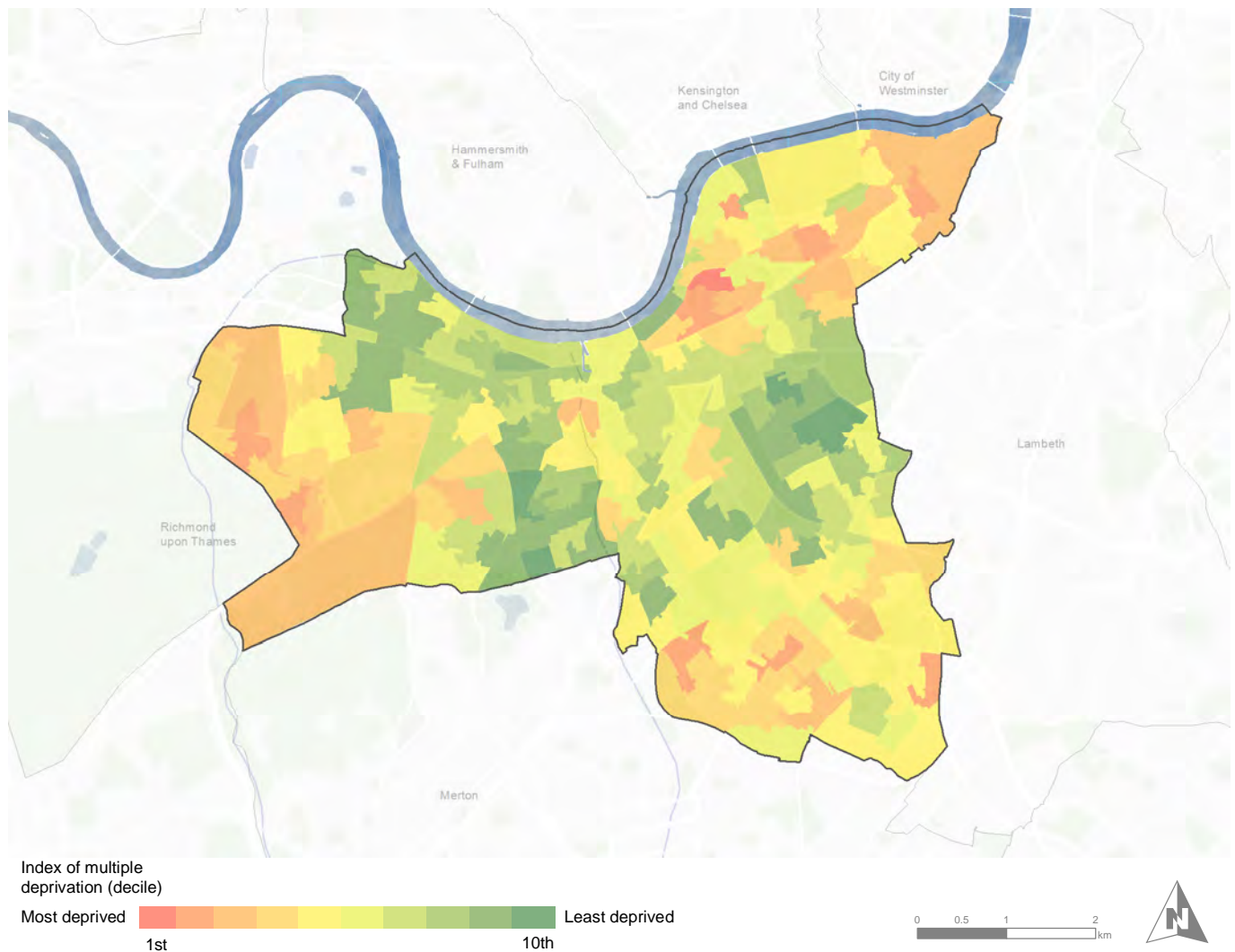
Despite this, there is a strong record of affordable housing provision in the borough with 1,307 new affordable homes delivered between 2014 and 2017. Social housing accounts for 19% of property. However, there remains a need for affordable housing as property to rent or buy in Wandsworth is not only unaffordable but also in high demand.

In some of the more densely populated areas of Battersea as well as in some of the more deprived areas of Roehampton and Queenstown, there is an abundance of both social housing estates and established terraced streets. In Wandsworth there are around 40 associations and between them they own over 9,000 homes in the borough.

In terms of housing type, a large proportion across the borough (around 90%) are terraced, flats, maisonettes or apartments. Housing estates cover only 10% of the area of the borough.

(Source, [Office for National Statistics](#), accessed October 2020).

## Index of multiple deprivation



**Fig. 40:** Index of multiple deprivation

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Wandsworth is now within the 50% least deprived local authorities in England, whereas in 2015, it was amongst the 50% most deprived. The most deprived areas in the borough are located in Putney Vale, Battersea (Winstanley, Doddington, Rollo, Savona and Patmore Estates) and parts of Tooting. However, there are no output areas in Wandsworth that are in the top 10% most deprived in England. A minority of output areas are in the top 10-20% or 20-50% of areas.

Wandsworth ranks amongst the least deprived authorities in England for four of the seven deprivation domains (Income; Employment; Education, Skills & Training and Health Deprivation & Disability) - it has a particularly high ranking for Education, Skills & Training and, compared to 2015, its rank and score for Income have improved.

In line with many other London boroughs, Wandsworth ranks amongst the more deprived authorities in England for the Barriers to Housing & Services and Crime domains.

Wandsworth ranks amongst the least deprived in London for five of seven deprivation domains (Income; Employment; Education, Skills & Training; Barriers to Housing & Services and Crime). Between 2015 and 2019, Wandsworth moved from the 50% least deprived to the 50% most deprived London boroughs in the Health Deprivation & Disability domain. Wandsworth's most deprived measures were the Living Environment and Income Deprivation Affecting Older People measure both regionally and nationally.

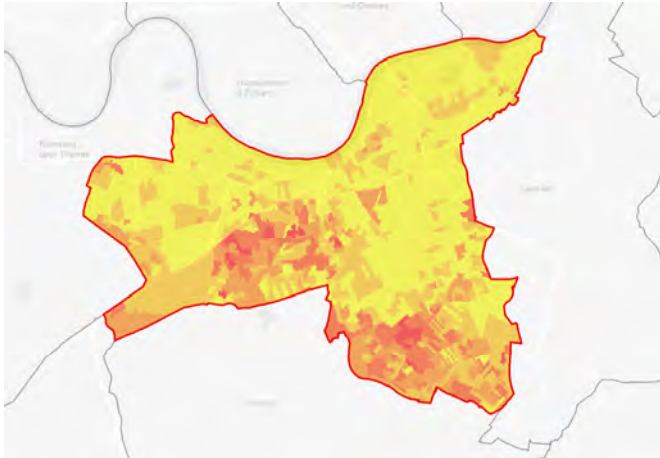


Fig. 41: Ethnicity - % Asian

The Asian communities in Wandsworth are concentrated in Earlsfield, Southfields and Tooting. 10.9% of the population in Wandsworth is Asian or Asian British.

The percentage of Asian or Asian British people in Wandsworth has increased in the last two decades from 7.8% in the 2001 census.

Asian communities in Wandsworth tend to live in areas of slightly greater deprivation in the borough although are concentrated in two main clusters and not so readily dispersed within the wider community.

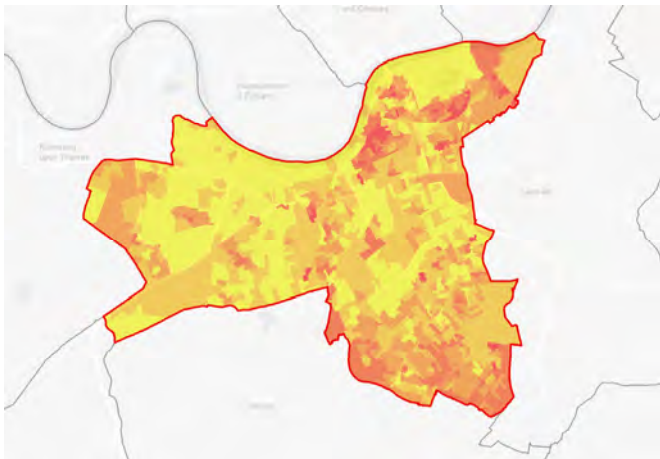


Fig. 42: Ethnicity - % black

The percentage of the population of Wandsworth that are of Black or Black British ethnicity is similar to that of Asian or Asian British at 10.7%.

The population of Black or Black British people has similarly increased since the 2001 Census, when Black or Black British people made up 9.6% of the total population.

The Black or Black British population is slightly more dispersed among the wider community than the Asian community, although there is a particular concentration around Tooting, as with the Asian communities.

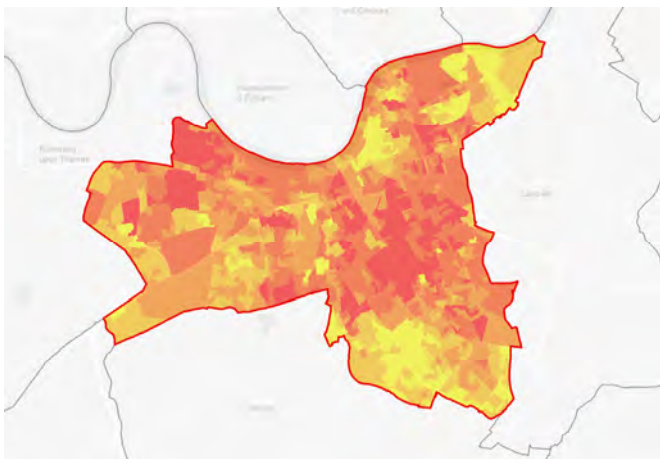


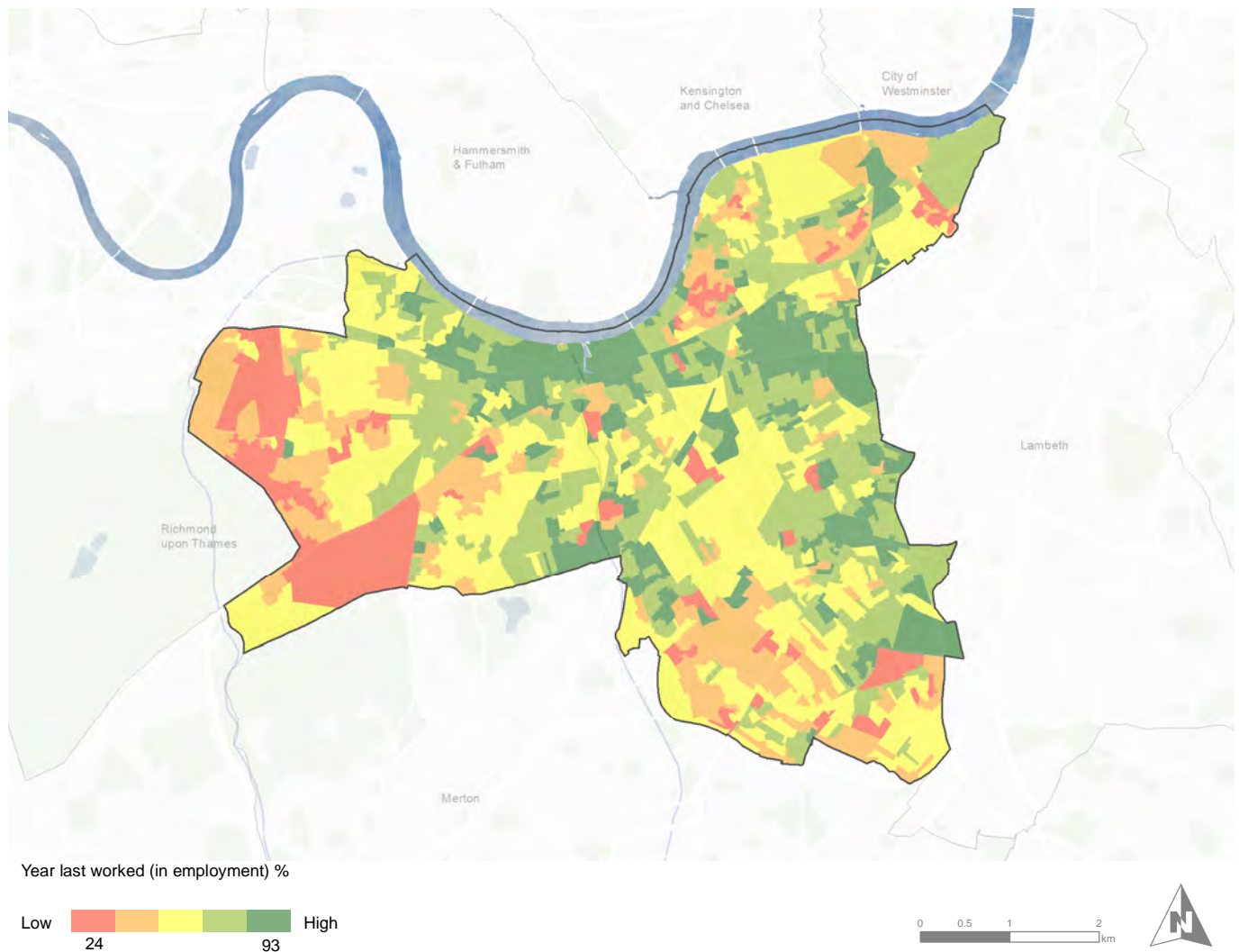
Fig. 43: Ethnicity - % white

70.3% of the population of Wandsworth is of White ethnicity and the distribution of predominantly White communities mirrors the areas of Wandsworth that are less deprived.

Wandsworth remains a predominantly White area, however, the ethnic diversity of the borough has increased in the last couple of decades. In the 2001 Census the proportion of the total population that were White was 78%.

The White community is relatively well dispersed across the whole community, although is lower in Tooting and Battersea.

## Employment



**Fig. 44: Employment rates**  
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The borough generally benefits from relatively high levels of employment and areas of high employment rate are relatively well spread across the borough. Areas of particularly high employment rate include Wandsworth Town, Balham and areas around Battersea.

Around 85.5% of the population of Wandsworth are economically active (16-64) and the economic activity rate is slightly higher for men than it is for women. Wandsworth's employment rate is 83.1% which is higher than the London average (75.1%) and the England average (76.2%). Wandsworth also has a higher self employment rate (14.4%) than London (13.2%) and England (11.1%).

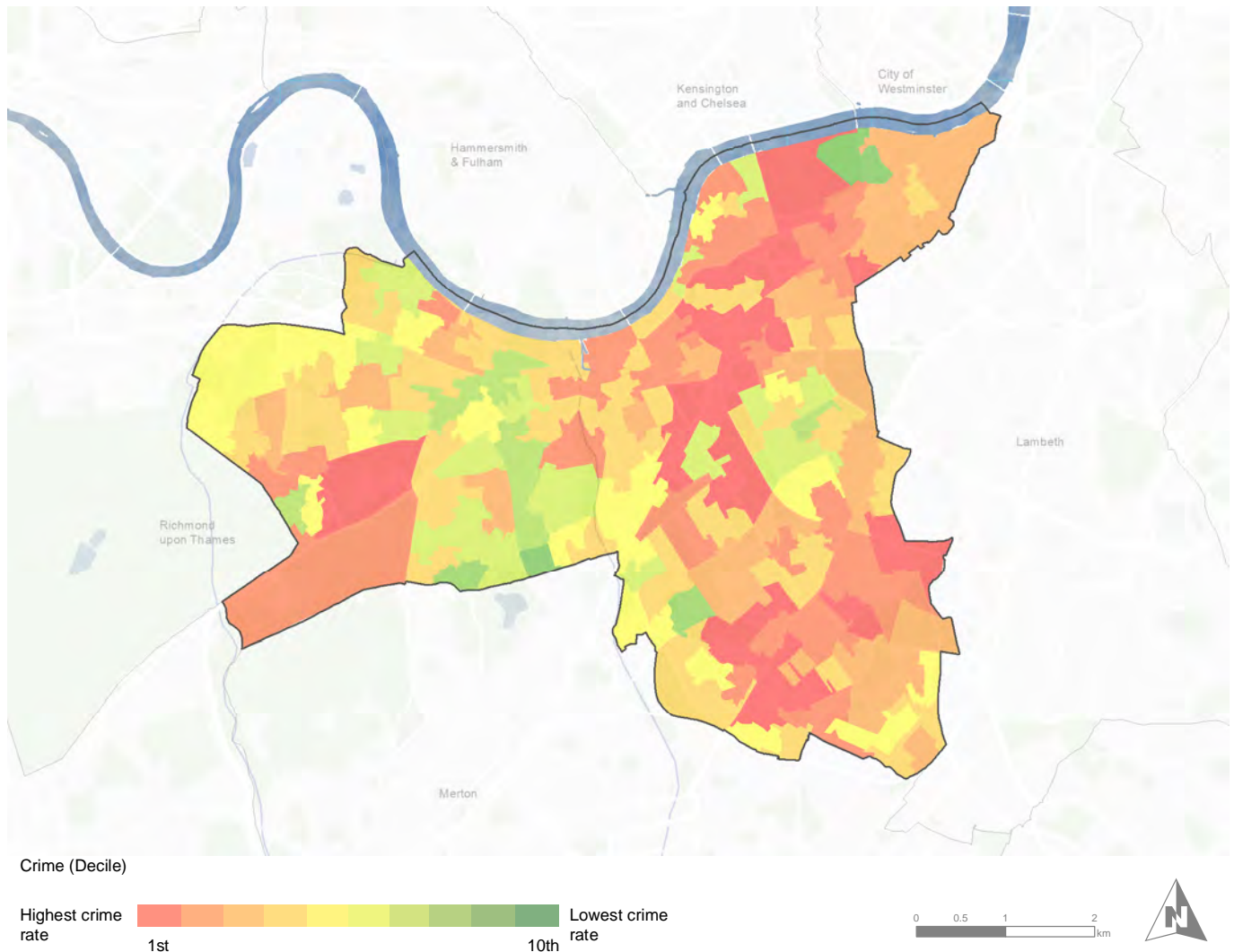
People employed in Wandsworth predominantly work in either professional occupations; associate, professional and technical occupations; and as

managers, directors and senior officials. The sector for which Wandsworth contributes the highest proportion of gross value added is real estate.

The average annual salary in Wandsworth (£39,077) is significantly greater than the London (£31,114) and England (£25,118) averages. The number of economically inactive in the borough (2.8%) is significantly below the London (4.8%) and England (4%) averages.

However, these higher average incomes are not spread consistently throughout the borough. In the more deprived areas of Putney Vale and Roehampton, employment rates are comparatively lower than they are in the less deprived areas of the borough.

## Crime



**Fig. 45: Crime levels**

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Wandsworth is considered the safest inner London borough in terms of notifiable crimes per person. The borough also has the lowest rate of violence with injury and knife crime of the 12 inner London boroughs.

The average crime rate per 1,000 population in Wandsworth in 2019 (6.5) was significantly lower than that of London (8.4) as well as being below the English average (7.0). The most common crime was antisocial behaviour, in line with London and England trends, followed by violent offences.

Over half of the crimes reported to the police were related to anti-social behaviour. However, despite being the second most common crime in the borough, incidents of serious violence had a crime rate of just 0.15 per 1,000 people.

Crime is concentrated in the more deprived areas of Wandsworth and is higher around some of the large commons. Some wards in Battersea and Tooting Bec have particularly high crime rates reflecting the more urbanised character.

Despite this, police data has shown a fall in crime with incidents of violent offences, gang-related hostility and gun crime all decreasing.

Among the lowest crime rates include offences such as possession of weapons, personal theft and shoplifting.





# Section 3

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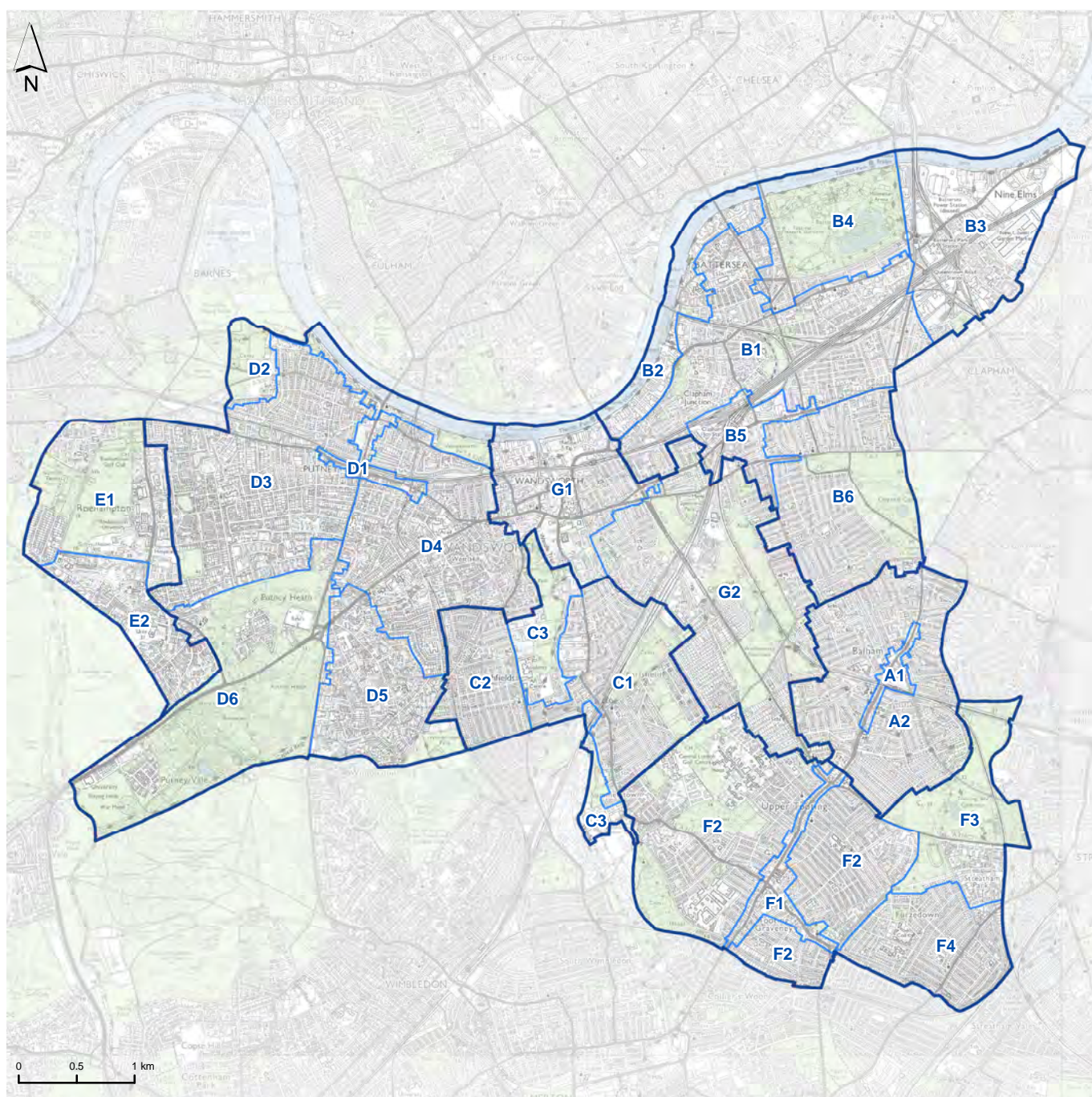
Character Areas

# Section 3

## Character areas

This section sets out the character of the borough, which has been divided into seven 'places' and 25 character areas.

The character areas are shown on [Fig. 46](#) and listed on the following page.



**Fig. 46:** Overview of character areas

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## Place

High level broad characterisation to reflect a sense of place and identify areas recognised by local people.

## Character area

A finer grain of detail to draw out the distinctive qualities important to character.

### 3.1 Structure of chapter

Each 'place' is introduced, followed by the character area profiles for the character areas within it. The profiles begin with the key characteristics of the character area, followed by valued features and negative qualities. These aspects aim to capture the key aspects of character at a high level rather than a comprehensive and detailed study.

The key existing building types in each character area are noted, with more detail on these building types contained in [Appendix E](#).

The profiles also provide an assessment of the area's sensitivity. These are high level judgements based on overall character. In reality, sensitivity will vary on a site by site basis. 'High' sensitivity does not necessarily mean no development can happen, but it highlights areas where extensive change is unlikely to be appropriate and where the design of any new development should be especially sensitive. Likewise 'low' sensitivity does not necessarily mean that development should happen, but that the character of the area may be enhanced through positive change. Each development site should be assessed on a case by case basis.

An overall strategy for the area is provided, in relation to whether, as a whole, the character of the area should be conserved, restored, improved or transformed.

The profiles end by providing design guidance for new development or planning and management, considering the area's character, valued and negative qualities and overall strategy. The design guidance provides notes relevant to the character area rather than repeating general guidance on elements such as sustainability, architecture, open space, SuDS and urban planning provided in documents such as:

- The [National Design Guide](#);
- The [Mayor Of London's Good Growth by Design initiative](#); and
- [Transport for London's Streets toolkit](#).

	Place and character area	Page number
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G1	Wandsworth Town and Riverside	158
G2	Wandsworth Common and Residential	162

# A Balham



Fig. 47: Period 2-3 storey terraces are a key feature across Balham

## *Summary*

Balham developed as a small town centre following the opening of the railway in 1856, becoming known as the 'gateway to the south'. The station stimulated development of the fields lying behind the early ribbon development along Balham Hill and Balham High Road, creating new residential estates. This development was further accentuated by the introduction of trams in 1903, and then the underground in 1926. The area is characterised by 2-3 storey Victorian and Edwardian buildings in the main, although extensive damage in WWII means there is frequent post-war infill.

The setting of the area is defined by its position between the large open spaces of Tooting Common, Clapham Common and Wandsworth Common.

## *Key changes and trends*

- Balham is an established urban centre with a significant amount of intact period housing, with little change expected.
- The Area Strategy for Balham is set out in the Local Plan. No significant change is planned, although its importance as an urban centre means that individual sites within the town centre will likely develop and transform over time, strengthening and restoring the existing character.

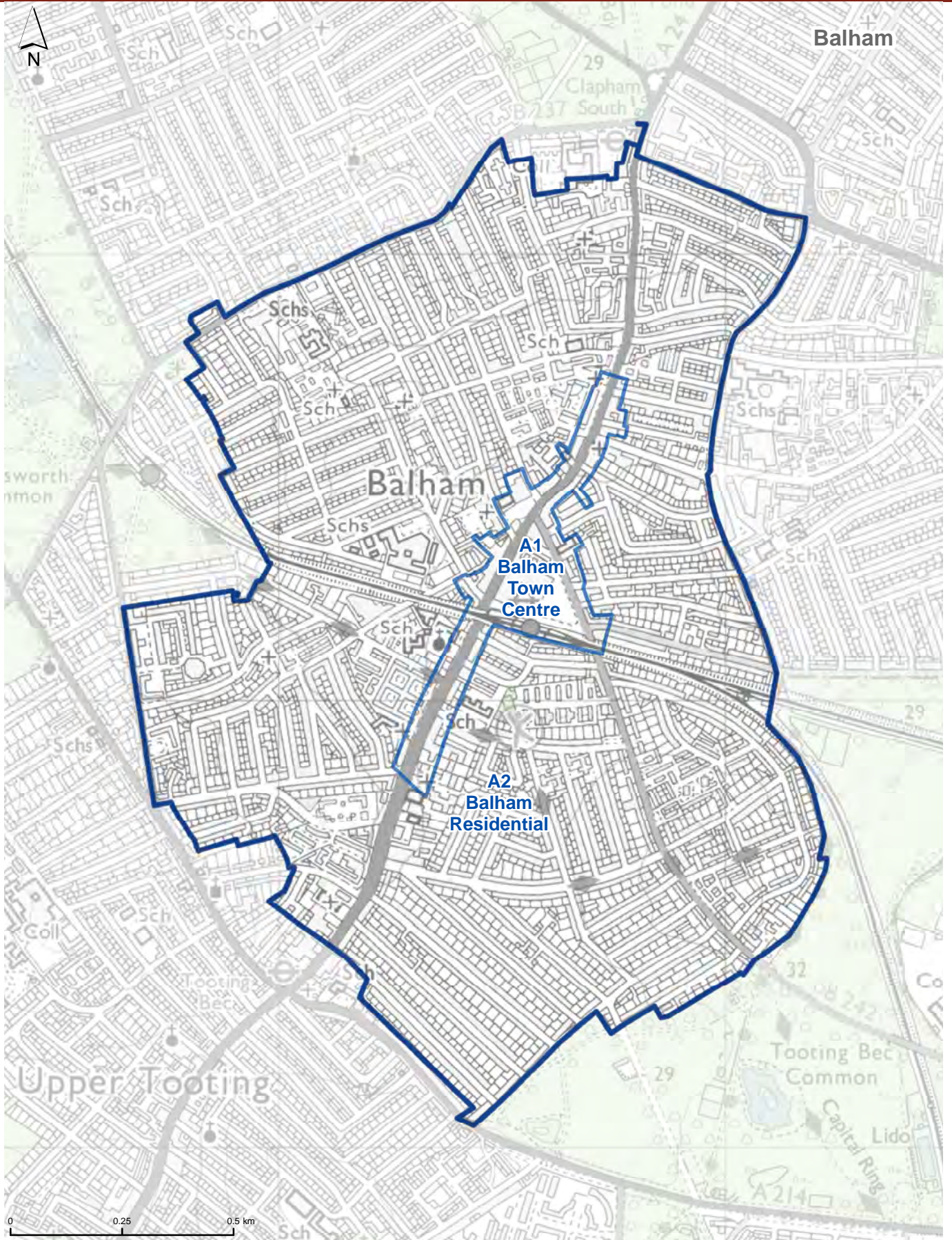


Fig. 48: Place A: Balham character areas plan

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# A1 Balham Town Centre



Fig. 49: Balham Station (grade II), designed by Charles Holden and built from Portland stone in 1926. Twin buildings straddle Balham High Road.

## Key characteristics

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Balham Town Centre is focussed along Balham High Road and includes the railway and underground station, and the northern part of Bedford Hill.

**Evident historic character. The town evolved along Balham High Road**, which is the presumed line of Stane Street, a Roman road linking Chichester to London. Balham developed as a small town centre following the opening of the railway in 1856. Balham High Road began to be redeveloped for commerce, and a tram service was introduced in 1903 along this route. The extension of the underground line in 1926 further encouraged Balham's commercial growth, with the town becoming known as the 'gateway to the south'. The magnificent Duchess Palace Theatre (at the corner of Yukon Road/Balham High Road), was emblematic of the grandeur of the area, sadly destroyed in WWII.

**A legacy of Victorian and Edwardian period 3-4 storey terraces with ground floor shops and offices or residential above.** Many of these are sited on narrow plots with red or yellow brick façades and gabled frontages to the streets. Most shopfront terraces are in good condition, but deteriorate towards Tooting Bec underground station. Generally frontages are positive, often grand, although infill buildings are sometimes unsympathetic to the period character and proportions.

**Red brick is a characteristic material.** Shop frontages generally contribute positively to the street scene, with a sense of diversity, variety and colour. There is a sense of individuality to buildings whilst maintaining an overall unity. Occasional use of ceramic tiles such as the area of public realm near the southern end of Hildreth Street, lend a sense of texture and richness.

**Positive landmarks on corner plots**, including the Bedford and Devonshire pubs. Other landmarks include:

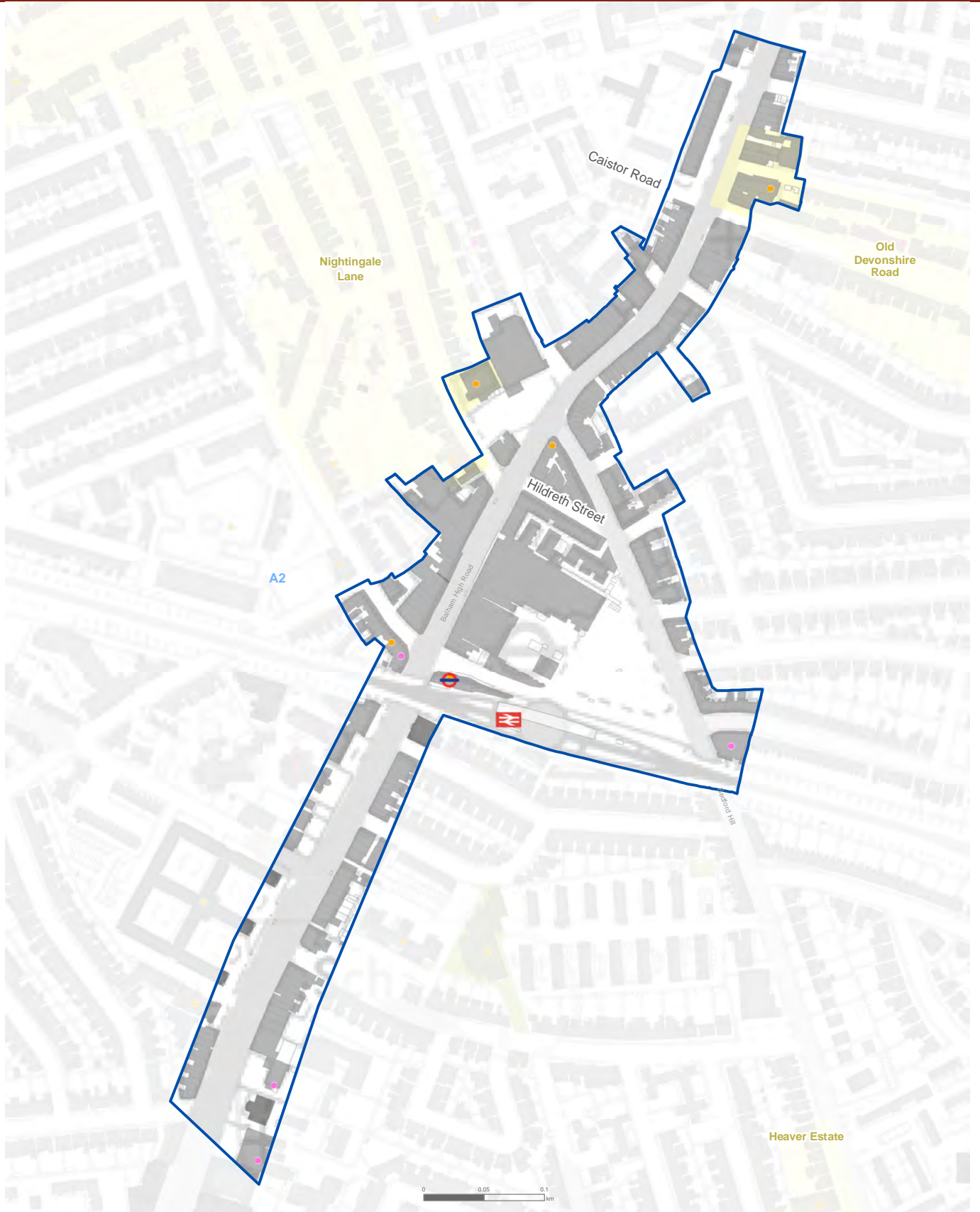
- St Mary's Church (grade II) with its brick and stone campanile and clock tower;
- Du Cane Court (locally listed), a memorable and distinctive 8 storey 1930s residential block in red brick. It once included a restaurant, bar and club. The gated entrances and gardens make a strong impact to the street;
- Polish Church of Christ the King

**Balham High Road is the focus of the town centre**, and is a relatively wide road. The prevailing building height along the main road is 4 storeys. Some buildings rise up to around 8 storeys, such as Du Cane Court, which feels in proportion to the scale of the street.

**A local destination for eating, drinking and shopping**, and a sense of vibrancy and activity. The town centre has excellent public transport accessibility. Away from the main road it feels calm. The town centre previously extended up further along Balham High Road, joining with Clapham South station, with a cinema, hotel and theatre among other commercial and leisure uses. Following bomb damage, many of the leisure buildings were redeveloped for residential use, changing the character of this part of Balham.

**An urban feel** with little green. Where street trees do exist, these are valuable in providing a sense of relief from the heavy traffic and hard streetscape. Balham High Road is busy with traffic and generally difficult to cross. The main road feels noisy and polluted, detracting from the perceptual quality of the space.

**Balham Station** is made up of two interconnected buildings, a rail station and underground station on the Northern Line. There is a memorial plaque in the entrance hall to 65 civilians killed in 1940 when the station was used as a deep shelter. Balham suffered badly from bomb damage, including extensive losses to the Victorian terraces around the High Road, Caistor Street and the area around the station.



**Fig. 50: A1 Balham Town Centre character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Intact Victorian and Edwardian 3-4 storey terraces lining the majority of Balham High Road with well preserved and maintained architectural details. The buildings create a distinct recognisable proportion to the overall street scene.
- Landmark buildings at junctions which positively address both streets (see key characteristics section).
- The historic interest provided by the grade II listed Hamilton Hall and Mansbridge House on Balham High Road, which are rare surviving early 19<sup>th</sup> century villas. Most villas along Balham High Road have since been demolished and replaced by shops. The condition and setting of these buildings could be enhanced, to improve their presence and their contribution to local character.
- Hildreth Street, a pedestrianised area with a vibrant market, seating and outdoor dining, which gives the town centre a community feel.
- The individuality of Shopfronts which add to local character whilst maintaining a sense of unity.
- Independent shops which help give the town a sense of local distinctiveness.
- The trees and vegetation on opposite sides of Balham High Road at Oldridge Road and Hanson Close, which marks the northern end of the town centre.
- Small parts of two conservation areas overlap with Balham Town Centre: Nightingale Lane Conservation Area, including the library (locally listed); and Old Devonshire Road Conservation Area.
- The whole of Balham High Road lies within an Archaeological Priority Area covering the route of the Roman road known as Stane Street.
- The proximity and ease of access to Tooting Common.



**Fig. 51:** Pedestrianised Hildreth Road, usually with a vibrant café culture and active street scene



**Fig. 52:** The Bedford pub is a strong landmark on the corner of Bedford Hill/Balham Station Road



**Fig. 53:** The Art Deco Du Cane Court forms a strong address to the street. Its gardens provide welcome green along Balham High Road

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- The town centre feels dominated by car parks, with parking for both Waitrose and Sainsbury's occupying prominent locations. These negatively affect the street scene.
- Improved maintenance needed in places - the condition of upper storeys and garish Shopfronts becomes more pronounced north of Ormeley Road, with gaps in the building line.
- Some individual negative developments, particularly residential buildings towards Tooting Bec e.g. the white and blue coloured Lumiere Court, which is a local detractor, providing a poor gateway into the town centre and from the nearby listed buildings;
- Cherwell House; and the two storey Halifax building which makes a poor contribution to its corner plot.
- The brick wall of the station leading to the Bedford pub lacks interest and activity.
- The busyness and poor air quality caused by traffic on the main road. A lack of street trees perceptually adds to the feeling of noise and pollution.
- The local parade of shops north of Caistor Road, which has poor quality frontages and plain buildings. However, the mature trees at this location are a positive feature.
- The lack of definition or a gateway to the town centre at its southern end.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Mansion blocks;
- Low-rise flats; mid-rise flats;
- Villas;
- Shopfront terraces.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

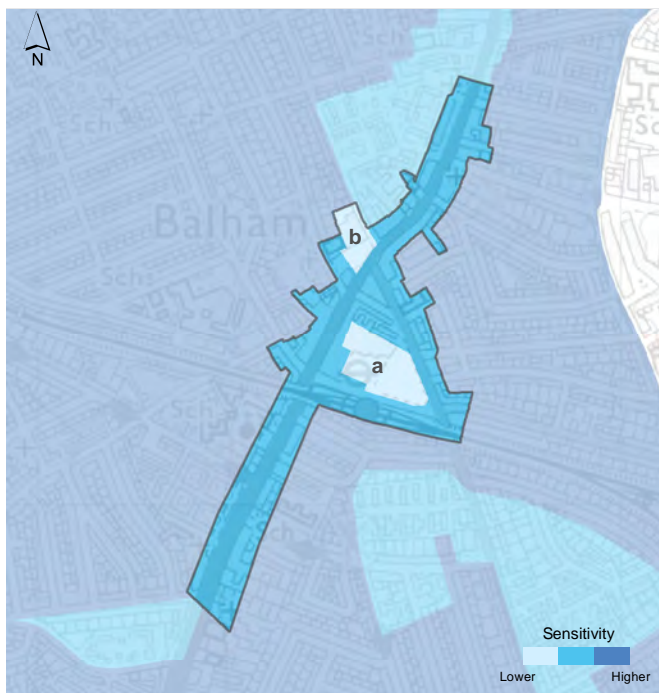


Fig. 54: A1 Balham Town Centre sensitivity plan

Overall Balham Town Centre has a medium sensitivity to change with the potential for targeted growth and redevelopment.

The character area has the potential to incorporate change and new development, as long as this respects:

- the proportion, scale and material quality of the remaining period buildings;
- the existing landmark buildings, preserving their setting and contribution to the skyline; and
- the function of the streetscapes as a shopping and leisure destination.

Due to the negative impact of the two supermarkets and car parks (Sainsbury's sub-area a and Waitrose sub-area b), these areas have a lower sensitivity to change, in particular the ground level car park site.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

<i>Conserve</i>	<i>Restore</i>	<i>Improve</i>	<i>Transform</i>
-----------------	----------------	----------------	------------------

Overall Balham Town Centre has a good sense of place, though its character is affected by negative elements in key locations - notably the supermarkets and associated car parks.

Therefore the strategy is to **conserve** and **restore** the town centre's character by encouraging town centre uses (including leisure destinations) and improving the negative elements to create a better centre.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Review supermarkets and car parking provision to create a better centre, with potential for considered development to improve the character of the town centre.
- Establish more green infrastructure, particularly street trees. Increase and improve open spaces.
- Improve and consider temporary pedestrianisation of roads to create café seating or more width to improve pedestrian experience and reduce the perceived dominance of vehicles.
- Create a stronger definition to the southern end of the town centre, distinguishing between the 'Places' of Tooting and Balham.
- Promote active frontages and ensure good maintenance of building façades along Balham High Road, particularly towards the south.
- Establish guidelines for Shopfrontages to ensure they contribute to the overall quality of the street scene. Encourage reinstatement of Shopfronts to achieve consistency in appearance.
- Protect the proportions of period buildings along Balham High Road, while recognising opportunities for landmarks with a design elegance up to 5-6 storeys at key junctions.
- Increase height of 1-2 storey buildings to tie in with existing 4 storey terraces, to create a consistent frontage along Balham High Road.
- Protect the setting and proportions of existing landmark buildings.
- Expand arts and culture, building on Balham's past as a destination for theatre and cinema.

## A2 Balham Residential



**Fig. 55:** Consistent red brick terraced houses in Dinsmore Road Conservation Area with modern infill developments in the background of the view

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Balham Residential encompasses the mixed residential streets surrounding Balham Town Centre. There is a gradual transition to the surrounding commons and residential areas of Battersea, Wandsworth and Tooting.

**The area developed rapidly in the late 19<sup>th</sup> century after construction of the railways made the area more accessible.** It grew as a lively, fashionable high-class suburb, close to the rapidly expanding rail network and within easy reach of the city. Large villas in spacious plots (e.g. Nightingale Lane) reflected the wealth of the area. Smaller terraces were built in more densely packed roads closer to the town centre.

**Residential character, with 2-3 storey buildings of yellow stock and red brick.** The overriding character is of attractive terraced Victorian/Edwardian houses arranged on regular grid patterned streets. Intact boundary walls and green hedges create a safe and well maintained perception. The most coherent are Nightingale Lane, Dinsmore Rd, Old Devonshire Rd, Culverdon Rd and Heaver Estate conservation areas. **Character becomes quiet and residential quickly away from the town centre,** although some roads are busy with through traffic.

**Individual buildings of high quality design and materials** add significantly to the street scene. A rich variety of architectural styles combine harmoniously.

The consistent warm red brick of houses in the Heaver Estate add richness. Important groups of houses are locally distinctive, notably grand 4-5 storey villas on Nightingale Lane ([Fig. 59](#)), Sarsfield Rd, grand Georgian villas on Cavendish Road and mansions on Upper Tooting Park. Rich details include bay windows, iron gates and cobbles at Dinsmore Rd, the historic sign on Alderbrook Rd and features such cobbles and traditional lamp posts at Western Lane ([Fig. 57](#)).

**Balanced proportions:** where buildings are taller they are also grander with broad frontages, and front the wider main roads, such as Balham High Road, Bedford Hill, Ritherdon Road, Cavendish Road, Upper Tooting Park, Balham Park Road and Nightingale Lane.

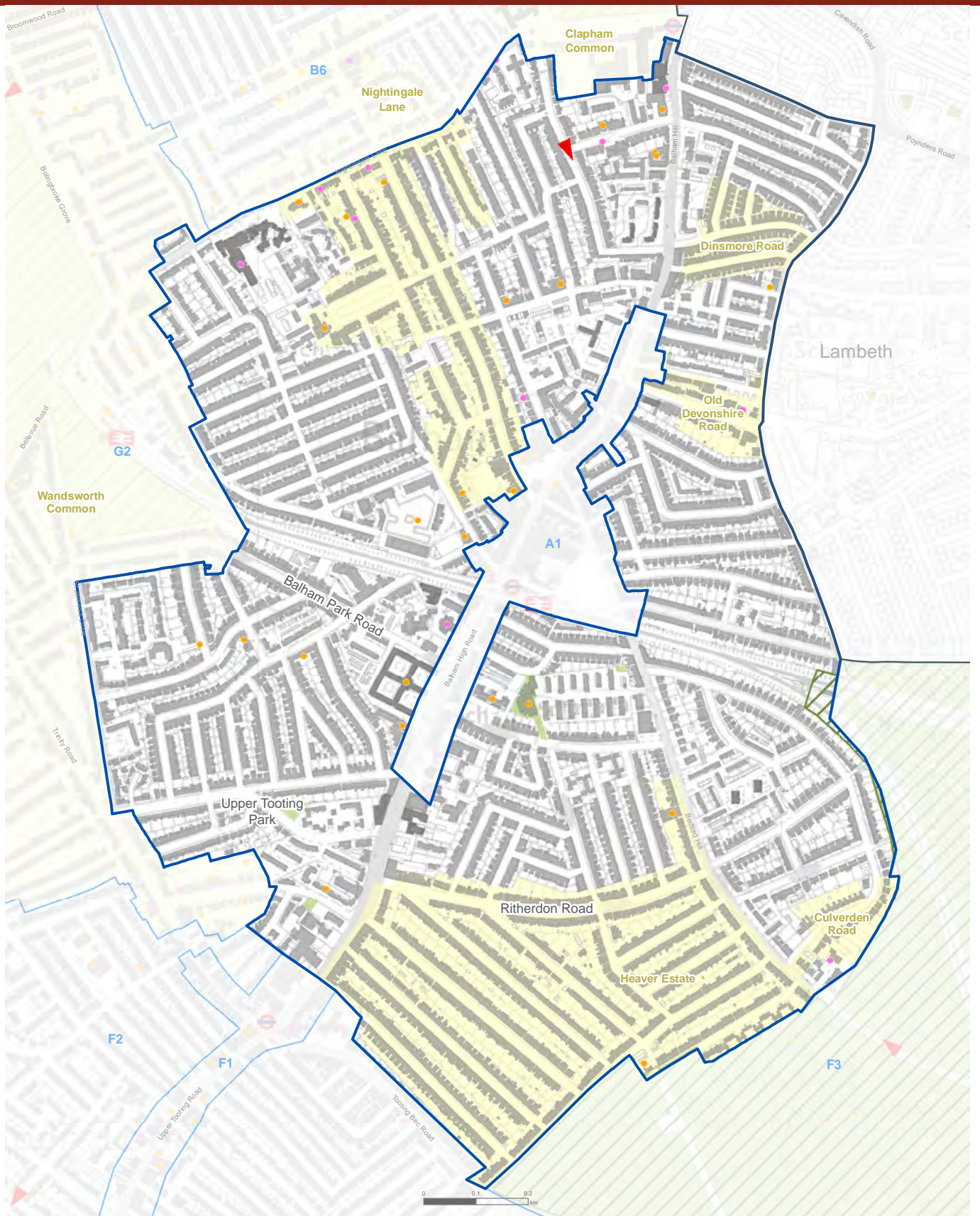
**War damaged sites were redeveloped with insensitive building designs, resulting in pockets of post-war infill** creating a patchwork of irregular character. There was also loss of features such as cast iron gates and railings, taken down during the war. Many mature street trees have been lost creating a fragile character overall, which, if further deteriorated through unsympathetic development, could result in complete change of character for the area. However, there is an opportunity to enhance the character of the area, with new characteristic developments. Post-war buildings are often low density, with opportunities to intensify residences while improving character.

**Community facilities** contribute to a lively neighbourhood with schools, churches, nurseries, a bowling green, parades of shops along Nightingale Lane and in Clapham South. There is good transport accessibility, with nearby stations at Balham, Clapham South, Tooting Bec and Wandsworth Common.

Few green open spaces in the character area itself, but the **nearby Clapham, Tooting and Wandsworth Commons** influence the area through mature trees. Nightingale Square represents the only true garden square in the borough. Hilly topography creates tree-lined vistas along some streets, which, along with green front gardens, softens the urban realm. Five storey buildings form prominent landmarks when looking up or downhill along streets. Landmarks of note include:

- Grade II\* Church of St Luke on Ramsden Rd, with its distinctive red brick tower and copper spire;
- 69-79 Nightingale Lane (grade II listed);
- Alderbrook Primary School;
- Ramsden Hall;
- The Priory (grade II listed);
- Du Cane Court, 8 storeys.





**Fig. 56: A2 Balham Residential character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Mature tree-lined streets, well-planted front gardens and intact front boundaries provide a welcome relief from the surrounding urban streets and contribute to a sense of calm and quiet.
- The heritage value and character of the townscape and individual buildings within conservation areas (and outside, where of similar character). Historic details e.g. Dinsmore Rd, Western Lane and the hanging sign on Alderbrook Rd signposting the Hillgate Place Employment area.
- Locally distinctive, high quality buildings, and attractive period houses forming coherent streets. There is a particular concentration of distinctive listed buildings along Nightingale Lane.
- Wandsworth, Tooting and Clapham Commons in close proximity, provide important open space and sense of green. The areas surrounding the open spaces form an important part of their settings. Street

trees help to extend their character into Balham Residential.

- Playgrounds and small spaces near Northcote Rd and the Commons provide facilities but also a place to gather and contribute to a sense community spirit.
- High quality materials including warm red brick which adds a sense of place and rich texture.
- Views along well-balanced and tree-lined streets punctuated by landmarks e.g. down Alderbrook Rd to Alderbrook Primary School; and from Tooting Commons to the Priory (grade II house).
- The residential character of the area between Balham Town Centre and Tooting Town Centre which provides an important sense of separation.
- Proximity and access to high street amenities and Tooting Commons with facilities such as the lido.
- Nightingale Square (locally listed), the only true garden square represented in the borough.



**Fig. 57:** Quiet, leafy character of Western Lane in Nightingale Lane Conservation Area



**Fig. 58:** A school tower forms a local landmark, viewed downhill Alderbrook Rd



**Fig. 59:** Landmark villas 69-79 Nightingale Lane (grade II listed)

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Post-war and modern infill developments are often not in keeping with the surrounding character e.g. Harberson Rd, Fontenoy Road, Boundaries Rd, and 1980s houses south of Hillbury Road - the scale (narrow plots), off-street parking and design detracts from the Heaver Estate Conservation Area.
- Removal of front boundaries for parking serves to fragment the consistency of the area.
- Tower blocks and low density 2 storey estate development around Little Dimocks/Many Gates.
- Loss of historic features e.g. cast iron railings
- Alterations to houses, e.g. in the Heaver Estate Conservation Area, have degraded character with

poor garden and boundary maintenance, additions to frontages such as utility cabinets, replacement of timber sash windows and front doors, and introduction of bin stores.

- Loss of mature street trees.
- Parking over forecourts, especially prevalent at commercial premises.
- Wimbourne House on Bedford Hill, negatively affects the setting of grade II Priory in Culverdon Road Conservation Area.
- Confused character and land uses at Balham High Road between Balham/Tooting, with poorly defined frontages to Balham High Road.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Semi-detached;
- Villas;
- Mansion blocks;
- Low-rise estates; mid-rise estates;
- Low-rise flats; mid-rise flats.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

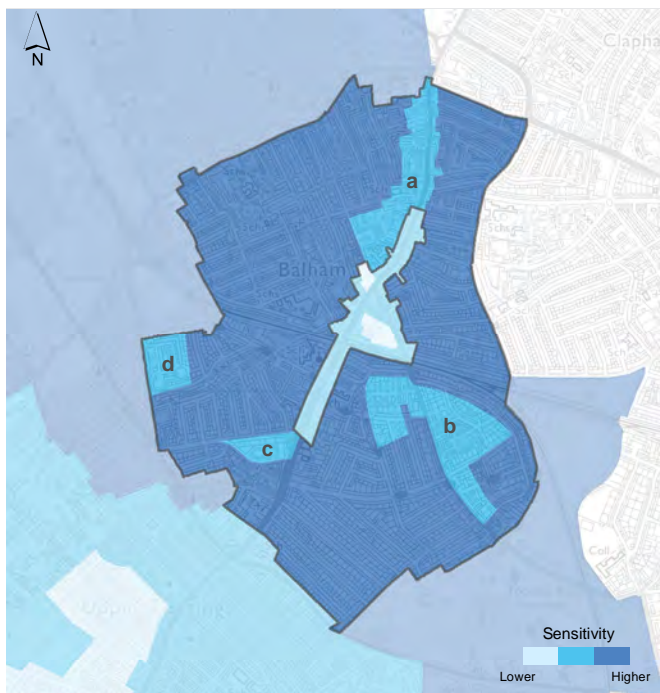


Fig. 60: A2 Balham Residential sensitivity plan

Overall, Balham Residential has a high sensitivity to change owing to its areas of high value and the potential for further change to fragment character. There may be opportunity for growth in targeted areas.

Areas of relatively lower sensitivity include: many of the post-war residential infill areas which are unsympathetic to surrounding context, such as Balham High Road/Clapham South sub area (a); Bedford Hill sub area (b); Marius Road sub area (c); and Sarsfield Road sub area (d). Sub areas (a) and (c) also reflect the opportunity for enhancement of sense of place along Balham High Road and gateways to Clapham South and Tooting respectively.

Any new development should reflect the character of the wider Balham Residential character area.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve

**Restore**

Improve

Transform

Balham Residential has a strong sense of place, although its character is fragmented by pockets of unsympathetic, mainly post-war infill development.

Therefore the strategy is to **restore** the character of the area, retaining, enhancing and reinstating historic character, and ensuring new development respects the prevailing pattern and positive key characteristics.

## Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Encourage upkeep of boundaries, façades and front gardens. Encourage planting in front gardens, including features such as traditional privet hedges (in Heaver Estate Conservation Area).
- Restrict parking over forecourts.
- Increase and maintain street tree planting: many of the streets would have had avenues of trees.
- Enhance Balham High Road between Balham and Tooting town centres to improve the sense of separation, by setting buildings back and creating a positive frontage to the main road through trees and gateway features to the town centre e.g. art/sculpture at the new development on Upper Tooting Park.
- Respect the scale and proportions of existing buildings and streets. Most streets would not be able to accommodate buildings exceeding the prevailing height of 2/3 storeys; however, wider streets and key junctions may be able to accommodate taller buildings; mansion blocks work particularly well in this area.
- Conserve, repair or reinstate original period features such as windows, boundary walls, traditional materials and front doors.
- Retain existing local parades and local community facilities.
- Respect the settings, proportions, scale and details of landmark features.
- Encourage use of characteristic materials such as high quality red brick and London stock brick.

# B Battersea



Fig. 61: Battersea's mixed riverside frontage illustrating new and old landmarks in close proximity

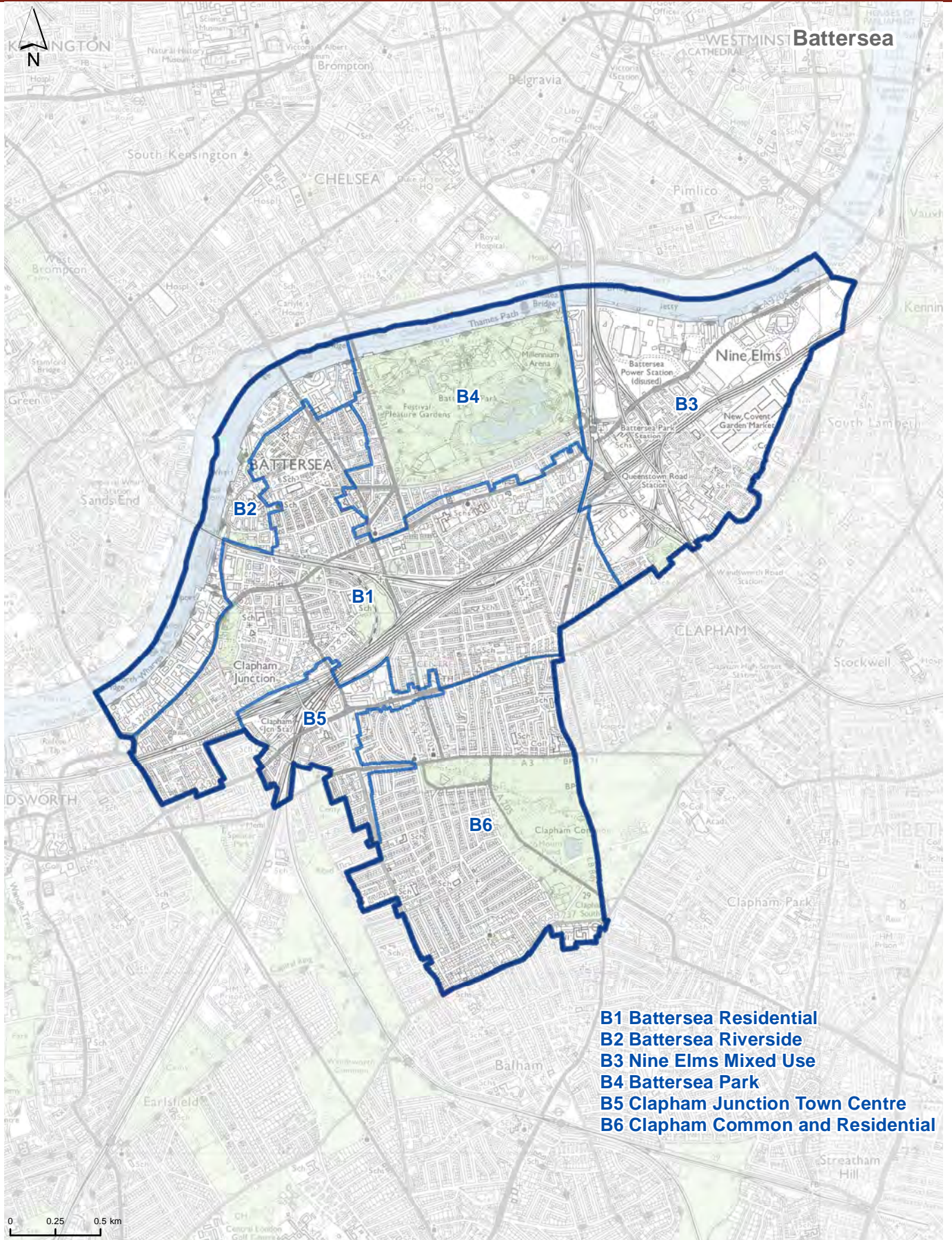
## *Summary*

Before the industrial revolution much of the Battersea area was farmland known as 'Battersea Fields'. The flat, fertile soils of the Thames floodplain were cultivated for market gardening including asparagus (sold as 'Battersea Bundles'). The area stretches along the River Thames, with the 83ha listed Battersea Park at its centre. Battersea is now also home to one of the largest regeneration projects in the country - focused around the grade II\* listed Battersea Power Station. The character, which includes the town centre of Clapham Junction and the area around Clapham Common, is typified by a Victorian and Edwardian townscape with a large number of important listed buildings.

The borough of Battersea was formed in 1900 before it was included in Wandsworth in 1965.

## *Key changes and trends*

- Extensive regeneration of the area around Battersea Power Station for mixed use development, a new mixed use area and public realm as a Central Activity Zone cluster, and a new Battersea Design and Technology Quarter. Growth and change in the area is underpinned by a two station extension to the Northern Line.
- Clapham Junction is a major development area for the borough.
- Regeneration of the York and Winstanley Estates north of Clapham Junction.
- A proposed new foot/cycle bridge across the Thames adjacent to the Cremorne Bridge.
- Crossrail 2, anticipated to begin running, largely through tunnel, from 2030, including a station at Clapham Junction.



**Fig. 62: Place B: Battersea character areas plan**

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# B1 Battersea Residential



**Fig. 63:** York Gardens is a valued open space. Trees help to soften the hard appearance of surrounding tall buildings

## Key characteristics

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Battersea Residential encompasses the area north of the railway, typified by mid-20<sup>th</sup> century public housing estates interspersed with Victorian terraced houses and modern housing blocks.

**A history of building and renewal and of social housing, dating back to 1903.** The current landscape owes much to the extensive damage inflicted during WWII and the subsequent programme of regeneration. In the early 1950s Battersea was a poor, white, working class area, with overcrowding and poor living conditions. The social housing regeneration, focused on improving living standards, began in 1955, rooted in the Modernist ideals of planned and efficient cities. High rise, high density developments built during the 1960s were subsidised by central government. The history of social housing goes back to the Latchmere Estate CA built in 1903, which was the first estate directly built by a council. A new masterplan for the Winstanley and York Road area will create, new linkages, a new park and community buildings.

**Relationship with the river:** historically many people were employed in riverside industries. Before the industrial revolution much of the Battersea area was fertile farmland, providing food for the city of London. The railways radically altered the landscape of Battersea first arriving in 1838. There was a major peak in house building with the population rising from 6,000 in 1840 to 168,000 by 1910.

**Historic estates of mixed heights.** Winstanley, Surrey Lane, and Doddington and Rollo form large estates highlighted text. Main text. The area is a mixture of 5 and 9 storey blocks, 11-20 storey towers and 2-3 storey houses. The Kambala Estate is a modest scheme of low-rise brick-built houses and flats.

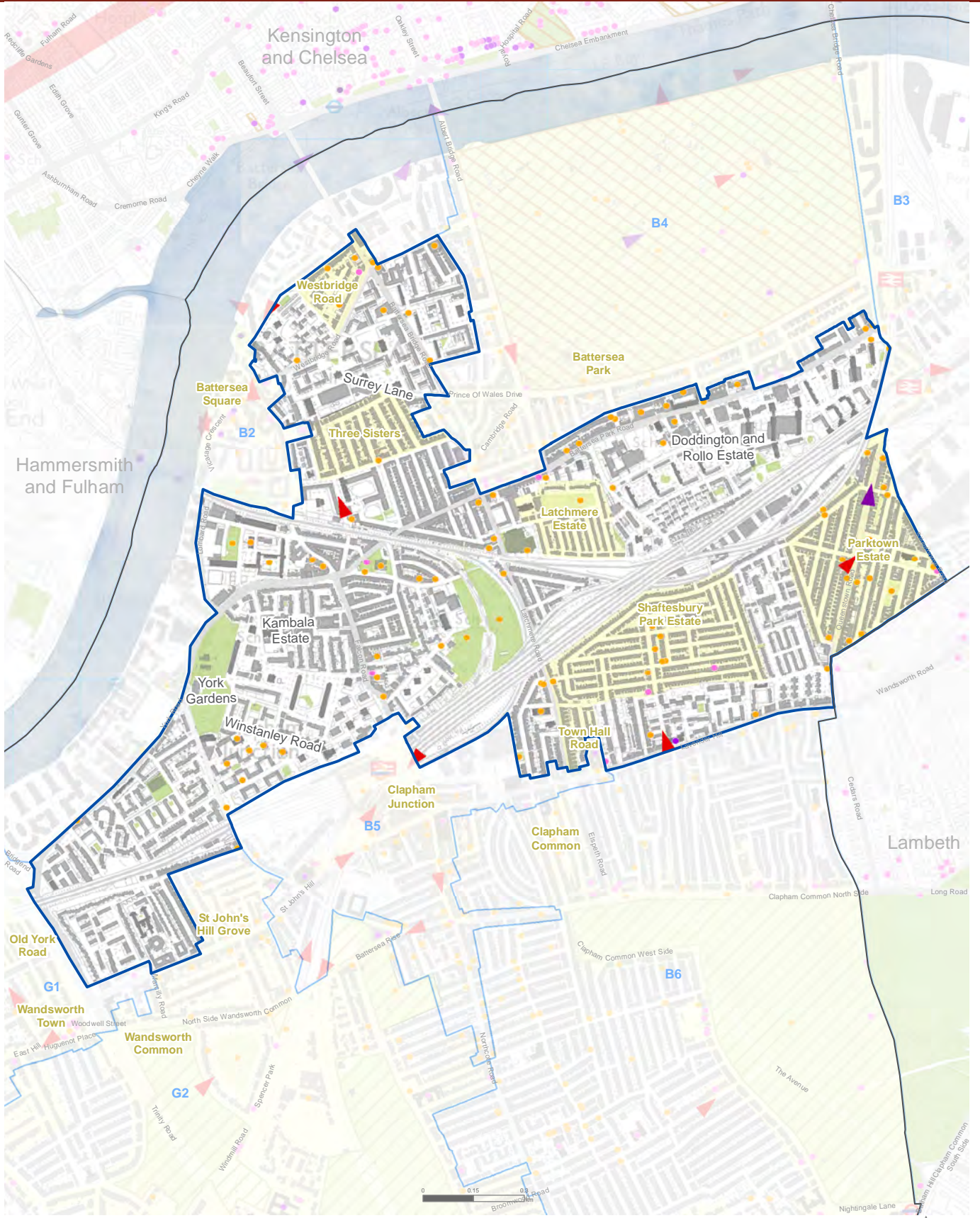
**Community facilities** were an important aspect of the original ambition of the estates. York Gardens was created in 1972. Formerly home to an adventure playground, youth group and carnivals, the open space created a strong sense of community in a diverse and densely populated area. Sporle Court tower (23 storeys) had a rooftop dance floor and play areas on the roof and 10<sup>th</sup> floor. Concrete relief murals were commissioned for the buildings, referencing their construction and relationship to industry along the Thames. Battersea Market and local shopping facilities reinforced the sense of community, now largely lost.

**A landscape of social problems in recent years.** In later years, the estates became associated with poverty and a reputation for crime. The social issues were epitomised by the London riots in 2011. The area contains some of the highest levels of deprivation in the borough.

**The estates are inward-looking and there is a general lack of connectivity to the surrounding areas.** Minimal activities within the estates and few active street frontages means they have a lack of natural surveillance and a reduced sense of community. North of Clapham Junction there are poor linkages between the Winstanley Estate and the town centre, with the railway presenting a major barrier.

**Pockets of remnant period 2-3 storey terraced housing** built from distinctive yellow London stock brick with red band courses, such as the Latchmere Estate, Shaftesbury Park Estate, Three Sisters, Westbridge Road, Park Town Estate conservation areas in addition to along Battersea High Street. Details such as doorway canopies add architectural richness to the street scene. The uniformity of the shape and proportions of the streets and buildings contrast with the open form of the estates. Street trees, green front gardens and planted boundaries soften the environment with a relative sense of calm.

**Battersea Park Road local centre** and nearby Battersea High Street provide shops and community services. Period terraced Shopfronts are punctuated by landmarks, providing a sense of history and texture.



**Fig. 64: B1 Battersea Residential character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Battersea High Street, with consistent proportions and character features of terraced Shopfronts and historic features such as lamp posts and granite setts.
- The concrete relief murals in the Winstanley, Badric Court Estate and Totteridge House.
- The public open spaces and other incidental green space, including Harroway Road open space, York Gardens and Latchmere Recreation Ground, which provide important focal points of greenery and biodiversity. These public spaces provide living histories for people still living on the estates.
- The Citizens of Battersea War Memorial, Christchurch Gardens (grade II listed).
- Latchmere Estate Conservation Area, with uniform terraces and simple, complementary architecture.
- Three Sisters Conservation Area, its special interest deriving from the locally rare pairs of semi-detached and detached houses of the 1860s-70s.
- The remnant terraces in Westbridge Road CA.
- Valued views in the Local Views SPD including views of the spire of the grade II listed Church of the Sacred Heart, towards and from the Shaftesbury Park Estate CA, to St Philip Church and its square and from Queenstown Road to Battersea Power Station.
- The consistency of the 3-4 storey period shopfront terraces with landmarks along Battersea Park Road.
- The setting of Battersea Old Town (within Battersea Riverside) and Battersea Park character areas, and the proximity of the river.
- Locally listed landmark buildings along the southern side of Battersea Park Road include The Latchmere and Theatre, The Old Imperial Laundry and the grade II listed Byron Villa Shakspeare Villa.
- St Philip Square, with the church as its centrepiece, is an important focal point.
- A overall sense of low rise housing and quiet, tree lined streets.



Fig. 65: A 21 storey tower in the Surrey Lane Estate



Fig. 66: A traditional mid-Victorian terrace on Octavia St in Three Sisters Conservation Area



Fig. 67: Low density 3 storey houses to the south of York Road.

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Poor maintenance of buildings and public realm, with narrow footways and few active frontages.
- Lack of distinctive architecture and landmarks.
- St Peter's Church has eclectic colour and material styles which detract from the street scene.
- Some areas are low density and do not address the streets - e.g. South of York Road.
- Many areas, particularly the public estates, lack a sense of place with few distinctive architectural features and many of the buildings set back from the street. The estates are inward-looking with a lack of wider connectivity.
- A perceived lack of safety within the estates due to dead ends, vandalism and little natural surveillance.
- Castlemaine House and Park South towers south of Battersea Park Road - are visual detractors due to their height and contrast with the prevailing scale.
- Tesco, car parking and the tall blocks in the Doddington & Rollo Estate south of Battersea Park Road address the street poorly and visually detract from the character of the adjacent Battersea Park CA.
- Infill developments, particularly the plain post-war housing (e.g. Brassey Square in the Shaftesbury Park Estate) create a patchwork effect and detract from the consistency of the area.



## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Low-rise estates; mid-rise estates
- High-rise estates;
- High-rise flats.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

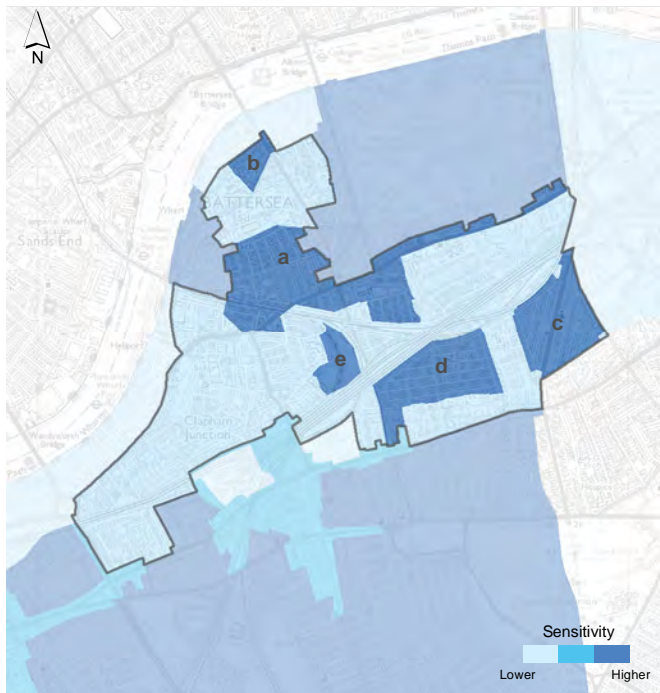


Fig. 68: B1 Battersea Residential sensitivity plan

Overall, Battersea Residential has a low sensitivity to change, with the potential for positive change which enhances the character of the area.

However, the following areas and elements are of high sensitivity and any change should respect their values and susceptible characteristics:

- on and around Battersea Park Road and Battersea High Street, sub area (a) due to the coherence and scale of the 3-4 period Shopfront terraces;
- the historic value and uniformity of the **conservation areas** and their settings, sub areas (b) Westbridge Road CA, (c) Parktown Estate CA, and (d) Shaftesbury Park Estate CA;
- **public open spaces**, which are relatively rare in the area, sub areas (e) Falcon Park / Shillington Gardens; and
- the uniformity of the **terraced streets and the Shopfront terraces**.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve

Restore

**Improve**

Transform

Overall Battersea Residential has a fragmented character and therefore the strategy is to improve its overall quality and coherence. Ideally, masterplans for large parts of the character area will provide a framework for further development in a holistic way.

## Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Incorporate focal points and a movement strategy across the area, while addressing the poor urban design of existing estates.
- Create new distinctive landmarks with a design integrity which improve legibility, focused along main roads and at key junctions. Surrounding public realm should be proportionately sized so the height does not overwhelm the human scale.
- Improve the legibility, connectivity and sense of place of the public estates. Improve natural surveillance, address to the street, architectural quality and provision of green space in existing and new developments.
- Reduce the dominance of vehicle traffic at the York Road/Falcon Road junction, while revealing vistas to the green space and listed war memorial in Christchurch Gardens.
- Enhance the appearance of Battersea High Street, and increase provision of local services for local communities. Extend the importance of Battersea High Street as a local centre.
- Improve and restore shop terrace frontages on Battersea Park Road and Battersea High Street.
- Enhance safety and quality of Shillington Gardens/Falcon Park, promoting activities, active frontages and use of railway arches.
- Increase street tree planting.
- Respect the settings, proportions, scale and details of existing landmark buildings.

## B2 Battersea Riverside



Fig. 69: St Mary's Church nestled among competing landmark buildings on Battersea Riverside including towers in Battersea Residential

### Key characteristics

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Battersea Riverside follows the River Thames from Wandsworth Bridge to Battersea Park. It includes the Battersea Square Conservation Area: the historic settlement of Battersea. The riverside has been the focus for residential and mixed use redevelopment on former industrial sites.

**The settlement of Battersea has its origins in a Saxon and medieval village, the surviving elements of which are designated as Battersea Square Conservation Area**, with the square its focal point. Once an isolated village, the construction of a bridge over the Thames in 1772 opened it up. The subsequent growth of villas as residences for city merchants made Battersea a small but thriving community. Until the late 19<sup>th</sup> century, much of the rest of Battersea was used for market gardening on former marshy land. Following construction of the railway from the 1860s the area grew rapidly, with both industrial and residential uses.

**Industry along the river once dominated the area's character**, employing local residents, and creating busy wharves with thick smoke and smells. Former manor houses were demolished to make way for industry such as the medieval York Place at the site of Price's factory. Manufacturing included mills, maltings, Wandsworth Distillery, a sugar factory and Price's Candle Factory - now the locally listed Candlemakers Apartments.

**Mixture of uses** including commercial, office, industry and residential flats. There are few leisure or entertainment destinations, and much of the area feels 'private'. Some historic industrial buildings have been sensitively re-purposed, such as the Royal Academy of Dance (4 storeys) which occupies a former warehouse. Other sites have been fully redeveloped. There is an **absence of activity or vibrancy** along much of the riverside, except for around St Mary's churchyard open space. The area ranks relatively poorly in terms of public transport accessibility.

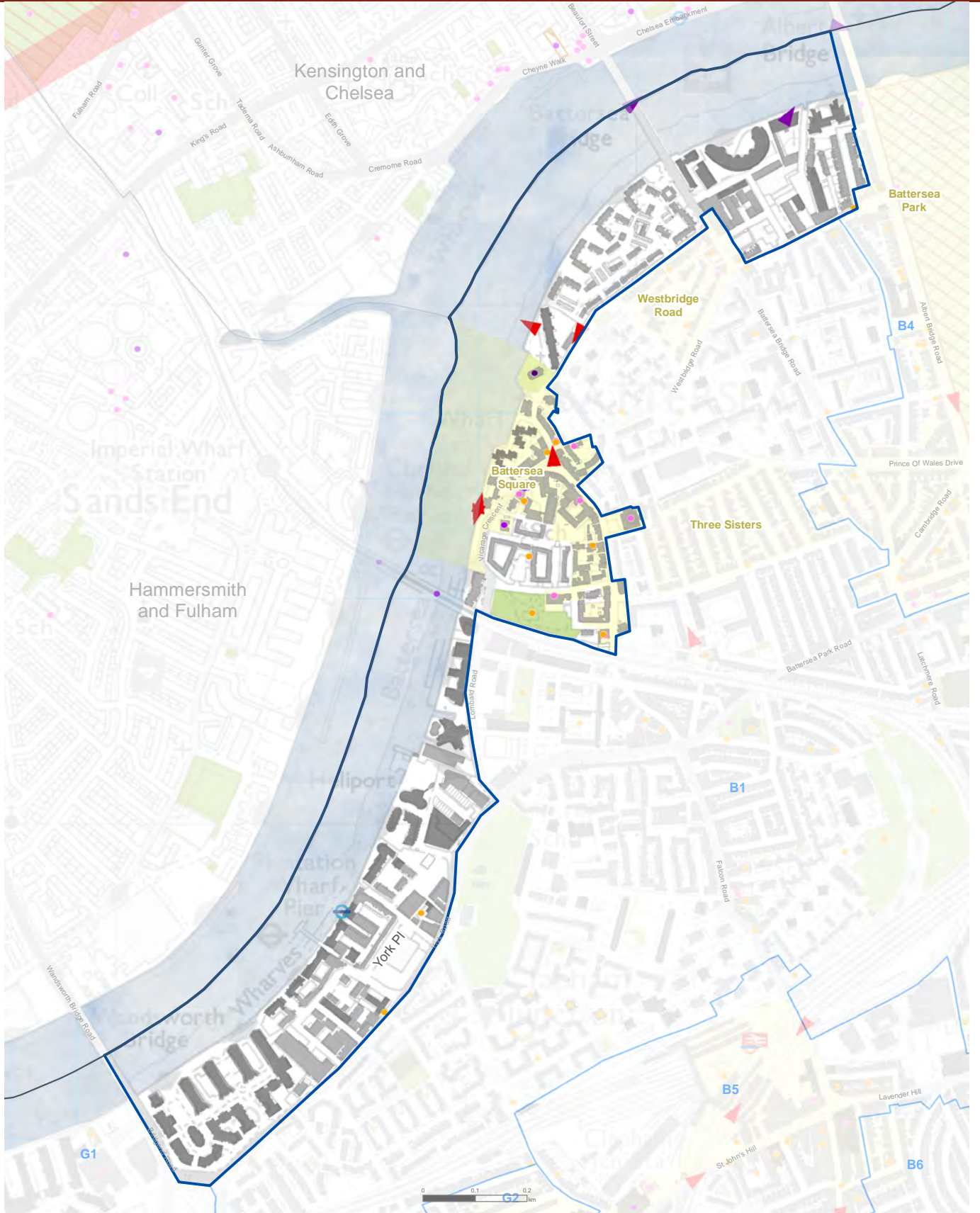
**Coarse urban grain**, with large scale buildings (except for Battersea Square Conservation Area), a mixture of modern and older buildings, as well as trading estates and car dealerships on Lombard/York Road, which have little distinctiveness. **A mixture of building heights**: buildings fronting the river rise to 18 storeys high, whilst around Battersea Square they are mainly 2-3 storeys, and provide positive frontage. The differences and diversity of architectural materials and styles result in **incoherent character, with little sense of historic character outside Battersea Square Conservation Area**.

**Battersea Square** has a strong sense of place and coherent, distinctive historic character. The pedestrianised area provides valuable space for gathering, eating and drinking. High quality public realm creates a pleasant environment.

**Landmarks** include:

- Church of St Mary (grade I listed) and Church of the Sacred Heart (grade II listed);
- modern buildings (not necessarily all positive);
- Albert Bridge (grade II\*), Battersea Bridge (grade II), and Cremorne Bridge (grade II\* listed);
- the former Sir Walter St John School (now Thomas's Preparatory School), dating from 1700 (grade II);
- the former Raven Inn on Battersea Square with its distinctive Dutch gables and quoins.

**An urban feel, with limited tree cover** or open space and often uninviting public realm, with the exception of Battersea Square Conservation Area including St Mary's churchyard and Vicarage Gardens and nearby street trees on Vicarage Crescent. Brick walls fronting the road here add historic character and help to define the street. The River Thames provides a sense of openness and richness from the houseboats near St Mary's Church.



**Fig. 70: B2 Battersea Riverside character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Battersea Square Conservation Area, for its historic character including the original street pattern and vestiges of the old village around the square; also for its sense of place and function for gathering.
- Historic buildings including Old Battersea House, the former Raven inn, Devonshire House (II\*) and the former Vicarage.
- Grade I listed St Mary's Church and surrounding green space which provides a valued riverside public space, as well as a landmark which adds rare historic interest and distinctiveness to the character area. It also has cultural value through its association with JMW Turner who sat at the oriel window in the vestry to paint many of his cloud scenes.
- The River Thames, for its sense of openness and access along the Thames Path, a well-used walking and running route; and proximity to Battersea Park.
- The visual interest, colour and richness provided by

the wharves, valued for distinctiveness, particularly where they retain historic elements such as at Ransome's Dock.

- Fred Wells Gardens (locally listed) and tree lined streets such as Battersea Church Road.
- Valued views, including the view from Battersea Bridge and from the riverside promenade, looking east downstream to Albert Bridge (listed grade II\*) (Fig. 73), as described in the Local Views SPD. Other views and vistas of interest include:
  - views across the Thames into RB Kensington & Chelsea with trees and historic buildings providing a scenic backdrop;
  - views up and down the river, such as the stretch from the quay around the slipway adjacent to St Mary's Church to the houseboats, and from Vicarage Gardens;
  - view of St Mary's Church from Battersea Square and from Battersea Church Road.



Fig. 71: Part of the historic St Mary's Church (grade I listed) and surrounding green space



Fig. 72: Battersea Square, the heart of the historic settlement of Battersea, successfully restored in 1990



Fig. 73: View from the riverside promenade looking downstream to the listed Albert Bridge

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Some imposing landmark buildings which due to their large massing appear monotonous and lack a local distinctiveness.
- Uninviting public realm with a lack of green.
- Absence of activity along the riverside, except for concentrations of activity around St Mary's churchyard. There are few leisure or entertainment destinations, and much of the area feels private, with limited access.
- Maintenance issues on streets and boundaries, particularly in the south of the area.
- Little reference to historic uses or character.
- Incongruous elements with awkward juxtapositions fragment the character, e.g. the Montevetro building which dominates the views around St Mary's Church; the 1970s flats on the riverside west of Vicarage Crescent whose layout, form and scale contrast with and detract from the character of Battersea Square Conservation Area; 1960s development to the east of Battersea High Street which contrasts with the historic pattern of development.
- Poor legibility, particularly east-west access to the river.
- Highly developed, monotonous frontage to the northern bank of the River Thames within LB Hammersmith and Fulham.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Mid-rise flats;
- High-rise flats;
- Shopfront terraces.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

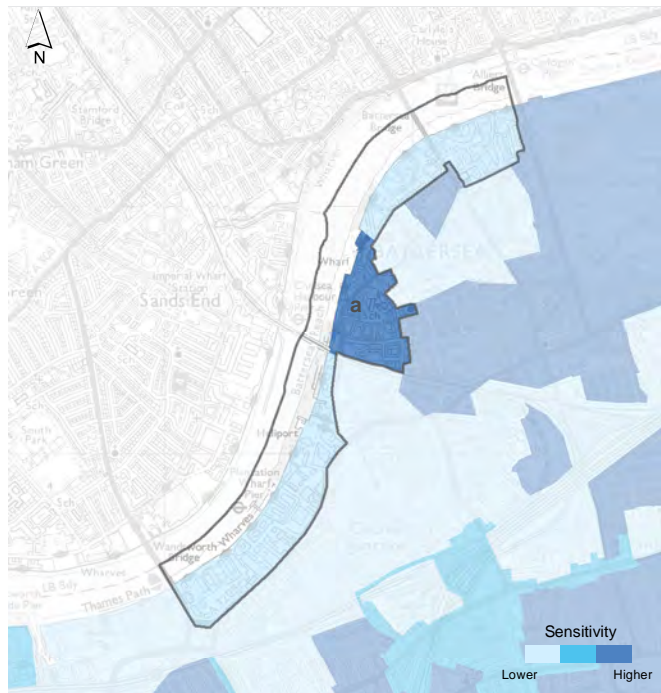


Fig. 74: B2 Battersea Riverside sensitivity plan

Overall, Battersea Riverside has a low sensitivity to change with potential for targeted growth, with the exception of Battersea Square Conservation Area, which has high sensitivity.

Much of the riverside has been redeveloped, leaving limited opportunity for further growth. However, some of the 1960s-70s residential developments are low-rise and provide a poor address to the riverfront. Any redevelopment could accommodate additional height as long as development provides additional public open space around the river and respect the area's valued features, and:

- the area's role as a visual backdrop and setting to the river in views from RB Kensington & Chelsea;
- the setting and views in and around Battersea Park;
- the historic character (both medieval and industrial).

Battersea Square Conservation Area (sub area (a)) has a higher sensitivity change due to its high value and relatively small scale.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve

Restore

Improve

Transform

Except for the Battersea Square Conservation Area, the character of Battersea Riverside is currently incoherent and lacks distinctiveness. This is partly due to the ongoing regeneration of the area and therefore its sense of place will hopefully improve over the coming years.

Therefore the strategy for the area is to **restore** and **improve** character, with new developments building on the existing character of the 'old town' Battersea and industrial richness along the riverside.

## Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Aspire to creating a continuous, connected and legible Thames Path route along the river, linked to an enhanced movement strategy to improve connectivity with the wider area - particularly east, including improved crossings over Lombard and York roads and linking to Clapham Junction.
- Create references to historic pattern, uses and elements where possible to bring coherence, legibility and integrity to the character area.
- Respect and restore historic elements, including St Mary's Church and surrounding green space, but also Victorian industrial buildings.
- New development should have a distinctive character that creates remarkable landmarks. It should provide excellent and inviting public realm as part of a coherent strategy rather than spaces between buildings. Active frontages to the Thames Path should be provided.
- Preserve linear views along the river.
- Retain the mixed uses including restaurants, cafés and pubs around Battersea Square to maintain a sense of activity and vibrancy.
- Develop and enhance the sense of place and focus at Plantation Wharf to aid legibility and quality of experience at this part of the river.
- Consider a wider public realm or cultural strategy to create a sense of coherence between the many different elements along the riverside.
- Encourage a mixture of uses to increase activity and vibrancy along the riverside.

## B3 Nine Elms Mixed Use



Fig. 75: Battersea Power Station during redevelopment

### Key characteristics

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

B3 Nine Elms Mixed Use is part of the GLA Opportunity Area which extends into Vauxhall in LB Lambeth. Focussed around Battersea Power Station, it includes the riverfront along the Thames and nearby former industrial area.

**An area of change and evolution:** at the time of writing the character of the area is dominated by construction sites, empty spaces and poor legibility. However, this is a temporary state and emerging new development will itself bring about new character. There are numerous planning documents setting out more detailed frameworks; see [Wandsworth Council's](#) website for information.

**Low lying, flat land adjacent to the river.** In Anglo-Saxon times the land around Battersea was low lying marshes. Once able to be drained, it was largely used for market gardening on the fertile soils and supplied the capital with fruit and vegetables transported downstream by river. In the 18<sup>th</sup> century 'Nine Elms' was just a small hamlet by the River Thames, located in the area where the American Embassy now is.

**Historically a fragmented landscape of infrastructure and industry.** The flat land and proximity to the river for transport of goods, made Nine Elms a suitable location for rail and industry. The railway was opened in 1838, and locomotive works and engine sheds developed amongst the 'Battersea Tangle'

of crossing and interconnecting railway lines. By 1945 dwellings associated with the former hamlet had been demolished and replaced by large industrial buildings along the Thames, interspersed with wharves, including a gas works, water works, colour works, brewery, Battersea Power Station and rail goods depots. A tramway ran along Nine Elms Lane. **The regeneration of the area will see it transformed by new, mixed uses, with residential, commercial, retail, office and cultural uses.**

**Large footprint and tall buildings with coarse urban grain.** Industry and infrastructure has left an imprint of huge-scale features, and whilst their scale is in balance with the surrounding environment, they have left a legacy of incoherent layout, inconsistent pattern and poor connectivity. New Covent Garden Market was relocated to Nine Elms in 1971-74. More recent developments such as Chelsea Bridge Wharf (14 storeys), Riverlight Nine Elms (up to 20 storeys), the US Embassy (65m) and Battersea Exchange (18 storeys) continue the trend of tall buildings. The overall result is an emerging urban high-rise character.

**Isolated pockets of residential estates south of Nine Elms Lane** are poorly related to their surroundings. The area suffered badly from bombings during the war, resulting in large scale redevelopment of new public housing estates including the Patmore Estate (constructed in the 1950s) of red brick 5 storey blocks set in expanses of poorly used open spaces.

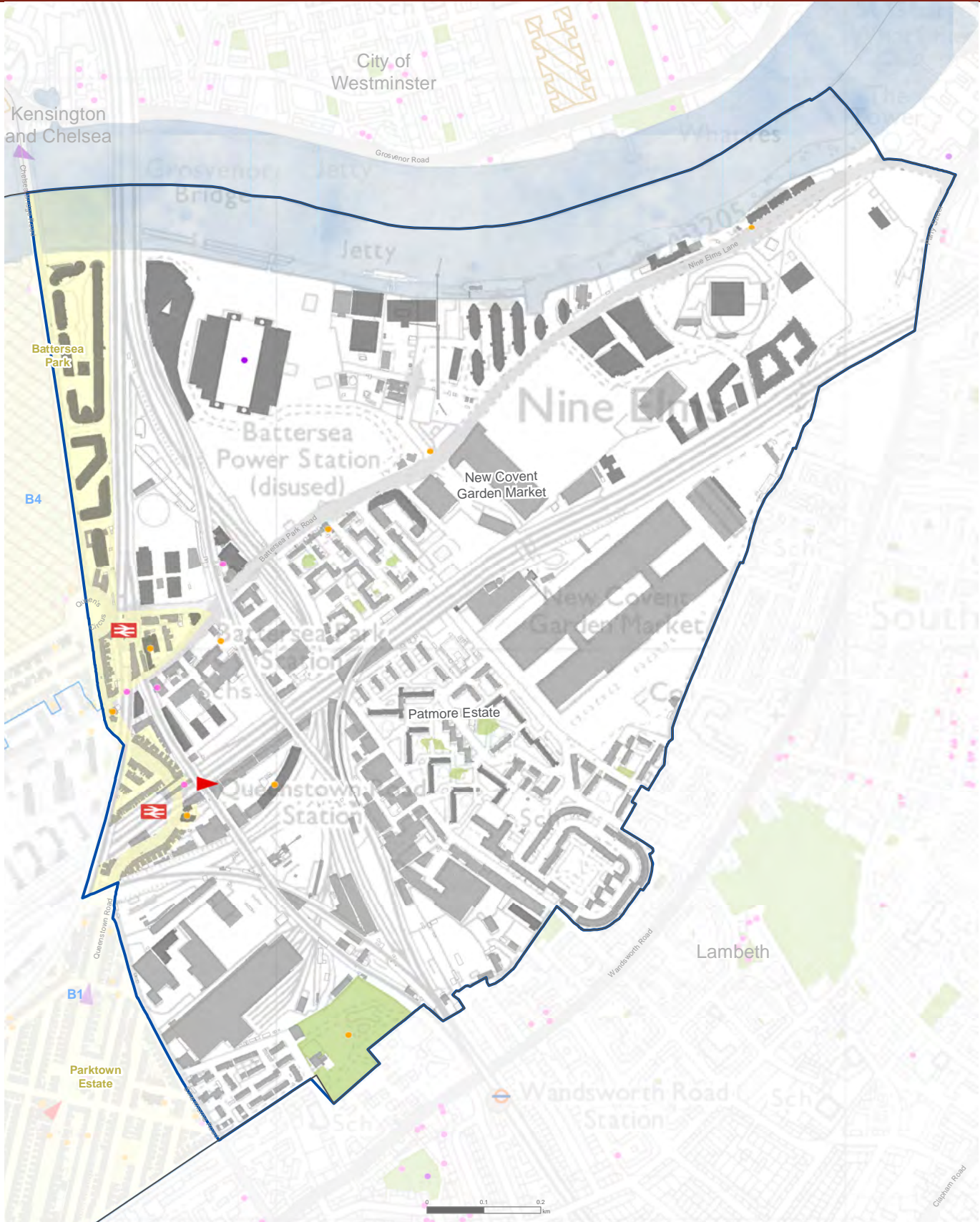
**Relatively little remaining historic fabric**, therefore remnant historic/cultural features and references are particularly valued. The western part of the character area is within Battersea Park Conservation Area.

Battersea Power Station is a key heritage asset, as are railway stations (Battersea Park and Queenstown Battersea), pubs and occasional Victorian houses.

**Typical materials are red, brown and yellow brick.** Large buildings are broken up with architectural details, bays and roof shapes. Substantial and tactile materials e.g. cast iron help to ground elements in the landscape.

Landmarks include:

- **Battersea Power Station** (grade II\* listed), an iconic London landmark (built 1929 to designs by Sir Giles Gilbert Scott; decommissioned 1983). Its chimneys are a prominent feature on the London skyline;
- New Covent Garden Market;
- US Embassy;
- Hamptons Depository.



**Fig. 76: B3 Nine Elms Mixed Use character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Battersea Power Station (grade II\* listed), its memorable and distinctive form of London-wide and international value, and symbol of Britain's industrial heritage.
- Mature trees, relative rarity in the area, for their visual amenity, biodiversity and resilience value.
- Railway features of heritage value, including Battersea Park Rail Station (grade II listed); the railway bridge on Battersea Park Road (grade II); and Queenstown Road Station (grade II listed).
- Individual buildings of heritage interest, including listed buildings such as the Masons Arms pub (grade II), a mid-19<sup>th</sup> century 3 story corner building.
- Battersea Park Conservation Area, although much changed in this area, it nevertheless provides a setting to the park and listed buildings.
- Heathbrook Park, valued by the local community as a place of tranquillity and as a setting for events such as Wandsworth Arts Fringe Festival.
- Cultural and historic value of the area's past industrial heritage.
- The River Thames and its setting, valued for its scenic qualities, activity and interest as well as the Thames Path. Houseboats and planting by Riverlight provide welcome texture and human scale.
- Views and vistas of interest, including:
  - from Chelsea Bridge to Battersea Power Station (locally valued and listed in the Council's SPD);
  - of the former Hamptons Depository at Ingate Place, a familiar warehouse landmark for passing train travellers.
- Sense of vibrancy and activity around the new Battersea Power Station development, as a result of rich art and culture, bars and restaurants.
- Battersea Dogs and Cats Home including Whittington Lodge (grade II listed), for its cultural value and historic interest.



**Fig. 77:** Colour and vibrancy created by artistic Grosvenor Arch gateway under Battersea Bridge



**Fig. 78:** Trees and planting go some way to softening hard urban realm of Nine Elms Lane by Riverlight



**Fig. 79:** Grade II listed Battersea Park Station (1865), of pale gaults with detailed stone and red brick details; and the grade II listed railway bridge

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Lack of public green open spaces (although partly due to construction sites). Some new developments have provided green space: e.g. Riverlight Nine Elms planting, although it is small in proportion to the scale of development. Embassy Gardens development is inward looking with a sense of 'privacy' to the spaces, and no public areas of open grass. Patmore Estate's open spaces and Carey Gardens do not fulfil the biodiversity or amenity functions they could do. Plans for new open spaces will be important for the area.
- Fragmentation (physical and perceptual) resulting in a lack of legibility; exacerbated by current construction sites. This is likely to be improved following delivery of good new developments (e.g. improved railway arches).
- Poor pedestrian environment along Nine Elms Lane.
- Discontinuity of the Thames Path, as a result of construction sites and the Cringle Dock Waste Transfer Station.
- Blank façades of New Covent Garden Market present an unwelcoming frontage to the street.
- Some awkward juxtapositions of old and new elements - e.g. the tower of the Battersea Exchange in relation to the 3 storey terraces at Queen's Circus in Battersea Park Conservation Area and development along Chelsea Bridge being overbearing on Battersea Park.



## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Mid-rise estates; high-rise estates;
- Mid-rise flats; high-rise flats;
- Large floorplate commercial/retail/industrial.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

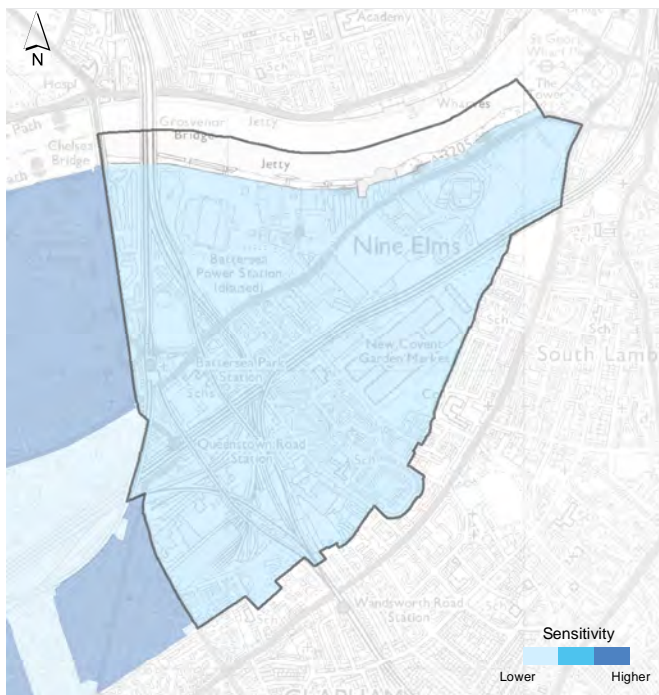


Fig. 80: B3 Nine Elms Mixed Use sensitivity plan

Overall, B3 Nine Elms Mixed Use has a low sensitivity to change, with opportunity for enhancements through development and change, as is already underway.

New development should respect and enhance the area's valued features and qualities. Sub-area B3a also has a low sensitivity to change but has a lower prevailing building height than the major development area north of the railway.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve

Restore

Improve

**Transform**

The existing character of the area is fragmented and incoherent, therefore the strategy is to transform the character of the place, as is already underway.

Any strategy should consider movement across the busy railway corridor that bisects the area.

## Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Ensure a continuous Thames Path along the river.
- Create high quality and generous publicly managed public realm at the riverside, achieving a balance of open space and built form. Ensure green infrastructure is physically connected and biodiverse with use of large trees.
- Establish a wayfinding strategy, connecting and promoting active use of new public spaces through integrating facilities and events. Enhance access to: the riverside; across Nine Elms Lane; over railway infrastructure and around estates. Consider a meanwhile use strategy to mitigate construction effects.
- Ensure new developments resolve existing physical and perceptual fragmentation.
- Preserve views and the setting of Battersea Power Station and the River Thames.
- Improve frontages, public realm and signage along Battersea Park Road/Nine Elms Lane. Encourage independent shops and businesses, emphasising local makers and artisans.
- Respect remnant historic features. Promote associations with the area's history and culture e.g. food, rail, bridges, former Nine Elms settlement, industry/creativity. Promote use of natural materials such as high quality brick, metal, cast iron, and architectural detailing.
- Improve integration of New Covent Garden Market with its surroundings - e.g. enhance uses in railway arches.
- Development should step down towards sensitive surrounding features, particularly including Battersea Park Registered Park and Garden.

## B4 Battersea Park



**Fig. 81:** Looking towards the river from Battersea Park to the red brick Queen Anne houses on Chelsea Embankment in RB Kensington & Chelsea

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Battersea Park encompasses almost the entire Battersea Park Conservation Area, as described in more detail in the [Conservation Area Appraisal](#). Its special character derives from the formal relationship between the park, the urban development surrounding it and the River Thames.

**Battersea was mainly laid out as fields for farming and grazing until 1846;** by the middle of the 19<sup>th</sup> century the marshy area known as the Battersea Fields had become an undesirable pleasure ground. London's population was expanding rapidly and the industrial revolution was causing increasing pollution. At the same time public parks were being recognised as the lungs of the city and part of the solution to slums and epidemics. An Act of Parliament was passed in 1846 to form a public park. The park was opened in 1858, originally designed for promenading and formal and informal recreational activities, including boating on the lake, listening to music in the bandstand, visits to the aviary, annual plant shows, a sub-tropical garden and cricket matches.

**Grade II\* Registered Historic Park and Garden of outstanding landscape interest and quality.** Its historic landscape is based on the formal and informal compositions within the park, its tree-lined carriageway drives, embankment promenade, lakeside settings, the views within and from the park and its relationship

to the River Thames. The influence of John Gibson on the planting of the park received acclaim and his original planting list of 1858 survives. More detailed information on the special qualities of the park can be found in [Historic England's Register](#).

**The combination of avenues of mature trees, lakes and open landscape with views over the river** provides high scenic quality and a feeling of serenity and tranquillity, with the promenade along the Thames popular for walking. The area has a coherent and distinctive character.

**Riverside setting** which extends the sense of nature, openness and space. The frontage to the river is dominated by well treed landscape setting with views of the bridges over the river and landmarks such as the four chimneys of the former Battersea Power Station (grade II\*).

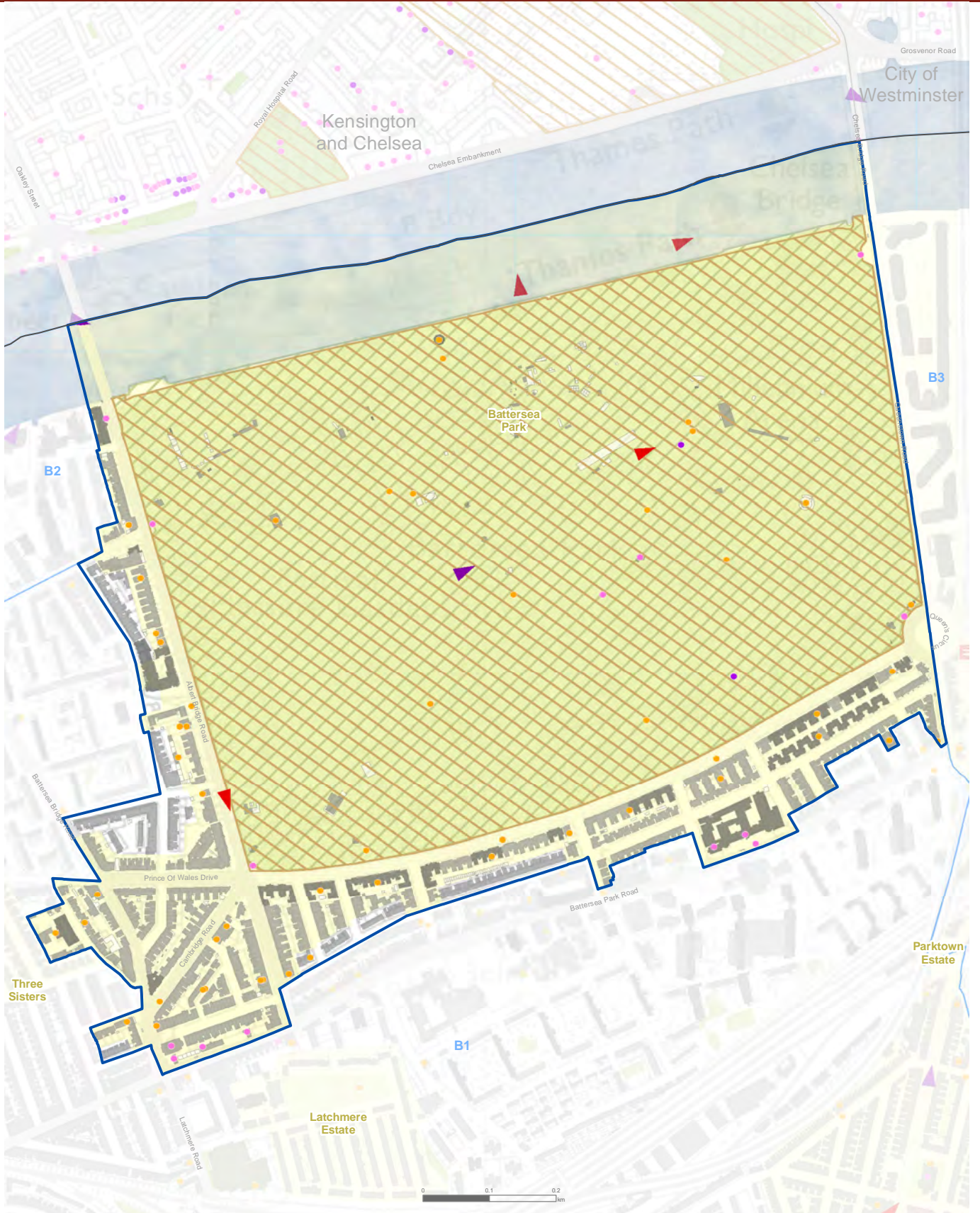
**Elements of its original Victorian gardenesque design and some of the 1951 Festival of Britain of Britain remain.** Battersea Park was chosen for the site of the Festival of Britain Pleasure Gardens in 1951; although most of the structures were demolished, much of the Grand Vista remained, including fountains, ornamental pools and a pair of Gothic towers.

**Victorian mansion blocks provide a robust backdrop and framing to the park.** On Prince of Wales Drive mansion blocks are late 19<sup>th</sup> century, and 5 storeys high, successfully respecting the proportions of the park and framing its southern perimeter. They were designed to front the park and continue the quality and character of similar buildings north of the river in Chelsea. The massing of the mansion flats is broken up by recesses, projecting front entrances and bay window sections as well as formal hedges to their boundaries. Behind these, 2-3 storey terraces have narrow frontages, giving a finer grain of development. West of the park 4-5 storey mansion blocks (e.g. Albert Mansions and Albany Mansions) and detached, semi-detached and terraced houses and the Prince Albert pub provide more variety.

**Unusual buildings**, rare in the context of Wandsworth, including Albert Studios (purpose built artists studios) and Kersley Mews (former stabling for horses).

### **Important landmarks include:**

- Albert Bridge (grade II\* listed), illuminated at night;
- the Peace Pagoda (locally listed).



**Fig. 82: B4 Battersea Park character area plan**  
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### Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Battersea Park, grade II\* Registered Park and Garden and Conservation Area. Valued for its cultural and historic character, sense of openness and as a backdrop to views across the river. Also valued for its relative sense of tranquillity.
- Nationally and locally listed features including Albert and Chelsea Bridges.
- Avenues of mature London plane trees, valued for their visual amenity, natural and structural value.
- Range of activities which attract visitors including a zoo, Battersea Arts Fair, marathons and play.
- Historic street furniture including traditional red pillar boxes and enamelled street signs.
- The cobbled street surface of Kersley Mews, one of the few intact cobbled surfaces in the borough.
- Valued views from within Battersea Park. Three specific views are of borough importance, listed in the Local Views SPD, these are:
  - view 3, from Albert Bridge looking east downstream towards Battersea Park with the chimneys of Battersea Power Station forming an important backdrop to the park;
  - view 4, from Chelsea Bridge to Battersea Power Station, where the striking silhouette of the four chimneys against the skyline gives significance to the setting of the building;
  - view 6, from east of the bowling green towards Battersea Power Station;
- Other views and vistas of interest include: views of Chelsea Bridge from the riverside promenade; views of the four chimneys of Battersea Power Station from many other parts of the park; views across the river (to RB Kensington and Chelsea) to Chelsea Embankment and the open space of the Ranalagh Gardens and Chelsea Royal Hospital; the vista south from Albert Bridge Road towards the Lighthouse pub.



**Fig. 83:** The 5 storey Prince of Wales Mansions (locally listed) provide a robust frontage to the park



**Fig. 84:** Locally listed Peace Pagoda in Battersea Park, built in 1985



**Fig. 85:** The River Thames from Battersea Park, with grade II\* listed Albert Bridge in the background

### Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Some incongruous infill buildings detract from the strong character and quality of the area (e.g. at the junction of Albert Bridge Road and Prince of Wales Drive).

### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Mansion blocks;
- Mid-rise flats.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

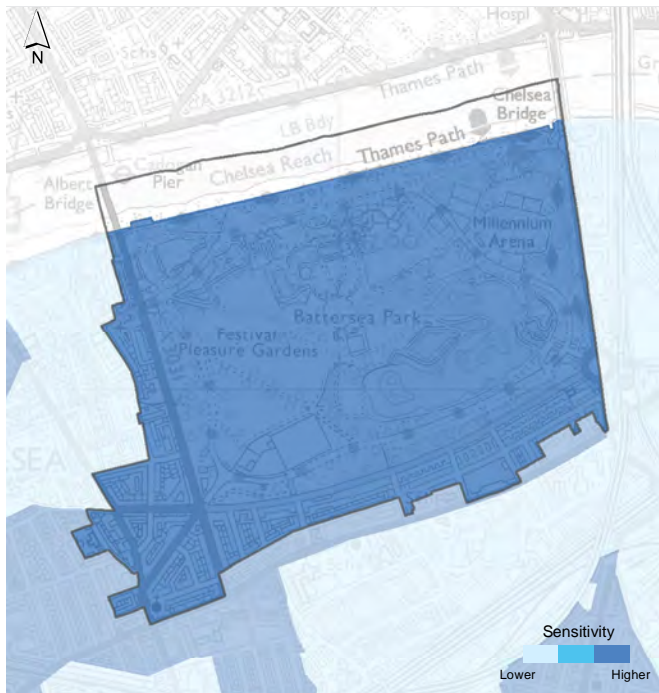


Fig. 86: B4 Battersea Park sensitivity plan

Overall, Battersea Park has a high sensitivity to change. The high value and high susceptibility of the area mean that any significant change is unlikely to be appropriate.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.



The existing character of Battersea Park is strong and the strategy is therefore to **conserve**, and enhance existing character.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Refer to the [Conservation Area Appraisal & Management Strategy](#).
- Conserve and enhance valued features including the listed and non-listed elements.
- Retain and manage existing trees, particularly mature trees.
- Preserve and reinstate original features in the public realm such as paving slabs, granite kerbs and setts.
- Reinstall historic features of buildings such as timber sash windows, front doors, front boundaries and pathways.
- Resist external additions to buildings such as satellite dishes and security grilles.
- Preserve the openness of the park and the riverside by resisting development in the wider area which would affect this perception.
- Protect the openness and framing of vistas towards the river, across Battersea Park and towards Battersea Power Station.
- Continue the programme of facilities and events in the park to maintain its vibrancy and importance as a destination.
- Respect the scale and proportions of existing period buildings and streetscape which is fundamental to the character of the area.
- Protect the character of the conservation area and views that retain the sense of open space and trees dominating the skyline.
- No capacity for buildings of 5 storeys and over has been identified.

## B5 Clapham Junction Town Centre



**Fig. 87:** The landmark Arding & Hobbs department store and its distinctive clock tower lends gravitas to the primary focal point of the town centre

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Clapham Junction Town Centre encompasses the station and the main shopping parade of St John's Road, flanked by the crossroads with St John's Hill/Lavender Hill to the north and Battersea Rise to the south. South of Battersea Rise, is Northcote Road which has a distinct and interesting character.

### **Prior to the construction of the first railway line in 1838, much of the area was laid out as fields:**

Lavender Hill takes its name from the lavender which was cultivated to produce perfume. John Roque's 1746 map shows a river, the Falcon Brook, flowing down St John's Road, and then north along Falcon Road to the Thames. The river was culverted in the late 19<sup>th</sup> century.

**Clapham Junction station** opened in 1863 and was the catalyst for development of the town to the south of the railway. The station a new cluster of civic buildings including the town hall, library, police station, court and post office to be developed along Lavender Hill in the 1880s and 1890s. The area became a thriving cultural destination, with the Pavilion cinema (where Asda now is), Shakespeare Theatre, (both lost in the war), the Grand Palace of Varieties music hall (now Clapham Grand) and the former Granada cinema (grade II\*). Today Clapham Junction station is a major transport interchange and noted as the busiest station in the UK. However, construction of the railway line resulted in a **major barrier to movement north and south.**

**Ridge and valley topography results in sweeping views.** St John's Hill and Lavender Hill mark the edge of higher ground, north of which the land falls away. St John's Road marks the former river valley of the Falcon Brook, with land rising up to either side. There are sweeping views down St John's Hill, looking across the valley to terraces stepping up to Lavender Hill.

**The junction of Lavender Hill/ St John's Rd/St John's Hill/ Falcon Rd is the focal point of Clapham Junction Town Centre.** Here the landmark tower of the Arding & Hobbs department store building provides a strong sense of place (see [Fig. 87](#)). There was previously a triangular open space at this point.

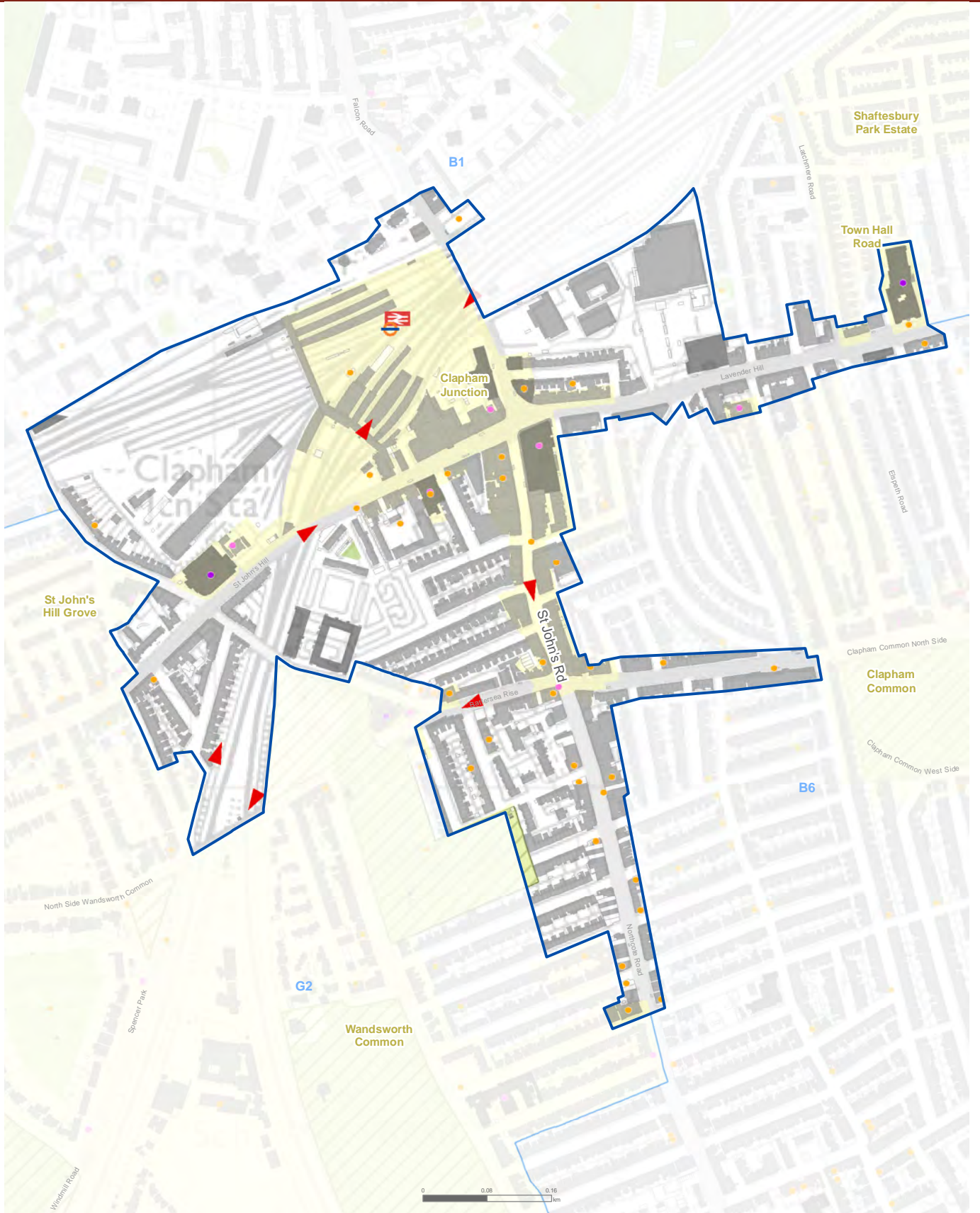
**Northcote Road**, a high quality shopping parade with frontages in good condition. Its distinctive character derives from the independent shops, the well maintained and often quirky frontages (see [Fig. 89](#)).

**Terraced streets with historic shopfronts at ground floor and richness in individual buildings, both new and old.** The streetscape is diverse, interesting and textured. Red brick is a particularly characteristic material. **Building proportions are modest, 3-4 storeys**, rising up gradually at junctions for grand landmark buildings, and towards the railway. Streets are built in tightly knit terraces with many units retaining their **original Victorian and Edwardian Shopfrontages** on narrow fronted plots. The traditional features of timber, narrow fascia boards with painted lettering, carved stone or moulded plaster corbels, add great interest and richness, although many have been lost over time and lack the consistency they once had.

**Landmark buildings at junctions and main roads** contribute to scenic street scenes and aid legibility. They include: the Arding & Hobbs store (grade II), the Falcon pub (grade II), Clapham Grand (grade II), the library (grade II), Battersea Arts Centre (grade II\*, formerly the town hall), and the Northcote pub.

**Bustling and active streetscape**, with a mix of uses and populated with shoppers, people using the station, cafés, workers and a vibrant nightlife. The town centre is a popular destination for bars, restaurants and nightclubs. There are also some negative effects of noise from trains and road traffic.

**Little green space in the area**, although Clapham Common and Wandsworth Common are close by. Existing mature trees are therefore distinctive and important in softening the urban character of the streets.



**Fig. 88: B5 Clapham Junction Town Centre character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Northcote Road's distinctive, high quality shopping parade with frontages in good condition.
- A high quality townscape in the town centre. In particular the architectural, heritage value and historic character in and around Clapham Junction Conservation Area, of special interest for its surviving Victorian and Edwardian townscape exemplified by the terraces of shops in St John's Road, St John's Hill and Lavender Hill.
- Landmark buildings, many of which are listed, which contribute to scenic street scenes, including the distinctive Arding & Hobbs building and Battersea Arts Centre.
- Existing mature trees and public open spaces which help soften the urban character of the streets, including the London plane trees in St John's Hill on the approach to Clapham Junction from the west.
- Views and vistas of interest, including:
  - vistas across mature open green spaces, the unfolding view of Clapham Junction from the railway from Waterloo, with the view of the towers in the Clapham Grand signalling arrival at the station;
  - south along Northcote Rd to the Northcote Road Baptist Church;
  - from the railway bridge in St John's Hill to the buildings stepping up Lavender Hill indicating the valley of Falcon Brook;
  - to the landmark St Mark's Church (within Wandsworth Common character area) from Battersea Rise looking west, and from the railway;
  - from the overbridge at Clapham Junction station towards central London landmarks including Battersea Power Station, the London Eye and the Palace of Westminster World Heritage Site.



**Fig. 89:** Northcote Road - the well-maintained and often quirky frontages give a distinctive character



**Fig. 90:** View north along Northcote Road to the landmark Northcote Road Baptist Church



**Fig. 91:** Typical 4 storey red brick shop terrace on Lavender Hill

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Many of the upper levels of buildings suffer from neglect, with some inappropriate Shopfront alterations at ground floor.
- Roads and the railway fragment the area, are barriers to movement, and reduce legibility. Noisy and busy roads detract from enjoyment of the street scene.
- Incongruous modern development with inappropriate scale/materials/details detract from the coherence of the street scene e.g. Asda on Lavender Hill, Mysore Road, which sits uncomfortably with the adjacent terraces on Lavender Hill, and modern developments at the junction of Plough Lane/St John's Hill which detract from the prominence of the listed landmark.
- The 'tunnel-like' pedestrian environment under the railway along Falcon Road, with accompanying air quality issues.
- Falcon Lane, with the lack of building frontage and its lack of connection with the town centre, its poor quality layout and appearance adversely affects the setting of the town centre and conservation area.
- Station entrances and the station building could have a grander sense of arrival. The Grant Road station entrance has an unwelcoming appearance and its public realm and the frontage to the businesses in the railway arches lacks sense of place.
- Clutter in places and a lack of green sometimes create a harsh pedestrian environment.



## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Shopfront terraces;
- Town centre retail;
- Large floor plate retail/industrial;
- Civic/transport.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

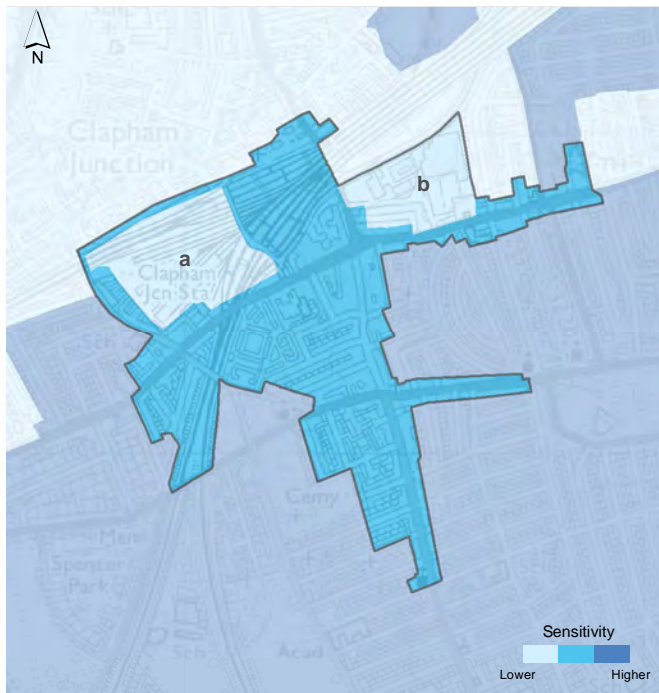


Fig.92: B5B5 Clapham Junction Town Centre sensitivity plan

Overall, Clapham Junction Town Centre has a medium sensitivity to change with the potential for targeted growth in key locations.

The following elements/qualities are more sensitive to change:

- Clapham Junction Conservation Area;
- the modest scale and overall coherence of the 3-4 storey terraced buildings and Victorian/Edwardian shop houses and their settings;
- the existing landmark buildings, their settings and their importance to legibility and views, as buildings similar or taller in height may detract from their cultural and visual prominence.

The areas around Clapham Junction Station, sub area (a) and Falcon Lane, sub area (b) are key locations in which targeted change may enhance character, legibility and the sense of arrival due to the presence of more detracting features.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

<i>Conserve</i>	<i>Restore</i>	<i>Improve</i>	<i>Transform</i>
-----------------	----------------	----------------	------------------

Overall Clapham Junction Town Centre has a strong sense of place, and the strategy is to conserve the key characteristics which contribute to this, notably the historic and modest scale shop terraces and landmark buildings.

There are elements which detract from the sense of place, and there is opportunity to restore a coherent distinctiveness and strengthen existing character by providing new developments of high quality which respect its positive key details, materials and scale.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Refer to the [Conservation Area appraisal](#) for relevant areas.
- Respect the proportions, scale and coherence of terraced streets, Shopfrontages and their settings. Reinstatement Shopfronts to their original design to achieve greater consistency. Encourage active frontages at night, avoiding opaque shutters.
- Any new taller elements should respect existing character, have design elegance and quality that marks them as landmarks rather than just an attempt to create increased density.
- Respect the settings, proportions, scale and details of existing landmark buildings.
- Protect the quality of valued views and vistas.
- Ensure good maintenance of building façades and public realm.
- Introduce further traffic management to reduce the dominance of cars and promote active travel.
- Enhance the sense of arrival and quality of the public realm at the station, identifying opportunities for art, wayfinding and high quality architecture. Consider opportunities to enhance the arches on the Grant Rd station exit.
- Create new destinations to enhance the visitor experience inspired by its past e.g. theatre/music venue/concept stores/design shops/galleries.
- Reintegrate the Falcon Lane area to the town.
- Enhance experience and quality of the public realm including seating, trees and planting.
- Maintain and increase quantity of street trees.

## B6 Clapham Common and Residential



**Fig. 93:** Landmark Church of St Barnabas (grade II listed) seen across the Common, and the unusual modern spire of St Andrews

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Clapham Common and Residential encompasses the part of the Common that lies within Wandsworth (the eastern part within LB Lambeth), and the surrounding residential streets of consistent, period terraced housing.

**Lavender Hill and Clapham Common North Side (A3) provide a framework to the area**, and were early coaching routes into London, following the high ground. In Saxon times Clapham Common was wild and marshy, and used as common land by the local community for collecting food, fuel, grazing animals, and milling grain. By the late 19<sup>th</sup> century houses of wealthy city-workers lined the Common on all sides. The Common was converted to parkland and formally designated for public use by the Metropolitan Commons Act 1878. It became a popular leisure attraction, with cricket, golf and sailing of model boats. During the World Wars, it was used for growing food. An underground deep level shelter at Clapham South station remains (grade II listed).

**Sense of openness and green**, provided by the Common, a well-maintained public open space and designated Metropolitan Open Land and Conservation Area. Avenues of mature plane trees give structure and sense of enclosure. The green character permeates through to the residential streets with street trees and vegetated front gardens.

**The Common is a destination for sport, leisure, socialising and relaxing** with facilities including playground, football pitches, car parks, tennis courts, open areas of grass and fishing in Mount Pond. **It also has some historic and natural character:** retaining the integrity of its historic layout, and historic features such as the bandstand (Fig. 95). A Site of Importance for Nature Conservation (SINC), supporting grassland, and ponds, and wilder area of woodland and glades.

**Residential streets of consistent, terraced houses in good condition.** There is a sense of formality and uniformity, with a regular grid layout and repetition in building frontages. Houses are 2-3 storeys, mainly terraced, but also semi-detached/detached. Materials are red or yellow stock brick with decorative features. Front boundaries are a mixture of railings and walls, generally intact, creating a positive streetscape.

**Local shopping parades**, Battersea Business Centre Employment Protection Area and Lavender Hill/Battersea Rise local centre provide diversity to the character, creating activity and interest to the area.

**High quality and distinctive townscape**, reflected by many of the houses being listed for their architectural merit. Houses fronting the Common are included within Clapham Common Conservation Area; including substantial and distinctive grand Georgian detached villas. They provide a positive frontage to the green space. There is also a small part of Clapham Junction Conservation Area in the north west corner which contains 2-3 storey terraced houses on Beauchamp Road.

**Long, open vistas** across the Common to tree-lined horizons, due to the area's relative elevation. There are also long vistas along tree-lined streets e.g. looking west along Mallinson Road, and the attractive curved roads of Sisters Avenue/Thirsk Road. Long views to towers on the South Bank such as Vauxhall Tower, appearing above the wooded horizon.

**A formal character**, both in the Common and surrounding streets. Although there are moments of tranquillity (such as around the ponds), overall there is little sense of wildlife. The Common has a sense of vibrancy and activity, with various activities (football, fitness, picnics, gatherings), and of vastness, with few intimate spaces. There is a more urban feel than some of the other commons due to the views of tall development and the influence of busy roads which pass around and through the Common, affecting its tranquillity.

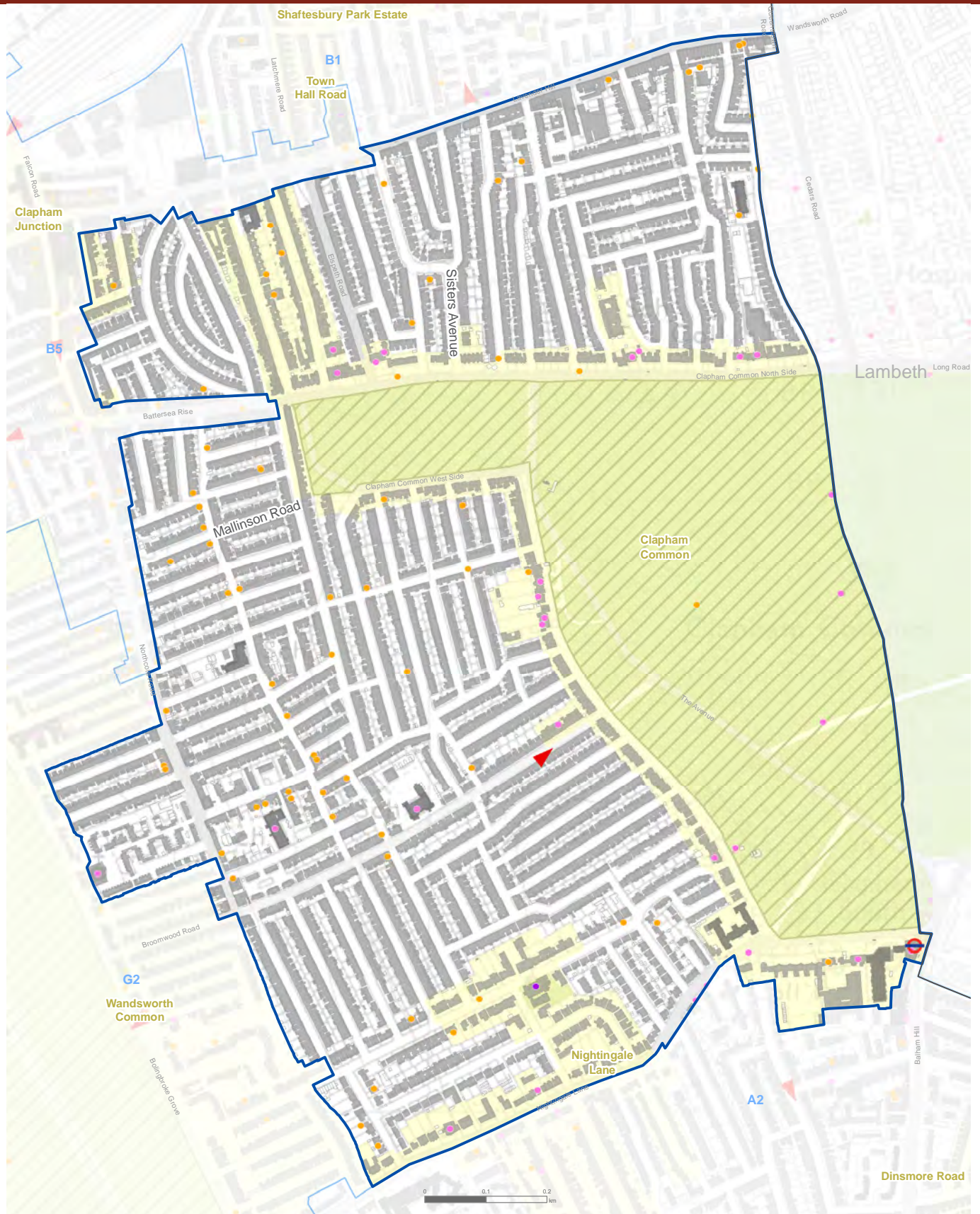


Fig.94: B6 Clapham Common and Residential character area plan

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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Clapham Common, (MOL, Conservation Area and locally listed garden), highly valued for recreation, leisure and play, as a space for gathering and socialising and as a setting for sports and events such as music festivals, and for its sense of openness, important in the context of the urban setting.
- Avenues of mature trees on the Common which are valued for their climate change resilience, structure, enclosure and biodiversity.
- The special relationship between the Common and the houses fronting it: the buildings create visual and historic interest and help define the open space.
- A large number of listed buildings and features: individual buildings of architectural and historic value, which provide variety and interest, including listed buildings, such as the Church of the Ascension (grade II\*), Clapham South tube station and the bandstand on the Common (grade II listed).
- The consistency, charm and architectural value of the period houses and their front gardens, including those which provide a strong setting to the Common. Rich details such as sash windows, moulded lintels, add a sense of local distinctiveness.
- Street trees, which soften the continuous residential street layouts.
- Local parades of shops, which add interest, vitality and provide local and independent services.
- Long open vistas and wooded skylines and proximity to Wandsworth Common and Battersea Park.
- Landmarks include:
  - the church tower of St Barnabas' ([Fig. 93](#));
  - Clapham South tube station;
  - Walsingham School (grade II listed); and Wix Primary School;
  - the bandstand on Clapham Common.



**Fig. 95:** The bandstand: erected in 1890 and now the largest Victorian example still extant



**Fig. 96:** Interesting white plaster details and quality red brick finishes near Clapham Common



**Fig. 97:** Springwell House (grade II listed), a Georgian house fronting Clapham Common

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- The busy roads passing through and around the Common affect the sense of tranquillity.
- Front gardens which are paved over or are missing boundary walls detract from the generally coherent streetscape.
- Biodiversity of the Common could be enhanced e.g. historically it supported acid grassland (some was lost following the vegetable production during the World Wars which led to increased fertility of the soils).
- The 20<sup>th</sup> century St Andrews Church is out of character with its surroundings and not sympathetic to the nearby listed church.
- Fairground site on the Common is visually unattractive when not in use.
- Some maintenance and litter clearance issued perceived in parts of the Common.

### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Semi-detached; detached;
- Villas;
- Mansion blocks;
- Mid-rise flats.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

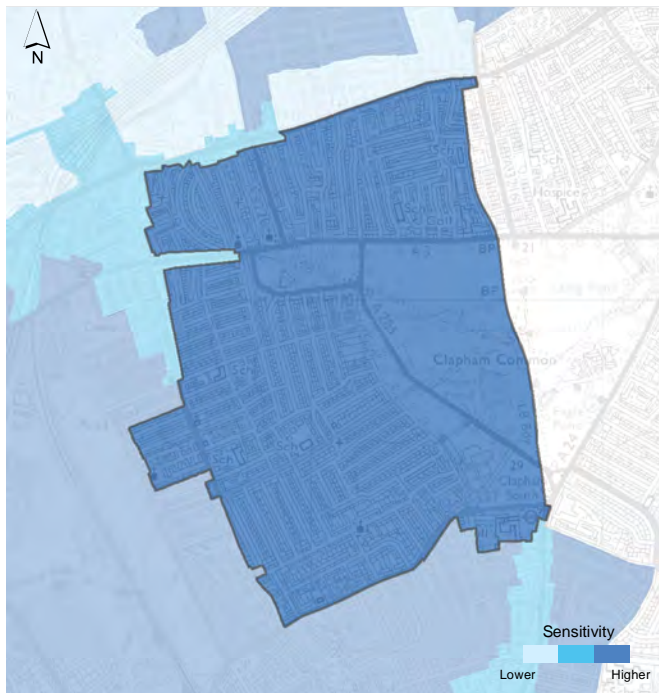


Fig. 98: B6 Clapham Common and Residential sensitivity plan

Overall, Clapham Common and Residential has a high sensitivity to change, and extensive change is not appropriate.

There may, however, be small areas of lower sensitivity where the townscape is less intact and does not reflect the character described in the key characteristics.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

<b>Conserve</b>	Restore	Improve	Transform
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The existing character of the area is strong, with a distinctive sense of place and many highly valued features. Therefore the strategy is to **conserve** the character, elements and features, whilst enhancing existing features where appropriate.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Ensure any new development uses high quality materials and respects the scale and historic character of the existing architecture.
- Respect the scale and proportions of existing buildings and streets. Some taller buildings (up to 4 storeys) exist on main roads and fronting the common (up to 6 storeys in the south west and near Clapham South local centre) but otherwise the scale of built form is very consistent. Wider streets and corner plots might be able to accommodate buildings over 2-3 storeys but should not exceed 4 storeys.
- Maintain and enhance street trees, front gardens and boundary walls.
- Preserve and maintain mature trees, particularly the avenues on Clapham Common.
- Maintain and encourage the use of traditional materials - primarily brick. Ensure special attention to choice of materials and architectural details to reflect the rich townscape of the area, avoiding the use of timber or plain façades.
- Maintain the historic skyline by ensuring any roof extensions are highly sympathetic to the architectural building and street character.
- Enhance the biodiversity of Clapham Common.
- Maintain and enhance existing wooded skylines and open vistas.
- Maintain the openness of the Common and streetscape by resisting permanent development that would affect this overall perception.

# C Earlsfield and Southfields



Fig. 99: Tree lined streets with post-war development abutting period terraces

## Summary

Earlsfield came into existence as a place name around 1870 following construction of the railway and later station, overtaking the former hamlet of Garratt further south. Southfields, originally part of Earl Spencer's Estate sold off in the 1870s developed with uniform 2 storey terraces with some commercial / industrial premises since converted into residential flats.

In 1916 a new bridge across the River Wandle was built, connecting Earlsfield and Southfields. New development continued in the area before WWI, including a number of low height estates including Magdalen Park, Fieldview and Openview estates. While the River Wandle has in part been built upon, it is still marked by the large open space of King George's Park, flanked by industrial sheds.

## Key changes and trends

- The local centre around Earlsfield National Rail station is a focus for new development where appropriate, including increased housing.
- The northern end of the area abuts the Wandsworth Town area of change with significant development planned including a number of tall buildings completed and planned.
- Parts of the Wandle Valley are a protected employment zone which are likely to undergo redevelopment over time but maintain an industrial/commercial focus.
- The area also borders a major new dense development at the former greyhound racing track, which is in the adjacent LB of Merton.

# Earlsfield and Southfields

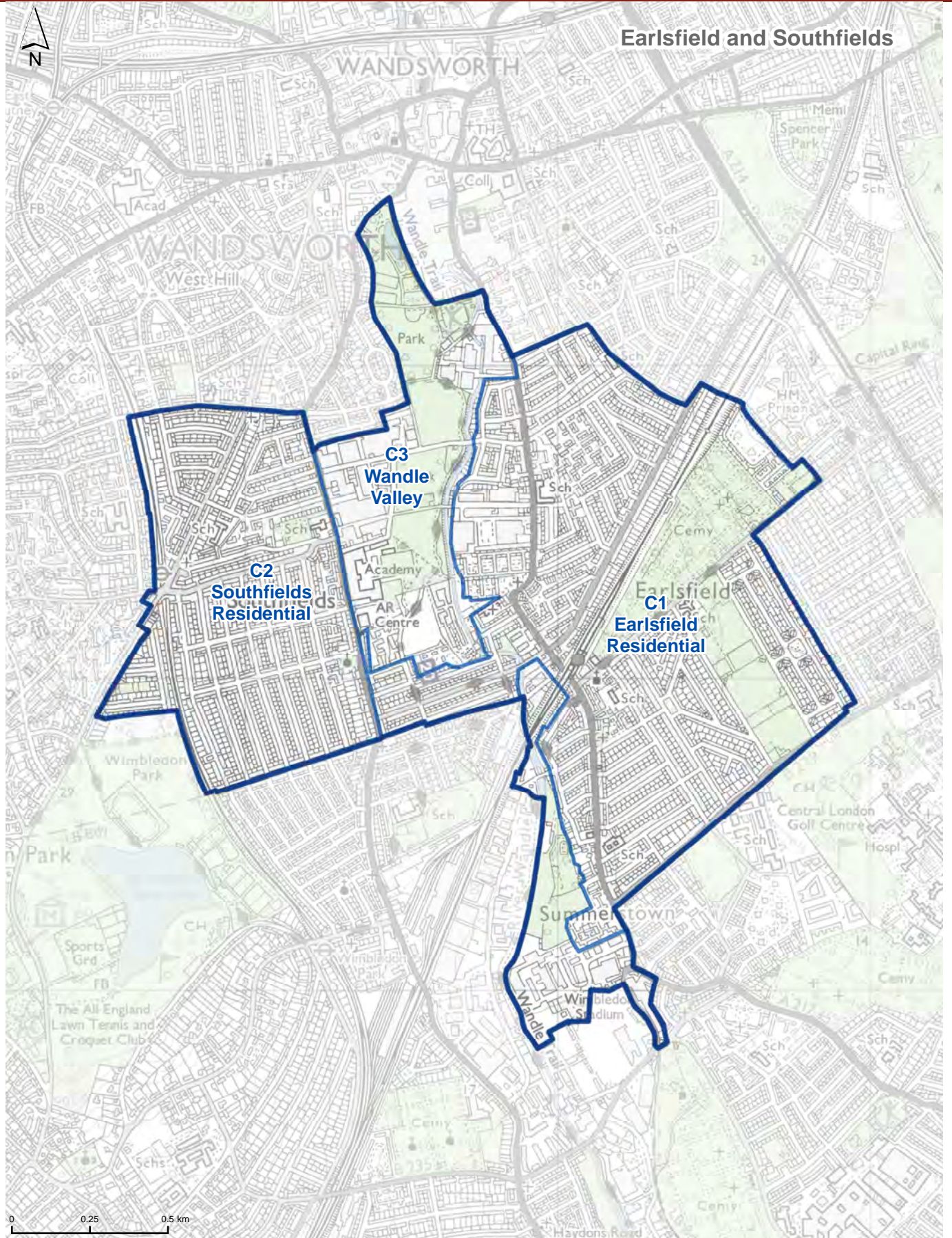


Fig. 100: Place C: Earlsfield and Southfields character areas plan

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# C1 Earlsfield Residential



Fig. 101: Period terraced housing on St Anne's Hill

## Key characteristics

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Earlsfield Residential sits between Wandsworth Town, Wandsworth Common and the Wandle Valley, with a gradual transition in character to Tooting, to the south. Its focus is Earlsfield centre and rail station on Garratt lane.

Earlsfield's origins lie along Garratt Lane, the historic route along the Wandle Valley, with villas and houses growing in close proximity to the industries, mills and factories which had developed next to the River Wandle. The rest of the area was still open fields. Terraced houses and shops grew around the station in the late 19<sup>th</sup> century, with growth booming in the 1920s and 30s, following the building of the Magdalen Park Estate.

The western side of the character area lies on flatter land in the former floodplain of the River Wandle (some of which is in flood zone 2), with topography rising up in the east towards Wandsworth Common: Magdalen Park Conservation Area lies on south west facing slopes, allowing extensive open views.

Green and spacious feeling, from tree-lined streets, low-rise houses, and open spaces. Although there is a mixture of building types, the sense of greenness and openness helps to knit the character of the area together. Particularly distinctive parts of the area include the broad tree-lined Magdalen Road which is complemented by the green frontage to Wandsworth

Cemetery (Fig. 104). In the terraced streets, green front gardens and open skies creates a quiet, family feeling and some sense of wildlife, while in estates there is a sense of community. Important to the sense of openness are the network of green open spaces, many of which are MOL, including Wandsworth Cemetery and the Openview open space.

The overriding character of the area is the Victorian terraced streets, although there is a mixture of other housing types including the planned estates of semi-detached houses in the Magdalen Park Conservation Area and pockets of infill post-war and modern estates. Terraces are typically red brick or yellow London stock. Infill estates occupy areas damaged by WWII bombing (in the north of the area), former industrial areas (by Wandle Way), and at the former workhouse (Watertown Road Estate). Houses are generally 2 storeys rising to 3 along main roads such as Earlsfield Road. Garratt Lane and in the modern estates south of Burntwood Lane, they rise up to 4 storey blocks. In the estates in the south of the area tower blocks rise up to 5 storeys with two 15 storey towers off Blackshaw Road.

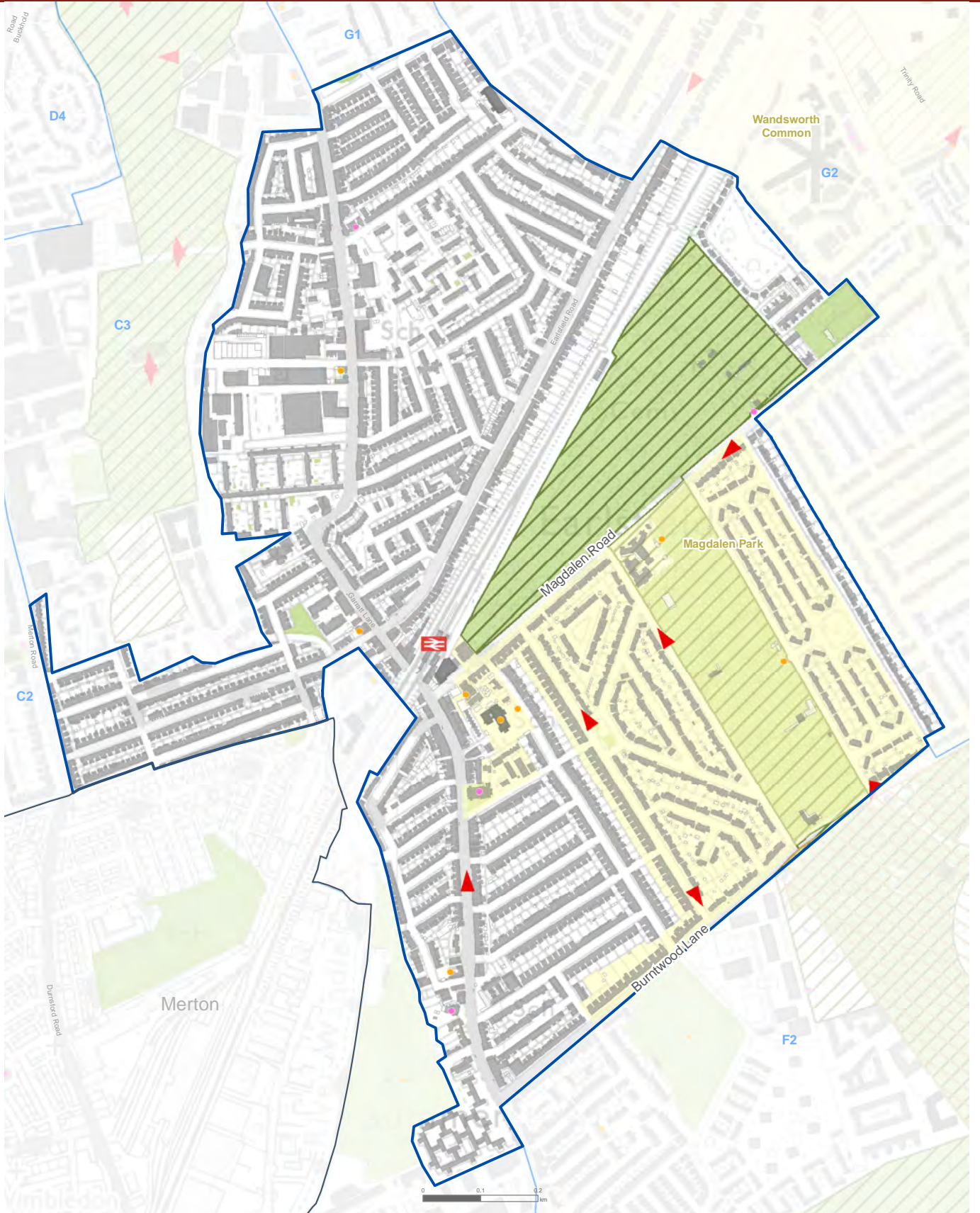
Magdalen Park Conservation Area covers a significant area in the centre of the area. Its special character derives from the coherence of the two formally laid-out, low-rise council estates separated by an open space with playing fields, allotments and a primary school. Houses are 2 storey terraces, semi-detached and maisonettes.

A number of individual buildings add interest and texture to the street scene, including landmarks of grander scale and high quality architecture. These include:

- St Andrews (grade II listed); St Mary, Wimbledon Road; and Earlsfield Baptist Church;
- Earlsfield House (grade II listed);
- Earlsfield Primary; Smallwood Junior (grade II listed); and Swaffield Primary School, St Anne's Hill;
- Earlsfield Public Library;
- Earlsfield police station (locally listed);
- Station Master House, Earlsfield Road;
- Wandle pub, forming a prominent feature at the junction with Garratt Lane/Earlsfield Road;
- Springfield Hospital (grade II listed).

Earlsfield local centre and the train station provide local amenities and relatively good accessibility.





**Fig. 102: C1 Earsfield Residential character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- The network of green infrastructure formed by the Metropolitan Open Land comprising the cemetery, the Openview open space and Garratt Park which link in to the wider strategic green infrastructure of the Wandle Valley and Wandsworth Common.
- Historic features and buildings including the period terraces and landmark buildings.
- The high quality, well-proportioned, excellently detailed and distinctive well-preserved character of Magdalen Road and the strong character of houses on Burntwood Lane in the Magdalen Park Conservation Area.
- The historic value of the St Clement Danes Almshouses, chapel and lodge (grade II listed), dated 1848, a group of 2 storey cottages in gothic revival style arranged around a garden.
- The facilities of Garratt Lane including independent shops and restaurants, the Tara Theatre, and the library on Magdalen Road.
- The quiet nature of the majority of the residential streets, promoting active travel but also in close proximity of fast travel to central London.
- The sense of openness, as well as specific views and vistas of interest, including:
  - to the spire of the Victorian era Church of St Andrew (grade II listed) from Garratt Lane;
  - trees frame the vista downhill along Magdalen Road, complemented by the high quality period terraces;
  - open views to the south west from the Magdalen Park Conservation Area;
  - from the main roads to Wandsworth Cemetery and Lambeth Cemetery (in Tooting Residential character area), with well-vegetated boundaries, attractive railings and landmark churches.



**Fig. 103:** Landmark Earlsfield House on Swaffield Road, grade II listed; built in 1902 as a workhouse



**Fig. 104:** Tree-lined Magdalen Road and attractive brick/ iron railings demarcate Wandsworth Cemetery



**Fig. 105:** View south along Garratt Lane, to the spire of the grade II listed Church of St Andrew

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Some deterioration of built fabric and poor maintenance of buildings e.g. windows, doors, front boundaries, although there are signs of refurbishment of properties. Loss of period features.
- Dropped kerbs and removal of boundary walls.
- Inconsistent use of materials detracts from character, including replacement and loss of period features;
- In the Openview Estate; plainness to the housing façades and a lack of detailing.
- Fragmented urban grain of the estates, including south of Burntwood Lane outside the character area, and Waverton Road Estate
- Frequent pockets of infill development have been occupied by estates of varying ages: from post-war through to 90s. These are generally inward looking, inactive frontages, poorly connected, illegible, lack character and have few footways e.g. Swanmore Court. The cumulative effect of the various estate developments creates a fragmented character.
- The traffic-dominated, busy Garratt Lane with associated issues of air quality.
- Discordant mixture of elements, some conflicting, which combined with busy traffic can feel chaotic.
- Loss of green (front and back gardens) and street trees which risks losing a key part of the character.

### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Low-rise estates; mid-rise estates;
- Low-rise flats;
- Mid-rise flats.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

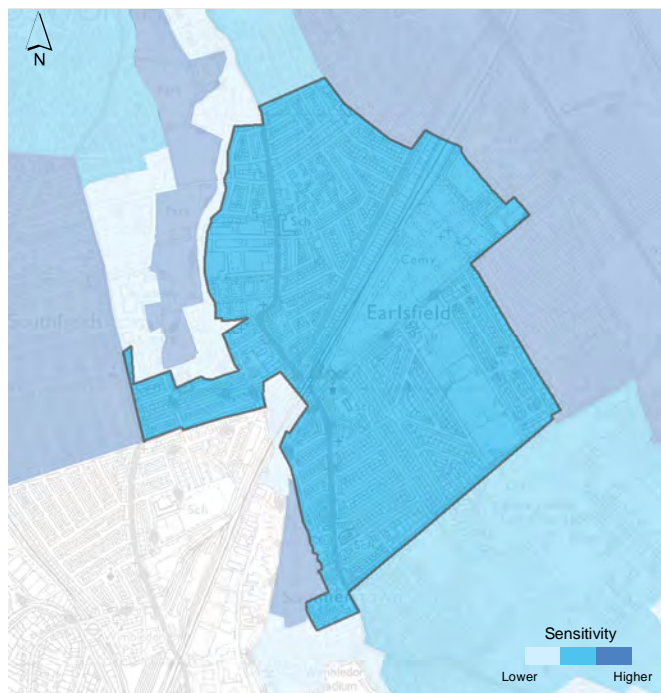


Fig. 106: C1 Earlsfield Residential sensitivity plan

Overall, Earlsfield Residential has a medium sensitivity to change. The area can accommodate targeted growth, as long as this respects:

- the scale and proportion of the period terraces;
- the setting and prominence of existing landmark buildings;
- the sense of openness and key views;
- the existing green spaces.

The **terraced streets** and the **Magdalen Park Conservation Area** are higher sensitivity due to their historic and cultural value, coherence and quality of architecture.

The estates could potentially accommodate intensification within lower density garages and smaller blocks.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve

Restore

**Improve**

Transform

Earlsfield Residential has a less coherent character than other areas in Wandsworth, as a result of the pockets of infill estates and the deterioration in condition of buildings in some areas. Therefore the strategy is to **improve** the character of the area, with future new development using historic references as well as taking opportunities to add new character and sense of place.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Maintain and encourage the use of traditional materials, and materials of period properties: red, yellow and grey brick are characteristic.
- Respect the scale and proportions of existing buildings and streets. Most streets would not be able to accommodate buildings over 3 storeys. Wider streets, corner plots and the areas towards the river/parks might be able to accommodate buildings exceeding the prevailing height. There are also opportunities for mid-rise buildings which are discussed in section 4.6.
- Enhance legibility through adding high quality landmarks along main roads and junctions, while respecting the prevailing building heights and street proportions. Ensure different developments are integrated together in terms of legibility and continuity of streetscape. Promote the 'essential Earlsfield' character which is family-orientated streets with modest proportions.
- Maintain boundaries, façades and front gardens.
- Increase street tree planting.
- Increase legibility, access and movement within estates and connectivity with their surroundings.
- Improve the streetscape along busy Garratt Lane
- Promote active frontages on key routes.
- Conserve, repair or reinstate original period features such as windows and boundary walls.
- Respect the settings, proportions, scale and details of landmark features and the conservation area character.

## C2 Southfields Residential



Fig. 107: Consistent Victorian terraces on Clonmore St

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Southfields is characterised by the coherent streets of Victorian and Edwardian terraces of similar design.

**A high degree of consistency of architectural style and street pattern** (urban grain), which creates a sense of local distinctiveness, cohesion and strong townscape character. There is strong historic character from the modest Victorian and Edwardian terrace houses of similar high quality design,

**Richly detailed townscape of high quality natural materials dominate**, including prominent gable frontages and intact front boundary walls. Original tiled patterned garden paths add interesting textures. Materials are predominantly red brick and painted render in pastel shades. Boundary walls are low red brick with occasional original timber gates. The continuity of and high quality architectural design are recognised through designation of the Wimbledon Park Conservation Area. The townscape is attractive and memorable both inside and outside the Conservation Area.

**A harmonious streetscape with balanced proportions:** consistency of scale, materials and the balance of proportions of building height to street width. Streets have a strong pattern and result in a consistent urban grain. Building heights are 2-3 storeys and taller buildings are usually landmarks - such as

schools or associated with the local centre. Streets are narrow and laid out with formal regularity.

**A well-managed streetscape with characteristic elements in good condition:** intact front boundaries, well-managed properties and maintained front gardens, public realm and houses of high quality materials - particularly brick - give a rich texture.

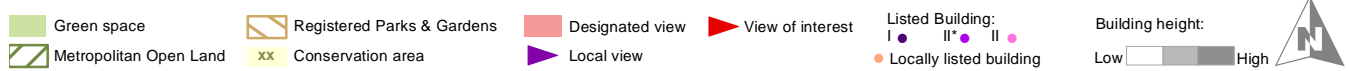
**Traditional, residential and suburban character with a relative sense of quiet, calm and lack of intrusion.** The area has a suburban gentility, and a feeling of safety and formality. Land use is predominantly residential, with a local centre around Southfields station but with few 'urban' influences.

**A green streetscape, as a result of consistent street trees, planted front gardens and well-maintained public realm.** Trees include pollarded limes. Wimbledon Park (Metropolitan Open Land) nearby is important for retaining the sense of greenness and openness as there is a relative lack of public open space in the area itself.

**The Southfields area developed in the late 19<sup>th</sup> and early 20<sup>th</sup> century, giving rise to an overall consistency of architecture and formal street pattern.** The area was formerly part of the wider Wimbledon Park estate, including Wimbledon Park and wider surrounding area. Parts of the estate around Southfields were sold for development in the 1870s and the railway and Southfields Station (locally listed) opened in 1889. Developed west of the floodplain of the River Wandle.

**The modest scale of the built form and the gradual rise in topography to the west, allows views across the area and to landmarks in adjacent character areas.** Landmarks include:

- the two red brick Churches - Church of St Michael and All Angels (listed grade II), built in 1897 in a gothic style; and the spire of St Barnabas Church, the current building of which dates from 1906;
- the factory of Frame Foods at 59 Standen Rd (grade II listed), now converted into flats which is a distinctive Art Nouveau style with green ceramic tiles ([Fig. 110](#));
- Riversdale School (grade II listed), dating from 1890, of red and yellow stock brick. A striking and dramatic roof-scape of cupolas, gables and tall chimneys form a visible landmark from some distance away (e.g. from King George's Park).



**Fig. 108:** C2 Southfields Residential character area plan

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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- High scenic quality and unified streetscene of the period houses, mature trees and front gardens.
- Green infrastructure, biodiversity and climate change resilience value provided by the street trees and well-planted front gardens.
- The heritage value and historic character of the terraced streets including the listed buildings and Wimbledon Park Road Conservation Area.
- Coronation Gardens public open space which is valued for its openness in the context of few public open spaces in the area and its historic character. It dates from 1902 to commemorate the coronation of King Edward VII, at which time it was cited as the borough's first park and "Wandsworth's new lungs".
- The local centre near Southfields underground station, where distinctive buildings at the junction contribute to a strong sense of place.
- Views and vistas of interest include:
  - A sweeping view along Replingham Road on higher ground, to the sweeping Southfields terraced streets including Wimbledon Park Road Conservation Area;
  - Individual views along the gridded streets, by virtue of the consistent architectural style.
- Easy access to King George's Park, Wandsworth and Wimbledon Parks and the river.
- Independent shops including those along Replingham Road and the nearby Southfields Library.
- Local planting initiatives including front gardens and community planting around tree bases.
- An overall village atmosphere within the local area, with a calm atmosphere.



**Fig. 109:** Distinct tree lined, grid pattern street at Elborough St



**Fig. 110:** The distinctive ceramic tiled factory of Frame Foods (grade II listed), now converted into flats



**Fig. 111:** Shops in the ground floor of terraces form a local shopping parade at Replingham Rd

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Front gardens which are paved over or are missing boundary walls detract from the coherent streetscape.
- Where buildings have been unsympathetically altered, or occasional infill plots this detracts locally from the area's overall coherence, such as boundaries, front elevations and roofs.
- Side elevations of corner plot buildings and their public realm are visually prominent, and where they have been unsympathetically altered this detracts from the overall character - e.g. the asphalt in front 130 Wimbledon Park Rd in the Wimbledon Park Road Conservation Area.
- On-street parking detracts from the streetscene (but is preferable to parking in front gardens).
- Traffic affects the village character in some parts of the area, with accompanying issues of air quality.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

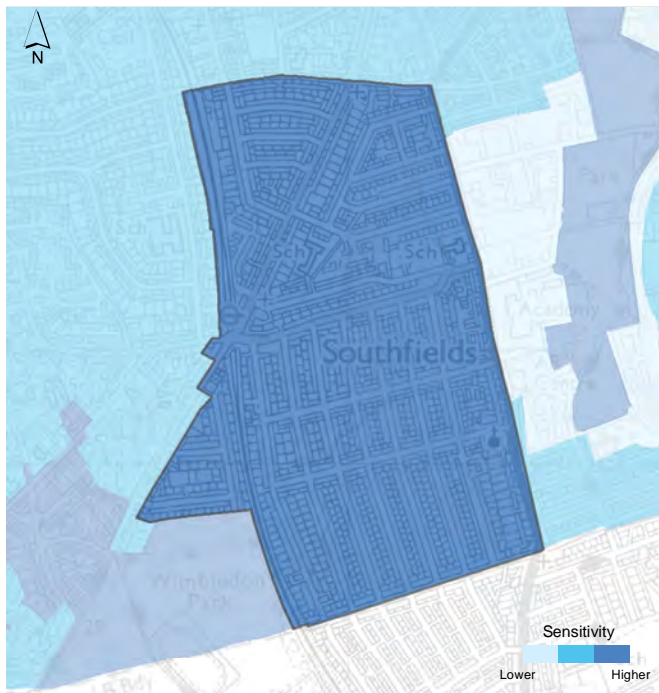


Fig. 112: C2 Southfields Residential sensitivity plan

Overall, Southfields Residential has a high sensitivity to change, and extensive change is not appropriate.

There may, however, be small areas of lower sensitivity where the townscape is less intact and does not reflect the character described in the key characteristics.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

**Conserve**

Restore

Improve

Transform

The existing character of the area is strong and cohesive; therefore the strategy is to **conserve** and enhance the key characteristics.

## Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Conserve, repair or reinstate original period features such as windows, boundary walls, traditional materials - primarily brick, front doors (e.g. remove porch infills) and tiled paths to maintain the character.
- Conserve and enhance street trees and planted front gardens with shrubs, small trees and hedges with a focus on biodiversity improvements.
- Respect and enhance the interesting buildings at Southfields local centre; ensure any new development is of sympathetic scale, design and proportions, particularly in relation to the conservation area.
- Conserve the consistency and suburban quality of the area, which is sensitive to new development and may be difficult to integrate. If new development is proposed in this area, ensure special attention to the choice of materials, scale, style and massing to reflect the rich townscape.
- Respect the modest scale and proportions of existing buildings and streets. Most streets would not accommodate buildings over 2-3 storeys unless on a primary route or in the local centre.
- Conserve the skyline, which is sensitive to tall elements including in other character areas due to the modest scale of the built form in the area.
- Enhance the functionality and biodiversity of Coronation Gardens.
- Preserve and reinstate green front gardens.
- No capacity for buildings of 5 storeys and over has been identified.



**Fig. 113:** Sports fields in the southern part of King George's Park, with the Academy behind

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Wandle Valley consists of two adjacent areas, separated by the London Borough of Merton, into which the character area extends further west and south. The character area is defined by the course of the River Wandle and its valley, comprising open space and industrial uses.

**The flat, shallow valley of the River Wandle and its former floodplain.** Much of the land either side of the river is within Environment Agency flood zones 2 and 3 and almost the whole of the character area is within a flood alert zone.

**Industry and manufacturing are a historic feature** of the character area, with the river an important source of water and power. In the post medieval period the main channel in this area was effectively a succession of level pools between mills. From the 17<sup>th</sup> century the area was a focus of industry and manufacturing, with mills and factories developing along the river. Industries included production and processing of iron, gunpowder, leather, linen and copper. Paper and parchment making was also prominent: historic maps show the Royal Paper Mills (which produced high quality playing cards), a chemical manure factory, a card and pasteboard factory and fireworks. From the 19<sup>th</sup> century settlement developed around the edges, with terraced streets of worker's dwellings around Bendon Valley, as well as a public house and school.

After WWII the middle of King George's Park was used as a site for prefabricated houses, providing homes for about 400 people.

**King George's Park has a municipal character.** It is a large public park, and a valued space for leisure and recreation. Facilities include tennis courts, a skatepark and playing fields to the south of Kimber Road. There is also a lake, cherry orchard and playground. The park is quite manicured with often little sense of the river being there, although it runs along the park's edge. The park was designed by Stephen Percival Cane and opened in 1923 by King George V. The park included a 'leisure' lake, an open air pool was built in 1938 (closed in 1993), a bandstand, menagerie and playground. The lake, water fountain, tennis courts and formal park design in the north of the park are all remnants of the original 1920s design.

**Garratt Park** was laid out as 'a recreation ground for children' in 1906 on the former garden of Garratt Mill.

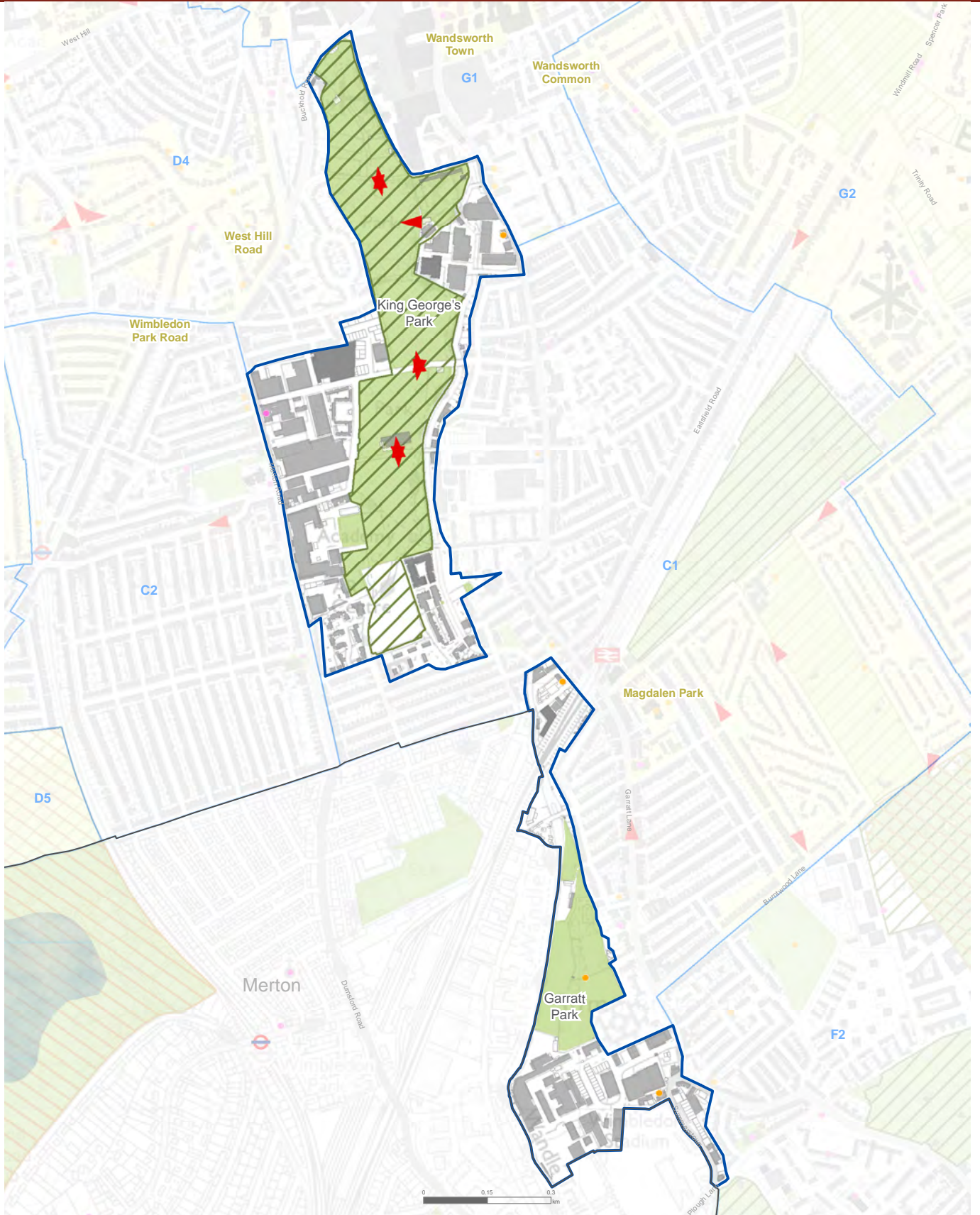
**The Wandle Trail** runs alongside the river, but diverts away from it around the southern end of King George's Park, where it follows residential roads within the Southfields and Earlsfield Residential character areas. The parks and open spaces in the character area form part of the Wandle Valley Regional Park.

**Built form on the periphery of the green spaces** is prominent, and where trees are lacking at the perimeters, becomes dominant. It consists primarily of large scale industrial units, education and retail units, transitioning to residential towers in Wandsworth Town Centre. These are noticeable, but mature trees help to integrate them into the landscape. Average building heights are around 1-4 storeys, increasing to 6-10m at the Academy. Few historic features of the former mills remain (e.g. Flock Mill), which have been converted to residential and commercial uses. Bendon Valley is designated in the Local Plan as an Economic Use Intensification Area.

**The river does not have a strong presence.** This is largely due to the municipal character of the park including ornamental planting, and a lack of indication of the watercourse by typical riverside trees such as willow, alder or poplar. Extensive areas of grass, mowed up to the river's channelised banks also contributes.

**Views are open and expansive** rather than interesting. Mature trees help to soften views of adjacent industrial units.





**Fig. 114:** C3 Wandle Valley character area plan  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- The recreation and community value provided by King George's Park, well-used and loved by the local community for formal and informal sports, leisure and relaxation. Flexible and accessible paths and spaces in the park allow for a range of uses, and as an off-road route to the town centre.
- The value of the area as part of the wider Wandle Valley Regional Park.
- The sense of openness provided by the parks (designated Metropolitan Open Land), providing relief from surrounding development. Garratt Park is a locally protected open space.
- Features of historic and aesthetic interest in King George's Park including Art Deco style gate piers and railings, the rose garden, the lake, memorial trees and the ornamental bridge.
- The biodiversity value of King George's Park, designated a Site of Importance for Nature

- Conservation (SINC) of borough grade II importance; the neutral grassland, woodland and standing water priority habitats; and the ecological value of the River Wandle, a SINC of borough grade I importance.
- The biodiversity, heritage and visual amenity value provided by the mature trees, including veteran trees. Mature trees provide an important screening of large buildings on the park's periphery.
- The River Wandle and the Wandle Valley Trail, valued for leisure and recreation, providing green links within the borough.
- Potential archaeological value indicated by its inclusion in an Archaeological priority Area.
- Views, including wide open views across the park and to features of interest within the park; of the river from the bridges on Kimber, Duntshill and Penwith Roads; and west towards the church spire of Holy Trinity Church in East Putney.



**Fig. 115:** The spire of the Holy Trinity Church in East Putney is visible over the industrial unit in the park



**Fig. 116:** View along the River Wandle from the bridge on Kimber Road.



**Fig. 117:** Mature trees in King George's Park help to soften tall buildings in Wandsworth Town Centre into the landscape

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- There is little sense of the river through much of the character area, particularly where it is culverted.
- The urbanised part of the Wandle near Wandsworth Town, where it has high hard edges and is faced by backs of buildings.
- The open spaces west of the Henry Prince estate are separated from the river with high walls with little relationship to the river.
- In industrial areas, units or parking back onto the river (e.g. Bendon Valley and Lydden Road, Penwith Road area) with limited space on the riverbanks for wildlife, and no public access.
- Wildlife value and historic character of the river could be enhanced.
- The hard, grey character of the industrial areas which are generic, and blank façades have little visual interest. They detract from the scenic qualities of the park.
- Some of the ad-hoc utilitarian buildings and roads have created visual intrusion and fragmentation, particularly within King George's Park.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Large floor plate retail/industrial.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

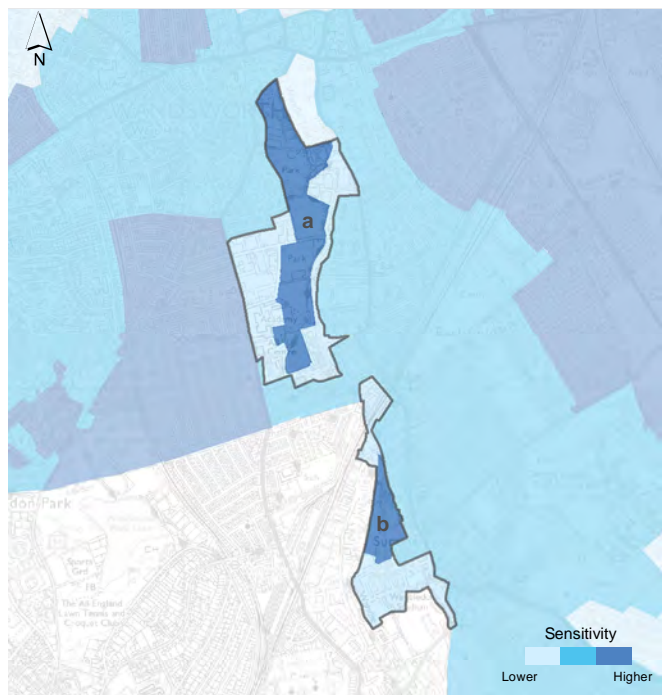


Fig. 118: C3 Wandle Valley sensitivity plan

Overall, Wandle Valley has a low sensitivity, with potential for positive change which enhances the character of the area.

The character area has the potential to incorporate change and new development as long as this:

- does not reduce the quantity or quality of open space;
- respects the setting of the parks;
- respects the recreation and leisure function of the character area.

The parks themselves, sub areas (a) and (b) have a high sensitivity to new development as it may affect their valued qualities.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve

Restore

**Improve**

Transform

Wandle Valley at present has indistinct character which could be improved through management and future planning. This includes developing a relationship with the river through access, building design and landscape interventions.

The valued features of the existing parks, river and Wandle Valley Trail should be protected and enhanced.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Refer to [King George's Park Management and Maintenance Plan](#), which sets out actions to conserve and enhance the park.
- In King George's Park: enhance local distinctiveness using its relationship with the river and historic industry; enhance biodiversity through less intensive management, allowing more natural river banks with off-line wetlands; re-engage with the river by lowering barriers and paths to make the water accessible.
- Plant appropriate riverside tree species along the river to make it more prominent in the landscape.
- Ensure developments offer public access and provide positive frontage to the riverside.
- Respect the small scale of the river corridor in the siting of any taller buildings.
- Enforce the river valley character in adjoining urban spaces by planting species such as poplar, willow, flag iris and marsh marigold.
- Improve integration of footpaths and green spaces. Ensure that any redevelopment of the industrial units provide riverside access.
- Conserve mature trees that soften views of industrial units.
- Better design industrial units to provide a stronger sense of place for the park and reduce the dominance of their massing through careful attention to façades and roof lines, incorporation of trees, and create active frontages and appropriate proportions along streets.

# D Putney



Fig. 119: Putney riverside

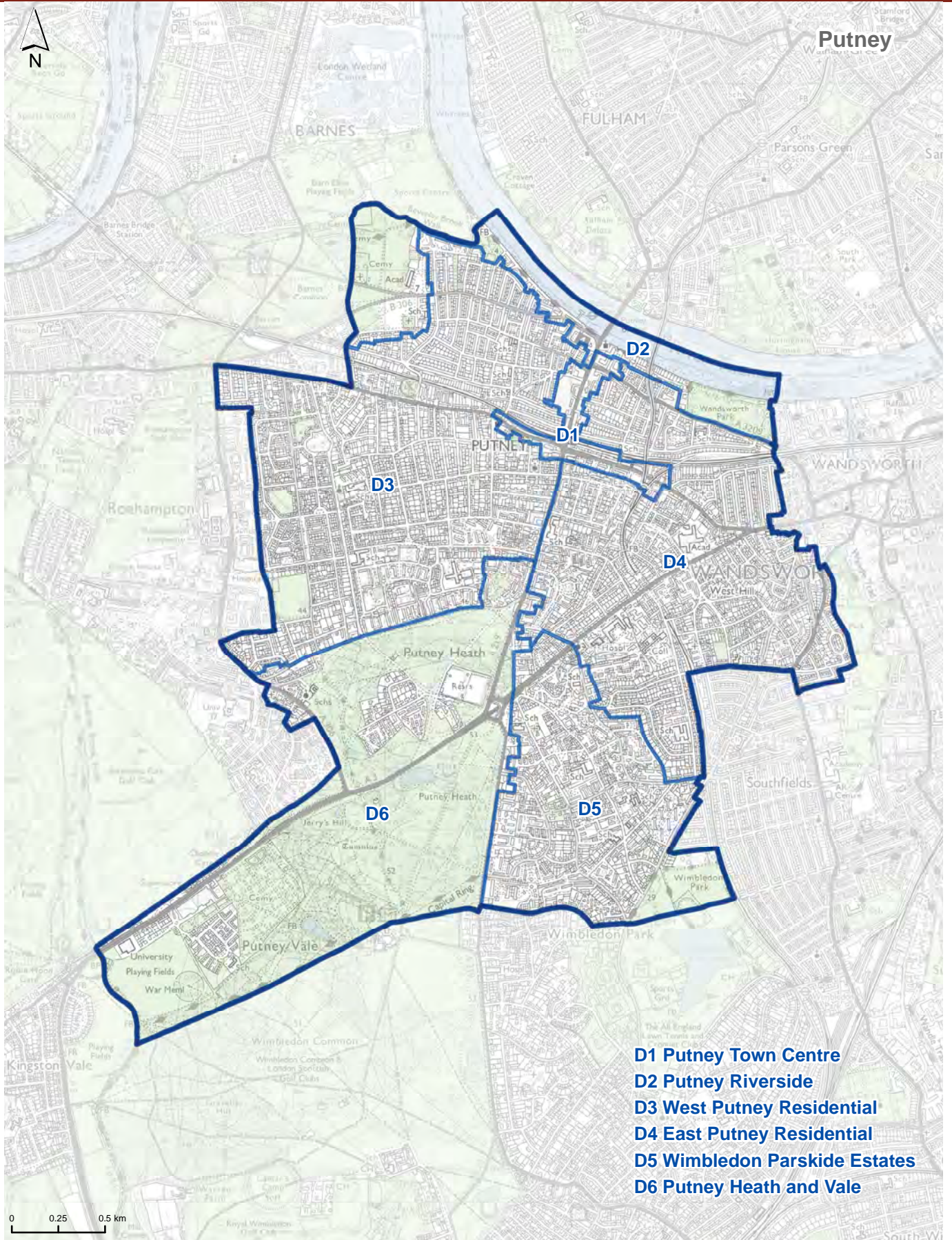
## *Summary*

Putney is characterised by its river-front location, including Putney Bridge and the Putney Embankment Conservation Area, reflecting its historic importance as a river crossing. It is also characterised by its residential tree-lined streets, many of which are recognised for their special character through designation as conservation areas. The land rises gradually away from the Thames up Putney High Street towards Putney Heath.

Putney is one of the oldest settlements in Wandsworth, with the bridge one of the earliest crossing points over the Thames and a number of villas built along main roads out of London, such as West Hill, from the early 18<sup>th</sup> century. The south of the area is dominated by the 160 hectare Putney Heath wooded open space.

## *Key changes and trends*

- Planned change in Putney Town Centre is set out in the Area Strategy in the Local Plan.
- Putney Town Centre and parts of Upper Richmond Road are earmarked for sensitive redevelopment where possible to enhance the overall character of the centre.
- The Thames Tideway Tunnel site at Putney Embankment is creating a new area of riverside public realm close to the listed Putney Bridge;
- Much of the wider Putney residential areas are likely to remain largely unchanged with just some ad hoc development of small sites when they become available.



**Fig. 120:** Place D: Putney character areas plan

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# D1 Putney Town Centre



**Fig. 121:** Busy traffic, varied Shopfronts and remaining architectural details at upper levels along Putney High Street

## *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Putney Town Centre is focussed along historic Putney High Street and Upper Richmond Road, offering a range of business, retail and leisure uses. Some of the Victorian and Georgian built fabric remains in upper storeys.

**Putney developed in the early 1300s as a strategically important river crossing, reinforced in 1729 with the completion of the first permanent bridge.** The town centre was densely developed by the late 19<sup>th</sup> century and over time this Victorian and Georgian fabric has been eroded with new developments. Regeneration in the town centre is planned, including the area in the Putney Wharf focal point (some of which is located within the Putney Riverside character area).

The town centre encompasses some small sections of conservation areas, including:

- **West Putney Conservation Area:** the section fronting onto Upper Richmond Road characterised by variety of building height, mass and density with a diversity of uses.
- **Charlwood Road / Lifford Street Conservation Area:** a small area of hardstanding at the rear of the shopping centre.
- **Oxford Road Conservation Area:** period residential properties along Burstock, Montserrat, Disraeli and Upper Richmond Roads.

**Consistently modest proportions along Putney High Street**, which is framed by 3-4 storey period terraces with shops at ground floor and generally well-maintained upper storeys. The retention of architectural details illustrates that the underlying historic fabric remains in place and indicates good potential for the restoration of a quality streetscape.

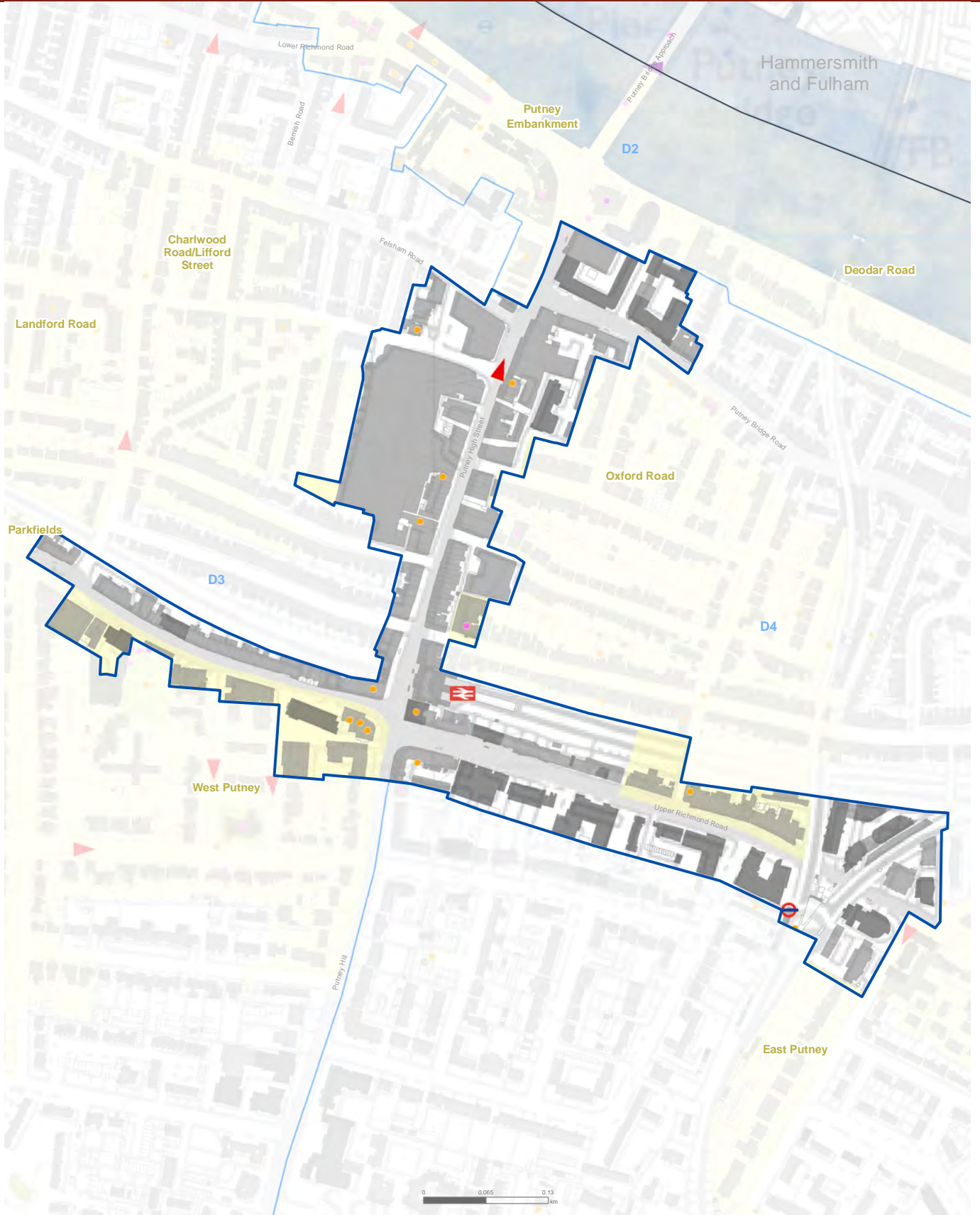
**Landmark buildings** including at the junction between Putney High Street and Upper Richmond Road with the 5 storey Zeeta House.

**The character of Upper Richmond Road changes at the junction with the High Street.** To the east, the town centre is dominated by 8-9 storey 20<sup>th</sup> century blocks with large floorplates and little address to the street, particularly on the south side. This is coupled with the retention of a stretch of period 2-3 storey terraces which creates an unbalanced streetscape. The western section of the road is more compatible with the prevailing urban grain of the surrounding residential area, with commercial uses occupying the lower floors of 4 storey period buildings interspersed among taller landmark elements including church spires.

**Frequent views along side streets to the adjacent residential character areas** of period 2-3 storey properties in East and West Putney Residential. There is a gentle transition in character with a gradual stepping down in scale of built form. The exception to this is the Putney Exchange Shopping Centre which is a large area west of the High Street, formerly occupied by industrial premises. The shopping centre is a notable distraction from the otherwise consistent historic frontages along the length of the road.

The railways, so important in the later growth of Putney, are not a strong influence on the character of the town centre, with the entrance to Putney station not very legible along the High Street, and East Putney station sitting at the edge of the character area. However, their presence gives the area **excellent public transport accessibility**.

**An urban environment dominated by heavy traffic.** Shopfronts often present a garish, poorly maintained and inconsistent frontage. A lack of planting and narrow pavements create a transient space. As a historic centre of small town it is lacking in a distinctiveness and feels quite separate from the scenic and natural riverside environment.



**Fig. 122: D1 Putney Town Centre character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Intact Victorian 4 storey terraces lining the majority of Putney High Street with well preserved and maintained architectural details. The buildings create a distinct recognisable proportion to the overall street scene.
- Landmark buildings including the 5 storey Zeeta House on the north east corner of the junction between Putney High Street and Upper Richmond Road; and church buildings immediately outside the character area such as Putney Methodist Church.
- Together with other buildings at the southern junction of Putney High Street, Zeeta House creates a distinct gateway to the town centre.
- Views north along Putney High Street towards the river, listed bridge and St Mary's Church.
- Proximity to the historic riverside frontage and Thames Path, with wider pavements where provided.
- Long-distance vistas towards this character area

from outside the borough across the river, including from Fulham Palace Scheduled Monument. Any development proposals should be assessed in the context of this vista.



**Fig. 123:** Commercial buildings with large floor plates along Upper Richmond Road



**Fig. 124:** New successful infill building in an art deco style at a junction along Putney High Street



**Fig. 125:** Landmark Zeeta House at the junction of Putney High Street and Upper Richmond Road

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Mixed management and maintenance of Shopfrontages along Putney High Street.
- Absence of planting along Putney High Street and much of Upper Richmond Road, except for some mature trees, mostly west of the High Street junction.
- Putney Exchange shopping centre detracts from the activity and proportions of the High Street, which is otherwise mostly framed by 4 storey period terraces with smaller scale shop units in the ground floor.
- Heavy traffic, combined with narrow pavements creates a busy and noisy environment along the High Street. The heavy traffic continues along Upper Richmond Road which makes it a car-dominant environment with few pedestrian crossings.
- Taller post-war commercial development along Upper Richmond Road is out of proportion with the surrounding period residential properties, including those that remain along the road itself. They also present largely inactive frontages at ground floor, detracting from the quality of the street environment.
- The taller blocks of Jubilee House and Putney Wharf Tower (within D2 Putney Riverside) detract from the consistency and character of the high street.
- Putney train station is not very legible along the High Street, with minimal positive address to the street.
- Poor air quality and high noise levels diminish the quality of the townscape for pedestrians.



## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces; Shopfront terraces;
- Mid-rise flats;
- High-rise flats;
- Town centre retail;
- Large floor plate retail/industrial;
- Mid and high-rise mixed use.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

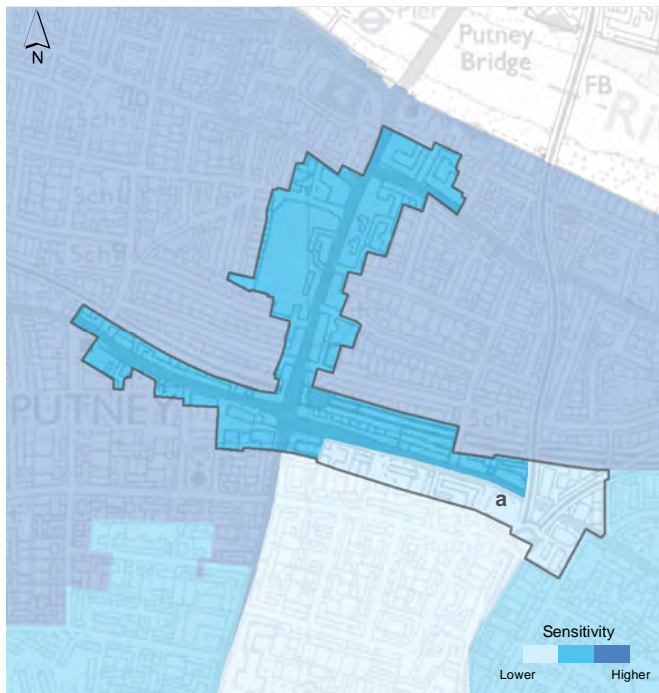


Fig. 126: D1 Putney Town Centre sensitivity plan

Overall, Putney Town Centre has a medium sensitivity to change with the potential for targeted growth.

The character area has the potential to incorporate change and new development, as long as this respects:

- the proportion, scale and material quality of the remaining period buildings. This includes opportunities for taller elements at junctions and as part of a legibility strategy for the centre;
- the existing landmark buildings, preserving their setting and contribution to the skyline; and
- the function of the streetscapes as a shopping and leisure destination.

Due to the presence of more inappropriate out of proportion modern developments, Upper Richmond Road, sub area (a) has a lower sensitivity to change than Putney High Street.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve	Restore	Improve	Transform
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The underlying historic built fabric, the human-scale proportions and proximity to the river give Putney Town Centre a sense of place, although its character has been negatively affected by traffic and unsympathetic development.

Therefore, the strategy is to **restore** the existing character by improving public realm, the maintenance of remaining characteristic features and ensure future growth reinforces existing character.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Establish guidelines and standards for ground floor Shopfrontages to ensure they contribute to the overall quality of the street scene.
- Ensure good maintenance of building façades and public realm.
- Seek to widen pavements and introduce further traffic management to reduce the dominance of cars.
- Establish more green infrastructure, particularly street trees, within the public realm.
- Identify opportunities to create landmarks across the town centre that respect existing proportions but improve the legibility of the area, particularly in relation to the two stations.
- Creation of public areas for dwelling rather than just narrow transient spaces.
- Protecting the proportions of period buildings along the majority of the High Street, while recognising opportunities for taller elements at key junctions. Taller elements should have a design elegance and quality that marked them as landmarks rather than just an attempt to create increased density.
- Protect vistas, where available, north towards the river and from outside the borough, particularly from Fulham Palace Scheduled Monument.
- Protect the setting and proportion of existing landmark buildings and the character of the conservation areas.

## D2 Putney Riverside



**Fig. 127:** View downstream from Putney Embankment framed by the distinctive avenue of mature London plane trees

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Putney Riverside is defined by the distinct sweeping bend of the River Thames. The area is punctuated by Barn Elms and Wandsworth Park, which, together with mature trees on the opposite bank, create a green setting. The public spaces are busy with leisure users on the river, in the parks and along the Thames Path.

**Famous home to the University Boat Race**, (first staged in 1829), Putney developed as the shortest route from central London to the south west of England. The riverside developed further upon construction of the first Putney Bridge in 1729, representing the only crossing over the Thames between Kingston and London Bridge.

**Much of the area sits within Putney Embankment Conservation Area, recognised for the importance of its 19<sup>th</sup> century boathouses.** The conservation area contains some of the oldest surviving buildings in Putney, including St Mary's Church. The Victorian, Edwardian and Georgian properties are set back from the generous Putney Embankment characterised by mature London plane trees and numerous slipways giving access to the river.

Downstream of Putney Bridge, the **Deodar Road Conservation Area creates a green frontage formed by private gardens to Victorian houses** backing onto the Thames. This creates a distinct suburban feel

immediately behind the busy Putney High Street, with the special interest created by the houses' relationship to the river. Heavy bombing during the war created distinct gaps in these houses still visible today.

Views upstream follow the **sweeping avenue of mature London plane trees**, with the green frontage continuing into Leader's Gardens and beyond to Barn Elms playing fields and the London Wetland Centre in the neighbouring borough of Richmond. The character area extends inland to encompass the edge of Putney Common. The Beverley Brook creates a dense belt of vegetation along the borough boundary.

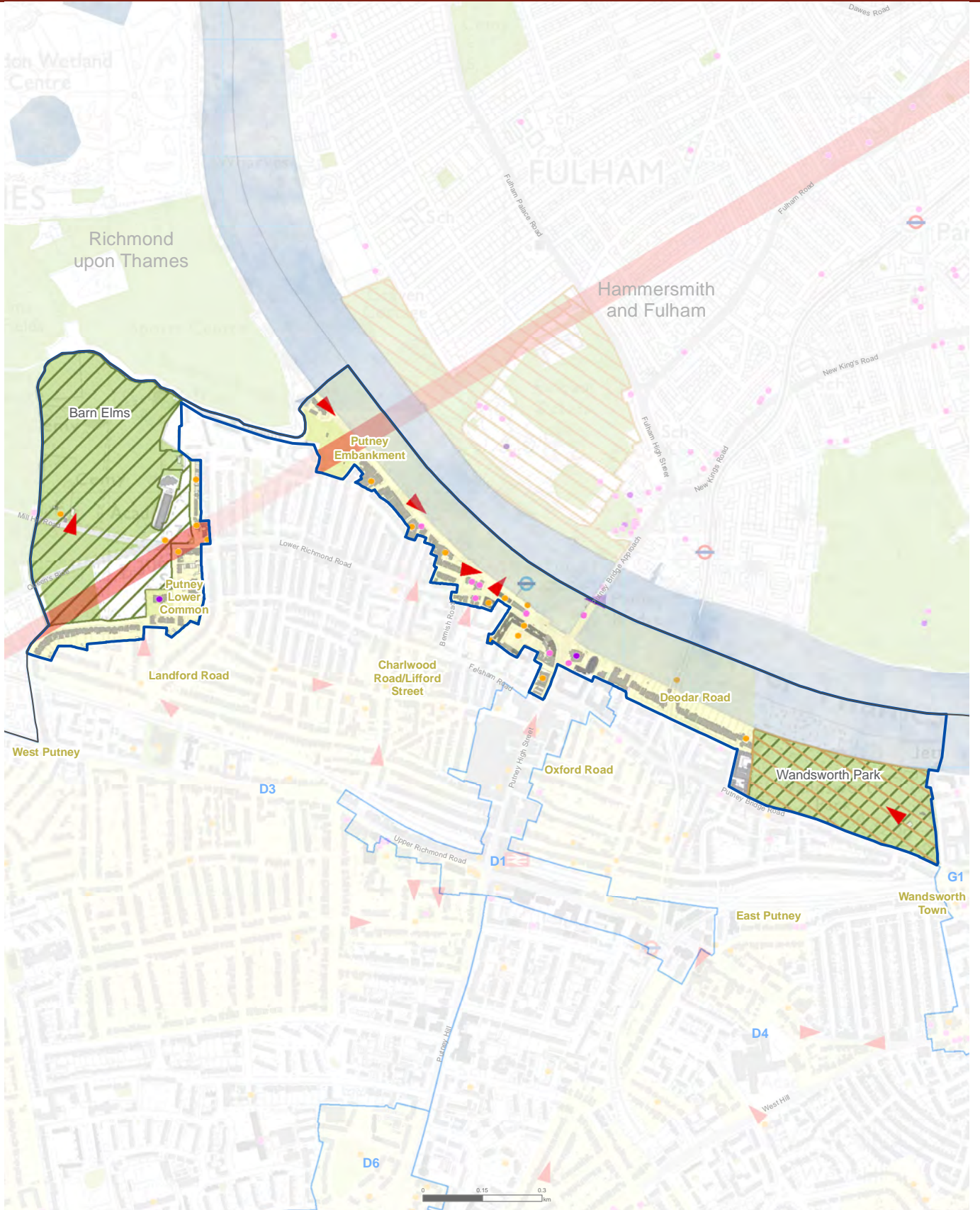
The river is relatively narrow on this stretch, with the mature trees and open spaces of the opposite bank **creating a green and consistent skyline**. Looking towards central London, Wandsworth Park marks a distinct change to the more urban and denser frontage of Wandsworth Town and Battersea.

Overall, the large number of pedestrians and cyclists lend a **promenade feel to the riverside, reinforced by consistent use of natural materials and well-considered street furniture**. Although busy, the river and mature vegetation lends a feeling of calm and tranquillity creating a strong perception of nature.

Despite a number of private properties fronting onto the Embankment, the **active frontages of the boathouses** and the open railings to the river and the parks creates a **feeling of openness** and engagement with the Thames.

The townscape, buildings (including some grade I listed properties) and open spaces are all well maintained, with **key characteristic historic elements present and in good condition**. Although some elements along Putney Embankment are in need of repair, they do not detract from the overall consistent quality of the riverside experience.

Buildings along the frontage vary in height from 2-4 storeys across the majority of the area and up to 5-6 storey mansion blocks close to Putney Bridge. **Church spires create distinctive skyline landmarks**, including the 15<sup>th</sup> century St Mary's Church which forms one of a landmark pair of distinctive towers either end of the grade II listed Putney Bridge; and Putney Lower Common Cemetery Chapel and All Saints' Putney close to the boundary with Richmond. The Putney Wharf Tower 21<sup>st</sup> century residential re-development (18 storeys) is highly visible on the skyline and in the backdrop of St Mary's Church, negatively affecting its setting.



**Fig. 128: D2 Putney Riverside character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- High scenic quality of the river, Putney Embankment and the riverside open spaces and mature avenue of London plane trees. The green skyline of the opposite bank adds to the value of this stretch of the river on the setting of this character area.
- The Thames Path and adjacent historic boat houses with slipways are highly valued leisure facilities.
- Memorable historic buildings including listed mansion blocks at Kenilworth Court and University Mansions, the Georgian Winchester House, and St Mary's Church (c. 1450) close to Putney Bridge.
- The grade II listed Putney Bridge, one of the oldest crossings of the Thames, and the locally listed Putney railway bridge, which frame views up and down the river.
- The proximity of amenities including bars, cafés, live music and open spaces
- Alongside the general feeling of openness, specific

views and vistas of interest include:

- upstream from Putney Bridge along the sweeping mature tree avenue of Putney Embankment, specifically noted in the Local Views SPD for its significance as marking the start of the University Boat Race;
- the protected viewing corridor from King Henry VIII's Mound in Richmond towards St Paul's Cathedral, which crosses the north west of the character area;
- sequential views downstream and across the river from Putney Embankment;
- glimpses towards the river and Embankment from perpendicular streets such as Thames Place, Waterman Street and Rotherwood Road; and
- views within and across the large open spaces including Putney Lower Common and Wandsworth Park.



**Fig. 129:** Kenilworth Court mansion blocks with views across Putney Bridge and the river



**Fig. 130:** The open expanses of Putney Lower Common



**Fig. 131:** Putney Wharf Tower dominating the skyline behind St Mary's Church

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Some of the elements within the public realm are in need of maintenance to preserve the distinct and consistent character of Putney Embankment (e.g. riverside railings and other street furniture).
- The setting of the most significant heritage assets (St Mary's Church and Putney Bridge) are adversely affected by the busy junction with Putney High Street and the modern Putney Wharf residential tower.
- The connection of the riverside with the High Street could be enhanced as the current change is quite abrupt and disconnected.
- Fulham FC stadium affects the riverside setting of the character area, detracting from the public green frontage of the rest of the northern bank.
- While important to the historic character of the area, the private houses and gardens of Deodar Road Conservation Area mean there is no public access along the riverside from the former Putney Wharf to Wandsworth Park meaning leisure walkers and cyclists need to negotiate the busy junction at Putney High Street and use local roads to continue their journey.

### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Mansion blocks;
- High-rise flats;
- Community/leisure.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

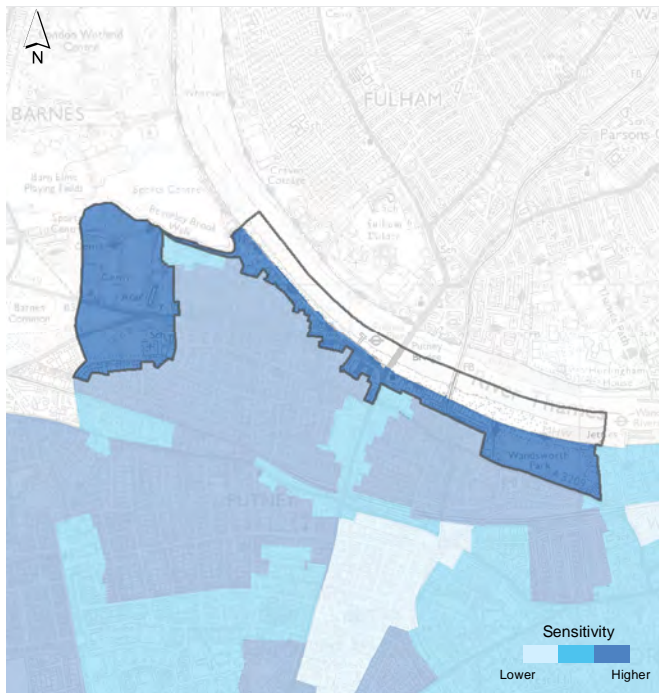


Fig. 132: D2 Putney Riverside sensitivity plan

Overall, Putney Riverside has a high sensitivity to change, and extensive change is not appropriate.

There may, however, be small areas of lower sensitivity where the townscape is less intact and does not reflect the positive character described in the key characteristics.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.



The existing character of Putney Riverside is strong, with a distinctive sense of place and high quality townscape. Therefore the strategy is to **conserve** the character, elements and features whilst enhancing existing features where appropriate.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Preserve the openness of the riverside and public green spaces by maintaining existing railing treatments and resisting development which would affect this overall perception.
- Respect the scale and proportions of the existing period buildings and streetscape which is fundamental to the character of the area.
- Protect the openness and framing of vistas towards the river, along Putney Embankment and across to the opposite bank.
- Maximise use of natural materials to integrate with the quality and natural feel of the existing townscape - including stone, timber, period brickwork and planting.
- Ensure good maintenance of façades, particularly where they present an active frontage to the Thames Path; and the public realm particularly including locally distinctive street furniture.
- Ensure new development along the river contributes to the valued leisure functions, including water uses, walking and cycling. Protect the public and pedestrian nature of the riverside.
- Protect the integrity and character of the conservation areas within and adjacent.
- Protect views from outside the borough, including from Fulham Palace Scheduled Monument.
- No capacity for buildings of 5 storeys and over has been identified.

## D3 West Putney Residential



**Fig. 133:** A detached Edwardian house on Woodthorpe Street in typical red brick with intact front boundary. Street trees add a sense of greenness.

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

West Putney Residential is characterised by its tree-lined streets, mature gardens and consistent pattern of period houses. There are 7 conservation areas, reflecting its high quality townscape.

**There is an overall sense of natural greenness** from mature front and back gardens and street trees which contribute significantly to the area's green infrastructure. In the south of the area green cover increases, reflecting the transition away from the town centre. High quality green spaces are primarily associated with conservation areas: particularly Dover House Estate Conservation Area. **The large areas of Metropolitan Open Land nearby are important for retaining the sense of openness** - Putney Heath, Roehampton and Putney Lower Common - as there is a relative lack of public open space in the area itself.

**The north east of the area contains some of the oldest houses.** The area grew following the opening of Putney Bridge in 1729, expanding west from the town centre. The 1896 map shows development around Lower Richmond Rd, Charlwood Rd and Felsham Rd; Upper Richmond Rd and the railway; and large detached houses opposite Putney Heath and extending north down Putney Hill. Meanwhile the north west of the area was still occupied by fields and farms.

**A well-managed streetscape with characteristic elements in good condition:** intact boundaries, well-

maintained front gardens, public realm and high quality materials - particularly brick - give a rich texture.

**High degree of consistency of architectural style, balanced proportions and street pattern** (urban grain), which creates a sense of local distinctiveness and strong townscape character. There is strong historic character from the large Victorian and Edwardian houses in terraced, semi-detached and detached streets, and individually attractive houses e.g. at junctions. Their rich detailing, consistency of scale, materials and the balance of proportions of building height to street width (both inside and outside of Conservation Areas), creates an attractive townscape. Building heights are generally 2-3 storeys. Even where buildings are not of the characteristic style, the consistency of proportion, mature street trees and green settings behind brick walls integrates more modern buildings (e.g. Cheval Court).

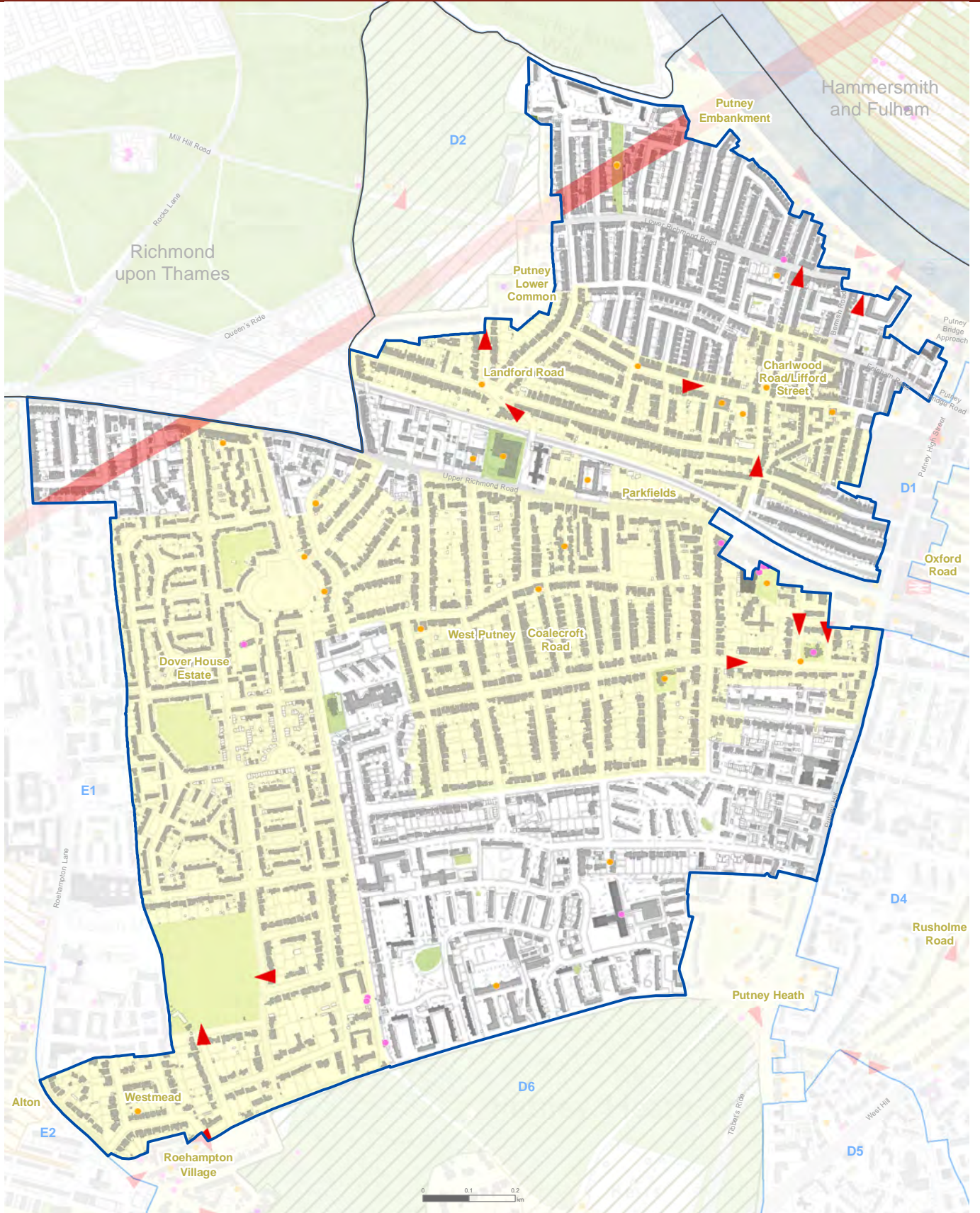
**Taller buildings, including some institutional uses are concentrated along major roads** and pockets of commercial use along the railway. Buildings along major roads are 4 storeys. There are 5-6 storey estates in the south of the area which are relatively well-integrated by the green open spaces and mature trees surrounding them.

**Landmarks include churches, and some larger mansion blocks/detached houses.** Landmarks include:

- Putney Park House (grade II listed);
- Putney Methodist Church;
- Our Lady of Pity & St Simon Stock RC Church;
- Hotham Primary School and Dryburgh Mansions;
- house at 115 Howards Lane ([Fig. 135](#));
- Polish Church of St John the Evangelist within the Putney Hill Park Estate in West Putney Conservation Area.

**The area has a traditional, semi-suburban character** characterised by residential land use and although relatively urban - i.e. its proximity to the town centre, ease of public transport accessibility, and busy main roads, there is a feeling of safety and surprising abundance of birdsong from mature trees and hedges. Local parades of shops provide an important function and destination for local communities.

**In the south east the urban grain becomes looser,** reflecting the transition away from the town centre to a more suburban character. The area includes a broad mix of least deprived and most deprived areas.



**Fig. 134: D3 West Putney Residential character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- High scenic quality and unified streetscene of the period houses, mature trees and front gardens.
- Locally distinctive picturesque cottages and open spaces in Dover House Estate Conservation Area.
- Green infrastructure, biodiversity and climate change resilience value provided by the mature street trees and private gardens which gives the area a leafy feel rather than being overly manicured.
- The well-managed small public open spaces, such as the playground at Tildesley Road and The Pleasance in the Dover House Estate Conservation Area, perform an important community function with opportunities for gathering, sitting outdoors and playing as well as for biodiversity. These are particularly valued because of a relative scarcity of public open space across the area.
- The heritage value and historic character of the townscape and individual buildings within CAs, but

- also the areas outside the CAs of similar character, as well as the nationally and locally listed buildings e.g. Grade I Roehampton House, Church of St John, Putney Park House, Exeter House.
- Quiet residential streets with a strong community spirit and independent businesses
- Valued views, including the designated LVMF view from **King Henry VIII's Mound in Richmond**, crossing the north west of the character area; and views and vistas of interest noted in Conservation Area appraisals and from site survey, including:
  - towards Hotham Primary School, Hotham Hall and Dryburgh Mansions;
  - over Roehampton Playing Fields;
  - of St John's Church from Burston, Ravenna and Ulva roads and St. John's Avenue;
  - north to Putney Lower Common from Egliston Rd
  - local views across Roehampton playing fields within Westmead Conservation Area.



**Fig. 135:** A distinctive house at Howards Lane forms a local landmark (locally listed), in West Putney Conservation Area



**Fig. 136:** Tree lined Gwendolen Avenue



**Fig. 137:** The distinctive roofline of Hotham Primary School is a local landmark

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Front gardens which are paved over or are missing boundary walls (replaced by parking or fences) detract from the generally coherent streetscape.
- Some more modern developments and estates lack biodiversity interest due to overly managed amenity areas and planting species less attractive to wildlife.
- On-street parking detracts from the streetscene (but is preferable to parking in front gardens).
- Use of inappropriate materials - e.g. timber, which is not complementary to character.
- Busy roads in some locations with air quality issues.
- Occasional examples of poor infill development e.g.

- post-war, low-rise infill along Genoa and Gwendolen Avenues; modern infill along Ravenna Road; petrol station on Lower Richmond Road; Pettiward Close; post-war housing on Upper Richmond Road; redevelopment of original villas along Upper Richmond Road within West Putney CA; Putney Animal Hospital on Clarendon Drive.
- Some examples of poor/generic taller developments e.g. high-rise developments along Upper Richmond Road; 6 storey modern development on Putney Hill/Chartfield Avenue.
- Lack of connectivity between the Dover House Estate Conservation Area and the wider townscape.
- Lack of public and particularly family facilities.



## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Semi-detached;
- Detached;
- Mansion blocks;
- Low-rise estates;
- Mid-rise estates.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

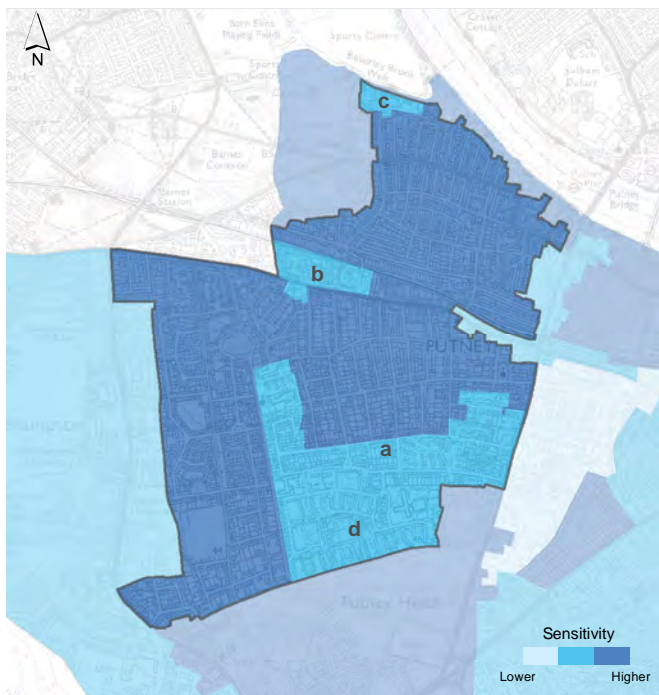


Fig. 138: D3 West Putney Residential sensitivity plan

Overall, West Putney Residential has a high sensitivity to change, and extensive change is not appropriate.

However, areas of lower sensitivity and which may be able to accommodate new development are: Chartfield Avenue, sub area (a); Upper Richmond Rd, sub area (b); Ranelagh Estate, sub area (c); Whitnell Way Estates, sub area (d) and **along the major roads**.

The estates (5-7 storeys) could potentially accommodate intensification within lower density garages and smaller blocks. There may be opportunity to rationalise space but a significant proportion of open space should be retained to set new buildings into their sites including mature trees and green boundaries. Busy traffic and wider streets reduce the sensitivity of Upper Richmond Rd and other major routes. Development should be set back and integrated behind tree planting.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

**Conserve**

Restore

Improve

Transform

The existing character of the area is strong, with a distinctive sense of place and characteristic elements in good condition. Therefore the strategy is to **conserve** the character, elements and features, whilst enhancing existing features where appropriate.

## Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Maintain and enhance street trees, front gardens and boundary walls (in preference to frontage parking).
- Maintain and encourage the use of traditional materials - primarily brick. Ensure special attention to choice of materials and architectural details to reflect the rich townscape of the area, avoiding the use of timber or plain façades.
- Respect the scale and proportions of existing buildings and streets. Most streets would not be able to accommodate buildings over 3 storeys. Wider streets, corner plots and the areas towards the river/parks might be able to accommodate buildings exceeding the prevailing height.
- Maintain the historic skyline by ensuring any roof extensions are highly sympathetic to the architectural building and street character.
- Enhance the quality, functionality and biodiversity of green spaces e.g. in the council estates, by incorporating play equipment, seating and management regimes which encourage wildlife.
- Maintain the sense of quiet and calm by containing taller buildings/more urban elements close to main roads and the town centre.
- Retain mature trees and protect access to the nearby open spaces.
- Protect the integrity and character of the conservation areas.

## D4 East Putney Residential



Fig. 139: Grade II listed house on Oakhill Rd, East Putney Conservation Area.

### Key characteristics

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

East Putney residential area has a mixture of housing styles, ages and materials. The essence of its character can be found in the spacious streetscape, villas and historic landmarks.

**East Putney developed along West Hill**, one of the early roads out of central London. During the 1800s the area was mostly fields and farmland with the exception of villas along West Hill occupying the high ground of the Putney ridge, with extensive views across the Wandle Valley to the east and River Thames to the north. A number of the 2-3 storey villas survive on West Hill/Keswick Road forming the nucleus of the East Putney Conservation Area. The railway and underground station at East Putney (1889) provided a catalyst for further growth in the north of the area.

**Relatively wide roads with generously spaced houses, vegetated front gardens and good sized rear gardens create a green spacious feeling.** West Hill is lined with an avenue of plane trees marking an attractive approach to Wandsworth; the properties along it are large and set back. **Topography rises to a high point at West Hill, providing long views along the wide sweeping roads.** The triangular greens (West Hill Road Conservation Area and Rusholme Rd Conservation Area) create local focal points with their mature trees. Glimpses between buildings to large trees in rear gardens gives an impression of greenness. Surviving grass verges add to the verdant character.

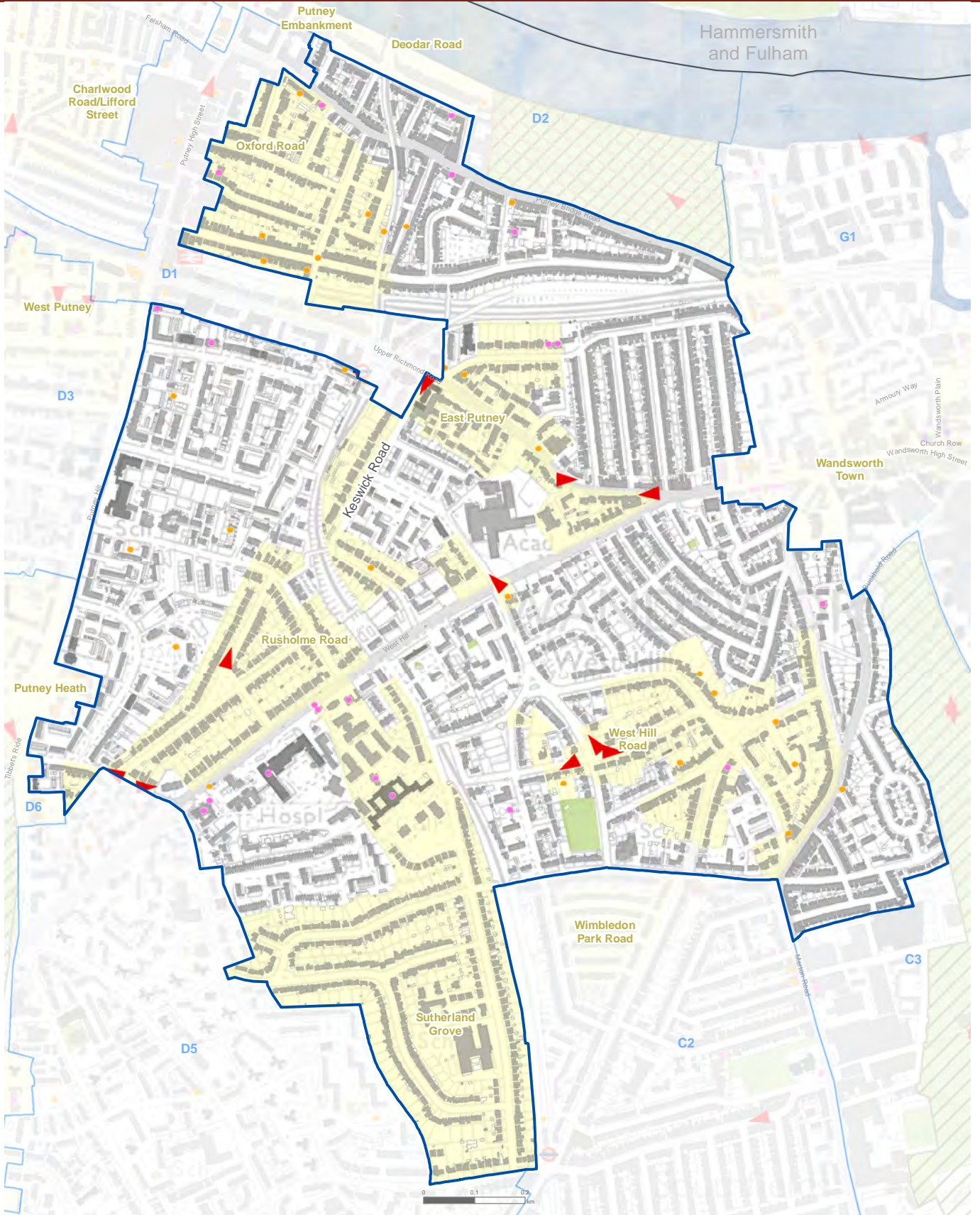
A mixture of housing styles, ages and materials creates a **visually rich appearance, whilst coherence is maintained through consistency** of scale, proportions and quality of materials. **Distinctive architectural elements provide interest to streets** e.g. windows and details such as elaborate brickwork and decorative balconies. Architectural styles and motifs include Arts and Crafts, and Old English cottage style (e.g. [Fig. 141](#)). Materials include good quality brickwork (yellow stock, red brick), render and hanging tiles, given coherence by their quality. Front boundaries are typically brick or timber and low level. Houses of different styles and ages lend a rich texture but also opportunity for fitting in new development. Areas of distinctive and high quality townscape are designated as conservation areas, including: Oxford Road, East Putney, Rusholme Road, Sutherland Grove and West Hill Road conservation areas.

**Taller building heights towards Putney town centre and on main roads** such as the mansion blocks Atlantic House and Oakhill Court on Upper Richmond Rd (both 5 storeys). This hierarchy of both building size and street width help legibility. Houses are typically 2-3 storeys and up to 8 storeys at West Hill.

**A relative sense of quiet, calm and lack of intrusion**, a domestic, residential character. Although the area is relatively urban (its proximity to Putney town centre, transport at Putney station and East Putney underground station, and busy main roads), there is a feeling of safety and greenness from mature trees and hedges. In the south east the urban grain becomes looser, reflecting the transition away from the town centre to a more suburban character.

**Landmarks punctuate the residential streetscape**, popping moments of interest and historic character into many streets. They include:

- 18<sup>th</sup> and 19<sup>th</sup> century 2-3 storey villas in West Hill and Keswick Road;
- tall spire of Holy Trinity church (grade II listed) can be seen from across the Wandle Valley;
- Whitelands College (now converted to flats), grade II listed 4 storey brown brick building built by Sir Giles Gilbert Scott (architect of Battersea Power station);
- The Royal Hospital (grade II listed), previously a grand 18<sup>th</sup> century villa;
- the railway bridge, Oxford Road Conservation Area ([Fig. 143](#));
- Putney High School;
- Rosslyn Tower (grade II listed).



**Fig. 140: D4 East Putney Residential character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- The junction of West Hill and Upper Richmond Road is within an archaeological priority area.
- Five local green spaces protected by the 1931 London Squares Preservation Act: Rusholme Rd; the triangular greens at Viewfield Rd, junctions with West Hill Rd and Melrose Rd; the junction of West Hill Rd/Wimbledon Park Rd; and junction of West Hill/Upper Richmond Rd. Other open spaces including St Michael's Church Field.
- Listed buildings and conservation areas noted for their architectural quality and interest.
- Street trees and green front gardens with their original boundary treatments (e.g. Rusholme Rd), and the separation between houses which allows glimpses through to mature gardens.
- Consistent building frontage to Wandsworth park which provides a open green setting with views to and walks along the river.
- Views and vistas of interest, including views noted in Conservation Area appraisals and from site survey, including:
  - view along Putney Heath Lane - both west towards Putney Heath and east to the spire of the Holy Trinity church;
  - the view corridor towards the Holy Trinity Church from the Wandle Valley character area;
  - long views into the East Putney Conservation Area e.g. from Upper Richmond Road to Keswick Road; from West Hill Road to Portinscale Road, from West Hill to Upper Richmond Road;
  - looking east along Upper Richmond Rd from within the East Putney Conservation Area;
  - view along the steep slope of Holmbush Rd gives a picturesque quality to the street vista;
  - sweeping views from the high point of the character area at the triangular greens at Viewfield Road/Melrose Roads.



**Fig. 141:** 1930s Old English cottage style house in Portinscale Rd, East Putney Conservation Area



**Fig. 142:** View of the spire of the Holy Trinity Church from Putney Heath Lane



**Fig. 143:** Railway bridge at Fawe Park Road

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- The mixed urban grain is quite irregular and inconsistent in paces. Regular terraced streets are interrupted by large blocks of flats from a later period. Insensitive infill developments, typically built following extensive World War II bomb damage are a mixture of styles and materials and reduce the coherence of the character area.
- Front gardens which are paved over or are missing boundary walls.
- Loss of traditional details, unsympathetic alterations including overly extensive roof extensions, loss of traditional street surfaces, street trees and gardens in favour of parking, insensitive advertisement signs and alterations to shopfronts.
- The Academy in East Putney Conservation Area lacks activation to the street
- Busy roads e.g. West Hill and which are often wide, with large areas of hard standing and serve to disconnect different parts of East Putney
- Newer development towards the eastern end of Putney Bridge Road fronting Wandsworth Park has some unsympathetic materials and detailing and not a positive frontage to Wandsworth Park.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Semi-detached; detached;
- Villas;
- Low-rise flats; mid-rise flats;
- Mansion blocks;
- Low-rise estates; mid-rise estates.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

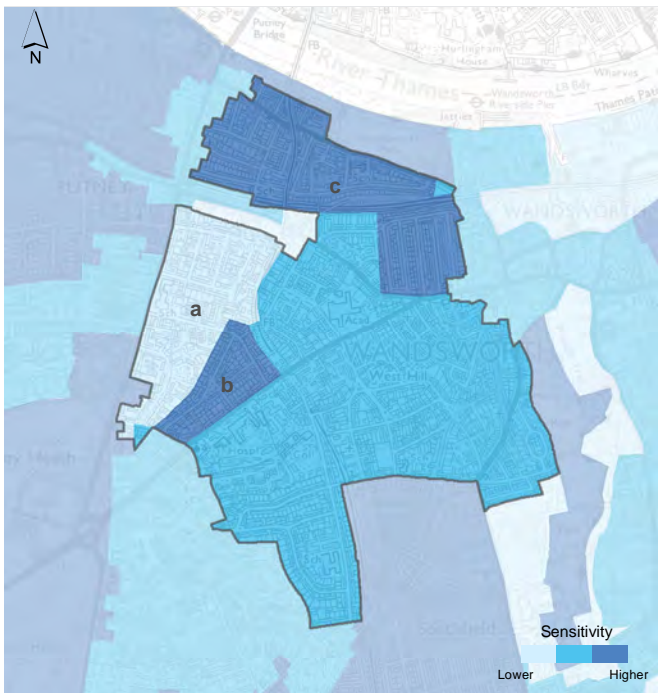


Fig. 144: D4 East Putney Residential sensitivity plan

Overall, East Putney Residential has a medium sensitivity to change.

A relatively high proportion of the area is conservation areas, reflecting high value townscapes. In addition to these, areas of higher sensitivity include: regular terraced streets which have an even grain and more formal layout, susceptible to buildings that would disrupt the coherence of pattern e.g. sub areas (b) and (c); and the consistent frontages opposite Wandsworth Park which provide a setting to the park - sub area (c).

Taller buildings are more likely to be able to be accommodated towards the town centre and main roads, (e.g. up to 5 storeys). Other areas of lower sensitivity include estates to the west (sub area (a)) owing to the less consistent pattern and mixture of building types which indicate that new development may be able to be accommodated. The sub area also has a higher proportion of existing mature landscape and trees which may help to integrate taller development.

Conserve

**Restore**

Improve

Transform

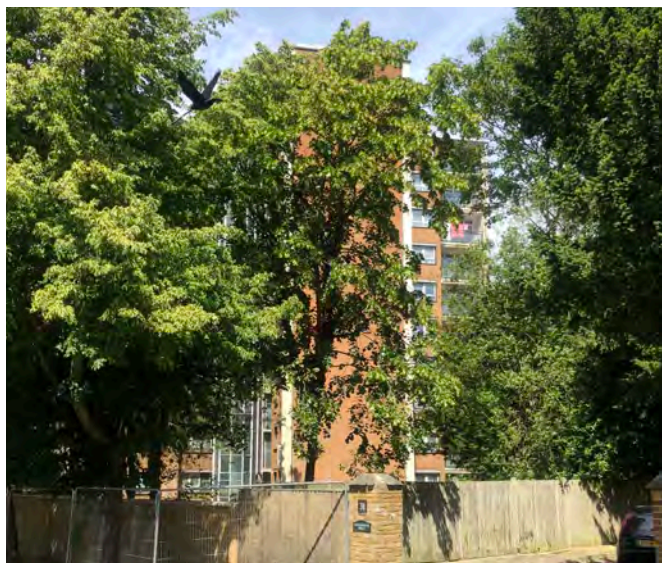
The green infrastructure, sense of spaciousness, the historic fabric and consistency of townscape, give East Putney Residential a sense of place, although its character has been negatively affected by insensitive infill developments. Therefore the strategy is to **restore** the existing character, by improving areas of irregular urban grain and pattern, ensuring any new development is well-planned and enhances the overall character of East Putney Residential by incorporating characteristic elements, protecting valued qualities and improving negative qualities.

## Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- The area's mixed character presents the opportunity for adding new development without adversely affecting character, if well-planned.
- New development should use historic references where appropriate.
- Consider the potential visual impact of development on long views along roads.
- Retain the sense of space between buildings which affords glimpses to mature gardens.
- Improve front boundaries, allowing views to the building behind, creating active frontages.
- Maintain and promote use of traditional materials and architectural details.
- Maintain the historic skyline by ensuring roof extensions are highly sympathetic to the architectural building and street character.
- Existing taller buildings are often sunken, reducing their impact, which should be considered for proposed tall buildings.
- Retain existing mature landscaping and green settings if any tall development is proposed, to ensure it is integrated into the landscape.
- Consider how to 'green' wide roads, either through planting or giving road space over to cycle lanes etc. Reinforce green street verges.
- Respect existing individual buildings of interest including landmark and listed buildings and the overall character of the conservation areas.
- Respect the scale and pattern of historic streets.
- Consider side and rear elevations of buildings.

## D5 Wimbledon Parkside Estates



**Fig. 145:** Eleven storey point block on Princes Way is well-integrated into the landscape with mature trees

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Wimbledon Parkside Estates is characterised by estates, both council built and private, set in spacious grounds. Its origins lie in being part of the former Wimbledon Park Estate

**Historically part of the former Wimbledon Park Estate**, formed around 1588. In 1765 Capability Brown designed the landscape and large lake (now in Wimbledon Park within LB Merton). Some of the historic character of large villas set in substantial richly landscaped gardens can still be perceived in the area's current character, particularly the spacious plots and mature trees. Only a few of the former villas survive, notably Fairlawns (in Putney Heath character area), the former ornamental garden of which is now a locally listed garden. Much of the estate was sold and developed on substantial plots in the 20<sup>th</sup> century by private builders and the local authority; the Victorian street layout survives along with many mature trees.

**Estates, consisting of houses of varying styles and heights but all set in spacious grounds.** Residential land use predominates, except for one local parade at Montford Place designated as important in the Local Plan, and five local schools. Modern estates and mansion blocks began to be developed in the inter-war period, many in the grounds of former 18<sup>th</sup> and 19<sup>th</sup> century villas. The area was sold as a place of the rural peace and quiet of the countryside with the facilities of the town. Estates are primarily relatively low density.

House forms and types include:

- 2 storey flats e.g. Colebrook Close (grade II listed) in landscaped surroundings;
- 2 storey terraces and semi-detached houses e.g. Tibbets Close (1950s) and Stourhead Close;
- 3-4 storey post-war estates e.g. Montford Place;
- 4½ storey mansion flats at Ross Court (inter-war);
- 6 storey post-war apartment blocks e.g. Limpsfield Avenue and 6 storey blocks set into the landform at Whitlock Drive;
- 8-11 storey point block towers (1950s) e.g. between Beaumont Rd and Princes Way, including Ackroyden Estate built by the LCC (see [Fig. 149](#));
- taller tower blocks of around 15 storeys such as at Keevil Drive;
- large 2 storey detached houses e.g. Victoria Drive Conservation Area, where houses are set back on wide plots, with tall boundary walls and green gardens. Quiet roads, and well detailed and well constructed inter-war houses of a traditional 'English vernacular' style. The houses share a similar form and materials;
- the row of quintessential high quality 2 storey detached 1920s houses with green settings, form part of the Bathgate Rd Conservation Area which continues outside of the borough into LB Merton.

**A green setting, increasingly so towards the south - near Putney Heath and Wimbledon Park which gives a suburban, spacious feeling.** Streets lined with grass verges and mature trees, some of which are the remnants of grand avenues associated with the former parkland of the Wimbledon Park estate in the south of the area. Mature trees, hedges and vegetation in front and back gardens creates a suburban street scene.

**Built form is set back from roads and taller buildings are generally well-integrated by dense vegetated boundaries** or sunken into the local topography. Buildings are relatively large, alongside large plots, creating a sense of proportion. There is a coarse urban grain with an organic feel to the layout of the roads. The dense boundaries give a feeling of privacy to the estates.

**Few landmarks**, but including:

- large mature trees, some of which are remnants of the former Wimbledon Park estate (see [Fig. 148](#));
- grade II\* listed Church of St Paul and the adjacent Grade II listed Wimbledon Park War Memorial;
- Linden Lodge School by Sir Edwin Lutyens (1933).

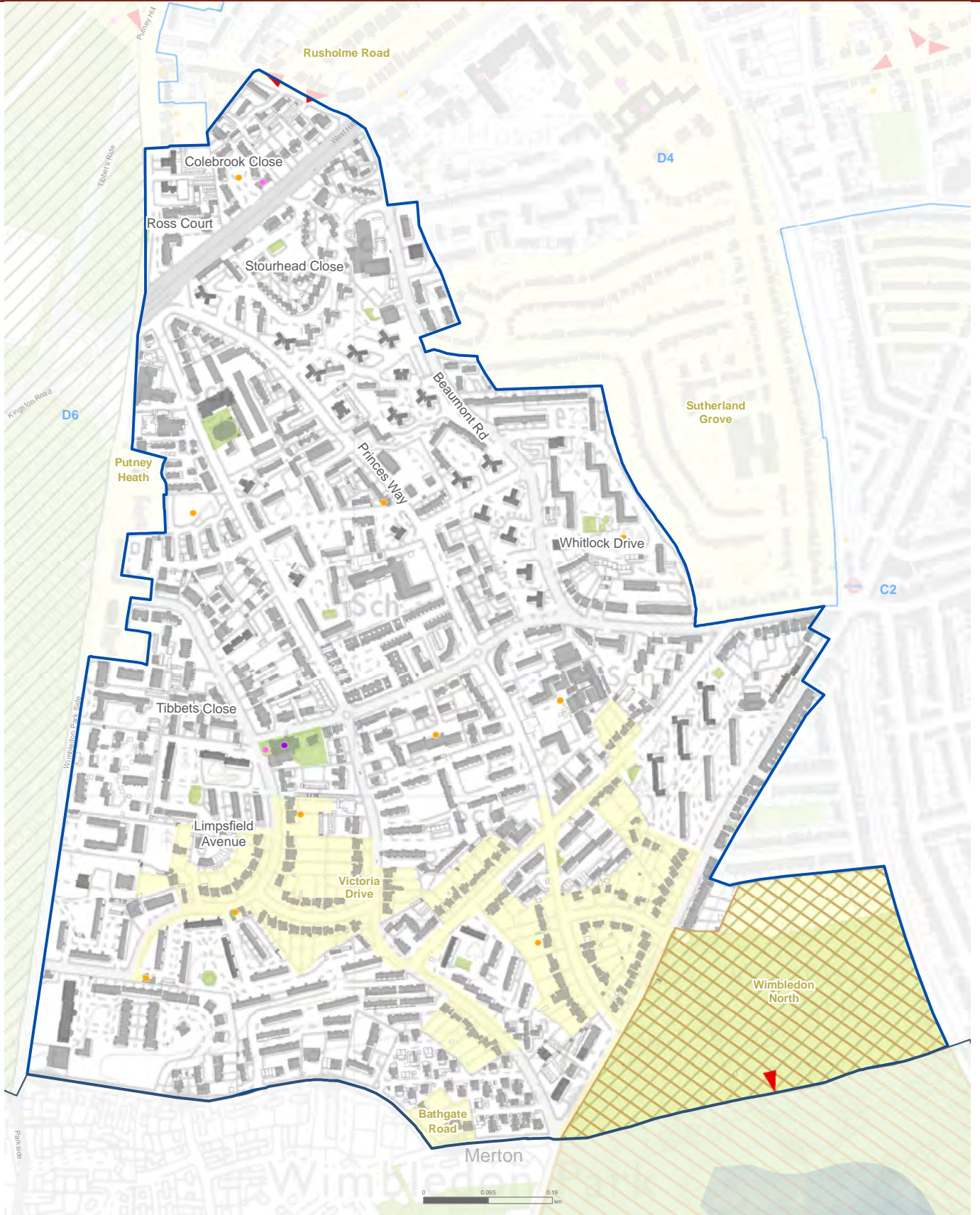


Fig. 146: D5 Wimbledon Parkside Estates character area plan

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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Wimbledon Park (the southern part of which lies within the London Borough of Merton), registered grade II\* park and garden as a surviving part of an 18<sup>th</sup> century park and lake by Capability Brown; also designated MOL and includes a section of the Capital Ring national trail. It is valued for:
  - biodiversity value (BAP priority habitat woodpasture and parkland);
  - leisure and recreation use by the local community;
  - mature trees including Horse Close Wood, which appears on maps from 1740 and may contain remnants of ancient woodland;
  - sense of green and openness, including the lake to the south in LB Merton;
  - the park's history as an ornamental park.
- Views and vistas of interest, include the view from Wimbledon Park south to the rising ground in LB Merton, to St Mary's church and the site of the 18<sup>th</sup> century Wimbledon Park House mansion (now demolished).
- The green setting created by large, mature street trees and grass verges which help to give the area a sense of spaciousness and attractiveness, some of which are remnants of the former parkland of Wimbledon Park.
- The high quality buildings including the listed buildings and those in conservation areas such as the Grade II\* listed Church of St Paul.
- The historic Southfields pond and gardens (locally listed), the remains of a garden of Edgecombe Hall Estate, valued for sense of relaxation and wildlife and sense of history.
- The former garden to Fairlawns (locally listed).



**Fig. 149:** The entrance to Wimbledon Park and distant view to a wooded horizon in LB Merton



**Fig. 147:** House on Windlesham Grove provides a historic reminder of the former Victorian lodge



**Fig. 148:** Landmark trees, grass verges and green front boundaries in Victoria Drive Conservation Area

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- The visual impact of the larger tower blocks where they rise to around 15 storeys, mainly in relation to views from other character areas, e.g. Andrew Reed House, which can be seen looming above more modest scale houses in the Sutherland Grove Conservation Area in the East Putney Residential character area.
- The fence and conifer screening around the athletics compound in Wimbledon Park, which is a visual detractor to the designated park and obscures the original Capability Brown designed vistas.
- Replacement of soft boundaries of houses with inappropriate boundaries and paving of front gardens to hard standing.
- Some areas of poor management in estates and streets.
- Poor connectivity around many of the estates, with boundaries providing obstacles or footpaths which do not connect.



### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Detached houses;
- Low-rise estates; mid-rise estates;
- Low-rise flats;
- Mansion blocks;
- Mid-rise flats;
- High-rise estates.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

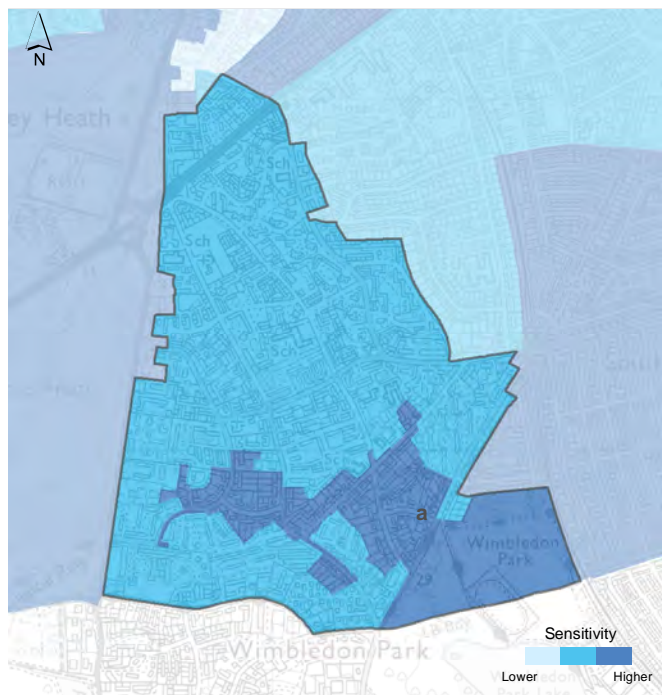


Fig. 150: D5 Wimbledon Parkside Estates sensitivity plan

Overall, Wimbledon Parkside Estates has a medium sensitivity to change.

The character area has the potential to incorporate change and new development, as long as this respects:

- the green, settings and sense of spaciousness, a historic reminder of the former Wimbledon Park Estate;
- remnant historic buildings, gardens, features, landmarks, and their settings.

Due to their high value, Wimbledon Park and Victoria Drive conservation areas have a higher sensitivity to change.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.



The existing character of the area contains some detracting elements, therefore the strategy is to improve the character of the place. The key characteristics should be protected whilst new development should address the relevant design guidance below.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Enhance historic park and garden in association with AELTC proposals as part of a wider Landscape Management Plan. Current application still to be determined.
- Improve athletics track boundary to reduce impact on the historic park. Enhance the quality and biodiversity through tree planting to provide shade and structure and improve legibility of its historic designed parkland character'
- Retain existing mature trees and vegetation and boundaries wherever possible.
- Integrate new development through planting large tree species.
- Improve the usability and biodiversity of the green spaces through enhanced management.
- Provide more accessible public open spaces.
- Consider any new development at the edges of the character area carefully to ensure the scale fits well with the context of adjacent character areas. Minimise visual impacts in other character areas through awkward juxtapositions of scale and proportion.
- Consider increasing density in places (e.g. where there are existing two storey buildings) but ensure the maturity of green spaces and trees are retained to soften buildings into the landscape.
- Enhance connectivity through the estates.
- Create new landmarks to add interest and aid legibility.
- Protect the integrity and character of the conservation areas.

## D6 Putney Heath and Vale



**Fig. 151:** People relaxing and playing in the open space on the high point of the tumulus at Putney Heath, beyond which is characteristic wooded skyline

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Putney Heath and Vale encompasses the full extent of the Heath, which continues south as Wimbledon Common in Merton borough. It also includes the Putney Heath Conservation Area and Roehampton Village Conservation Area, with its strong relationship to the Heath.

### **Putney Heath forms a high plateau in the borough.**

Its highest point is at Jerry's Hill. The gradient falls steeply to the west towards Putney Vale and Beverley Brook. The underlying soils are mostly sands, gravels and silty clays which are poorly-drained, nutrient-poor and acid conditions, and therefore heathland.

**Natural heritage of Putney Heath:** Wimbledon Common, Putney Heath and Putney Lower Common provide the largest extent of heathland in London. Localised areas of dry heath support heather, bell heather and dwarf gorse; and small but important areas of 'humid' heath which is very rare in the London area. **Semi-natural broadleaved woodland** covers the deeper, clay soils of the western slope. The large number of old trees and fallen decaying timber support scarce invertebrate species including the stag beetle. Open spaces and lakes are enclosed by dense woodland giving a sense of **seclusion, calm and wildness**, partly affected by traffic noise.

**A wooded skyline** in most places, with little development visible. Wimbledon and Putney Commons

are protected by the Wimbledon and Putney Commons Act of 1871 from being enclosed or built upon.

**Development in the character area is primarily residential, and houses are set back** behind boundary walls and well integrated into the landscape by mature tree planting.

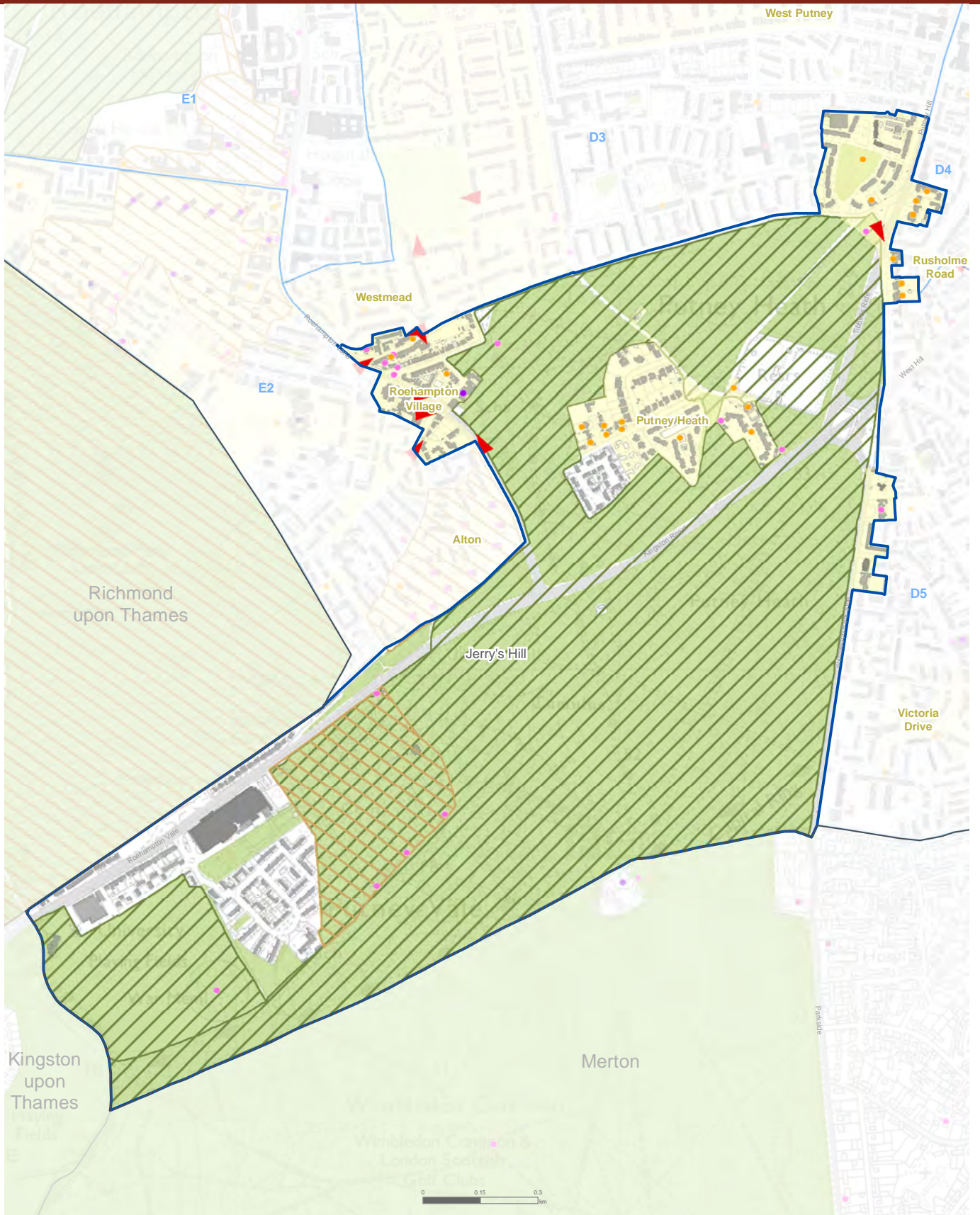
**Leisure and recreation use of the Heath:** numerous public rights of way including the Capital Ring long distance trail. The majority of the character area is designated as MOL (excluding the residential areas of Putney Village and Roehampton Village and university development in Putney Vale).

**Putney Heath Conservation Area** whose special character derives from the rural setting of the Heath, its openness and sense of space. It also includes the more formal villas such as Fairlawns (grade II listed) to the east, and the mansion flats of Manor Fields. Houses are generally set back in their own grounds, mimicking the Victorian houses they replace, giving a sense of spaciousness. Even at Manor Fields, closer to the urban centre of Putney, the gardens contribute to a sense of seclusion despite the adjacent major road junction.

**Roehampton Village Conservation Area**, which, despite the changes of hundreds of years, is still recognisable as a village that had its origins in the 17<sup>th</sup> century. The village retains much of its old rural character, with a small cluster of mostly small-scale domestic buildings, around a short commercial high street bounded on two sides by Putney Heath. The parade of village shops help convey a flavour of rustic Georgian charm and provide local amenities. Its rural character is enforced by soft edgings to the Common, red brick paving and granite setts help.

**Historic character:** until 1848 the Telegraph pub, was the location of a former Admiralty telegraph station, part of a system of visual telegraphy using large wooded boards, which enabled communication between London and Portsmouth. Landmarks include:

- Manor Fields Estate;
- 81 Wimbledon Park Side (Elmley House);
- Holy Trinity Church (grade II\*);
- Elevated focal points in Roehampton village including the parish church, the clock in Ponsonby Road, the Roman Catholic Church, the Village Hall, the Convent and the Kings Head and large venerable oak tree;
- Drinking fountain in Roehampton village Conservation Area;
- Roehampton War Memorial.



**Fig. 152:** D6 Putney Heath and Vale character area plan

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### Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- The natural heritage importance of Putney Heath/Wimbledon Common, which supports the most extensive area of heath on acidic soil in London and a complex mosaic of habitats including broadleaved woodland, acid grassland, scrub, as well as locally rare species such as stag beetle. The area is designated as a SAC and SSSI.
  - The recreation value and openness of the Heath, designated as MOL, including the Capital Ring national trail and national cycle network route.
  - High scenic quality, rural character and sense of spaciousness. Trees and vegetation are valued for biodiversity and also screening, helping to integrate development into its surroundings so it is hardly visible.
  - The high quality and good condition of Roehampton Village Conservation Area and Putney Heath Conservation Area.
- Cultural heritage, including the former Admiralty Telegraphy Station and remnants of the former Georgian estate landscape e.g. houses on Alton Rd/ Bessborough Rd in Roehampton Conservation Area.
  - Putney Vale Cemetery Registered Park and Garden.
  - Views and vistas of interest, including:
    - across the green space in front of the Green Man public house at the top of Putney Hill;
    - of Holy Trinity Church spire from Roehampton High St/Rodway Rd;
    - approaches to Roehampton village along Roehampton Lane from both the north west and south east, afford a dramatic sense of arrival and views of the Holy Trinity church and king's head public house;
    - view up the High Street from the junction of Roehampton Lane and Danebury Avenue, to the King's Head pub and oak tree, along the steeply rising street;
    - View of Holy Trinity Church from Alton Estate along Alton Road.



Fig. 153: Cottage style houses on Medfield Street in Roehampton Village Conservation Area



Fig. 154: Public footpath through the heathland



Fig. 155: Highlands Heath, Putney Village

### Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- The busy Kingston Road and large roundabout affect the tranquillity of Putney Heath, and provide a major barrier to pedestrian movement.
  - The huge scale towers in the Alton Estate to the west dominate many views in and around the area, detracting from the setting of small scale of Roehampton Village Conservation Area, e.g. along the approaches to the village along Roehampton Lane from the north and south.
  - Inappropriate roofing materials and oversized dormers on two buildings on the north side of Medfield Street in Roehampton Village Conservation Area are intrusive and detract from the character.
- Some developments such as Asda in Putney Vale, have localised impacts on scenic quality.
  - Reservoirs and their boundary fencing on Putney Heath detracts locally.

### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Semi-detached; detached;
- Villas;
- Mansion blocks;
- Shopfront terraces;
- Large floor plate retail/industrial.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

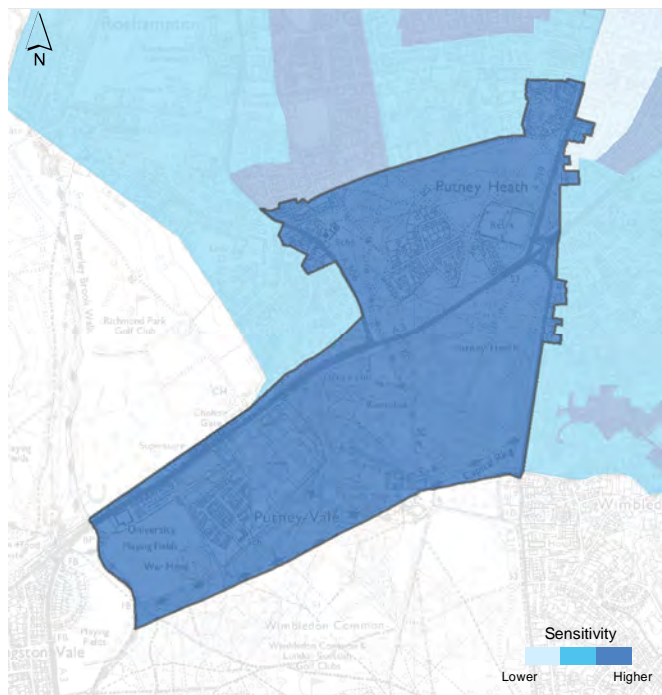


Fig. 156: D6 Putney Heath and Vale sensitivity plan

Overall, Putney Heath and Vale has a high sensitivity to change, and extensive change is not appropriate.

There may, however, be small areas of lower sensitivity where the townscape is less intact and does not reflect the positive character described in the key characteristics.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve	Restore	Improve	Transform
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The existing character of the area is strong, with the open spaces and built areas in excellent condition. Therefore the strategy is to **conserve** the character.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Enhance the condition of Wimbledon Common SSSI, currently predominantly in 'unfavourable - recovering' condition.
- At Putney Heath, undertake management outlined by the Wimbledon SAC 'Site conservation objectives' (Natural England, 2016), Site Improvement Plan, and the Views about Management of the SSSI (Natural England, 2005), including restoring the total extent of heathland to 48.6ha.
- Retain mature trees, particularly ancient, standing dead trees, fallen trees, stumps and roots in a state of decay. This includes urban areas such as local gardens, parks and roadside trees.
- Maintain or restore the open character of the heathland.
- Retain local centre functions in Roehampton Village, including shops, food stores, pubs and restaurants.
- Repair or reinstate original features such as timber windows and doors.
- Respect the existing proportions of historic buildings and the character of conservation areas.
- Protect and enhance valued views and landmarks.
- Ensure any new development is well integrated with appropriate trees and vegetation. Resist elements which would result in an urban character.
- No capacity for buildings of 5 storeys and over has been identified.

# E Roehampton



**Fig. 157:** The listed slab blocks (shown here in the Alton West Estate) of the post war era are a key characteristic of Roehampton

## *Summary*

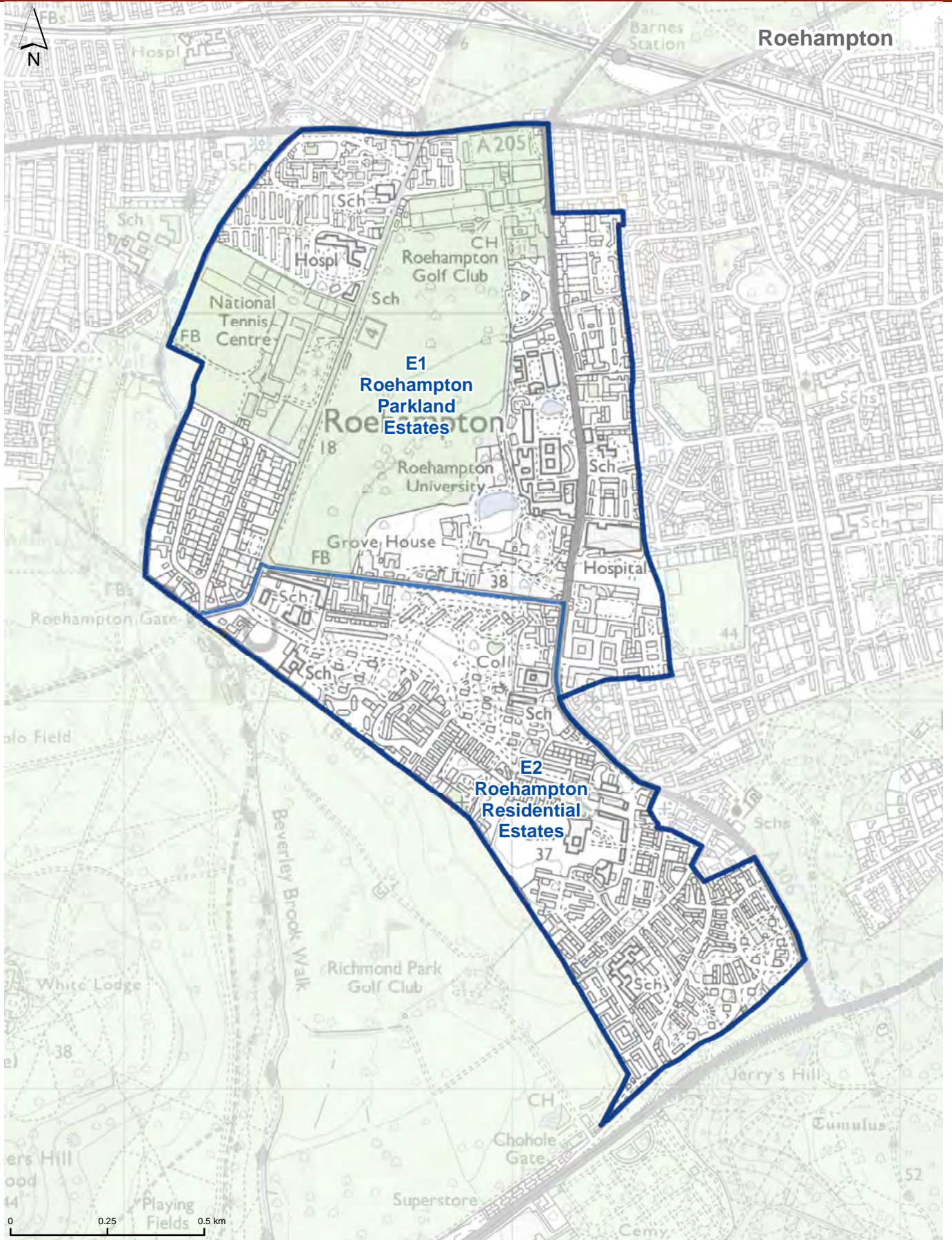
Roehampton is a mixed area, with some remnant large Georgian and Victorian mansions, 20<sup>th</sup> century housing estates, many of which were pioneering at the time, and the university campus. The 17<sup>th</sup> century village around Roehampton High Street is within Putney 'place' due to its relationship with Putney Heath.

The character of the area changed significantly in the mid to late 1950s with the introduction of the Alton Estate, said to be the finest examples in London of the post WWII idealism of buildings within a cherished landscape setting.

Much of the older parts of Roehampton are set around the large open spaces of the university and adjacent golf course.

## *Key changes and trends*

- Alton West and East is part of a masterplan being developed under the Area Spatial Strategy to redevelop the post-war estates of the 1950s.



**Fig. 158:** Place E: Roehampton character areas plan  
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# E1 Roehampton Parkland Estates



**Fig. 159:** Red brick imposing 5 storey building of the Bank of England Sports Centre

## *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Roehampton Parkland Estates encompasses the large scale buildings, institutions and houses set in and amongst open spaces including Roehampton Golf Course. There is a historic legacy of villas in landscaped grounds, many of which have now been re-purposed.

**Roehampton has an interesting legacy of grand villas from the Georgian era, when it was popular as a destination for aristocratic summer residences close to Richmond Park.** Some of the Georgian villas still survive, notably Grove House (grade II\* listed) and the Priory Hospital (grade II listed). The grade I listed Roehampton House (Queen Mary's Hospital) is even older, from 1710, and at one time formed part of an extensive estate and 17<sup>th</sup> century deer park (now the golf course). With the gradual demolition of many of the large houses starting from the 19<sup>th</sup> century, a number of infill residential areas were developed including the 4 storey crescent-shaped block of flats Fairacres (grade II), in 1936, and the large detached houses of Roehampton Gate and the symmetrically planned estate of Roehampton Close around the same time.

**A relatively large scale grain with a sense of order, balance, symmetry and proportion:** the Georgian estate legacy has imparted a sense of grandeur, space and proportion. This is reflected in the general balance

of building proportions to open spaces, with buildings set sensitively in their plots, usually set back from roads. In addition to the Georgian buildings, this also includes Fairacres and Roehampton Close (1930s) as well as the substantial detached houses at Roehampton Gate and along Roehampton Lane. Some of the more recent developments are less successful at achieving this. Taller blocks fit well into the landscape where their mass is in proportion to their height, such as the 5 storey Bank of England Sports Centre (Fig. 170). In the north of the area the mixture of buildings heights between 2-5 storey brick terraces and blocks and the 13 storey towers disrupt the predominant pattern.

**Extensive green open spaces,** including Roehampton Golf Course, the National Tennis Centre, Rosslyn Park FC the grounds at Grove House and the garden at Roehampton House. A significant proportion of the area is designated Metropolitan Open Land. The Beverley Brook runs along the western boundary, (also the borough boundary with LB Richmond upon Thames) although there is little sense of the river on the ground.

Few street trees, although mature trees in front gardens and grounds are important to creating a green character.

Predominant materials are brick (red brick and London stock brick).

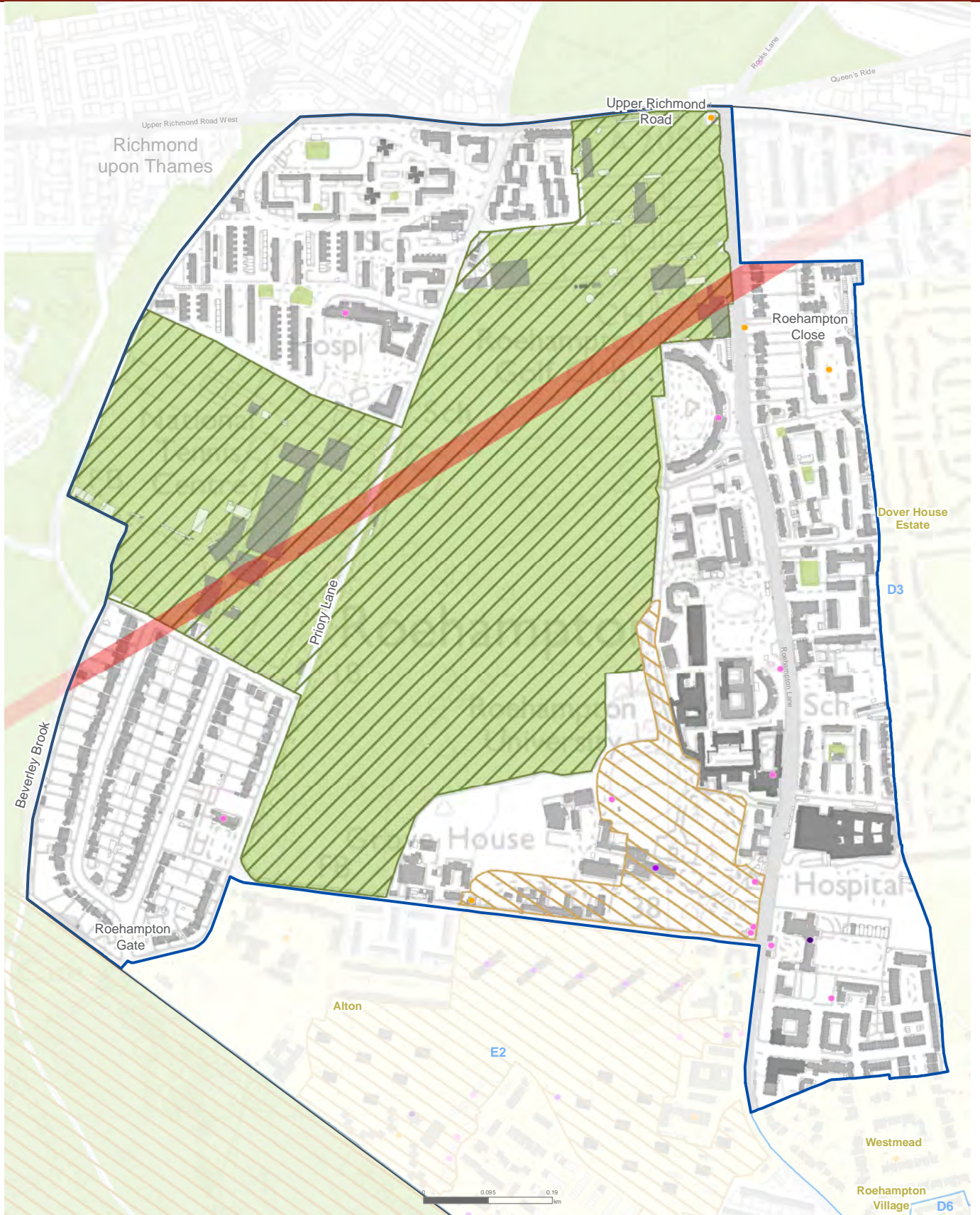
**Mixture of land uses** including a school, golf club, hospitals, a local shopping parade (Rockingham Close), Rosslyn Park FC, Roehampton University buildings and the national tennis centre as well as 20<sup>th</sup> century residential properties, both houses and flats. University buildings and hospitals occupy the former Georgian villas.

**Perception of privacy created by barriers and curtailed access.** The area feels fragmented by busy major roads, including Roehampton Lane and Priory Lane, and Upper Richmond Road to the north. Fast moving traffic creates a vehicle-dominated environment.

**Landmark buildings,** including Fairacres and Roehampton House which are large scale, imposing and important focal features.

**Roehampton Gate of Richmond Park** is a particularly distinctive feature, providing a connection to the grade I registered park and garden.





**Fig. 160:** E1 Roehampton Parkland Estates character area plan

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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- The historic landscape character (also extending to the south to Roehampton Residential Estates), of Georgian villas set in landscaped grounds, highly valued for their cultural and historic interest as well-preserved examples of the area's former grandeur.
- Grade I listed Roehampton House, dating from 1710 but containing early 20<sup>th</sup> century extensions by Sir Edwin Lutyens.
- Other nationally and locally listed buildings, including Grove House (grade II\*), Old Lodge and Chapel of the Sacred Heart, Digby Stuart College (grade II), Society of the Sacred Heart war memorial, Fairacres (grade II), Templeton House (grade II), The Priory (grade II).
- Grove House grade II registered park and garden, mid 18<sup>th</sup> century gardens and pleasure grounds of Grove House villa, including other listed features within the garden such as the dummy bridge, mausoleum, lodges and gates.
- The sense of openness provided by the area designated as Metropolitan Open Land including the golf course, national tennis centre, football club and the university grounds.
- The view corridor of the designated London View Management Framework view from King Henry VIII's Mound in Richmond.
- The relationship with and the role the area has in providing a setting to Richmond Park (grade I registered park and garden), including views from the park to the character area (noting the design vision of the Alton Estate blocks to form distinctive landmark elements on the skyline).
- The landmark buildings including Fairacres, the university and Roehampton House which are large scale, imposing and important focal points.
- Mature trees in front gardens and grounds are important to creating a green character.



**Fig. 161:** Large detached house on Roedean Crescent



**Fig. 162:** Low-rise and high-rise estates at Arabella Drive



**Fig. 163:** Looking into Richmond Park from Roehampton Gate

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- The sense of privacy, disconnection and fragmentation caused by high fencing around private land and lack of visibility. This results in roads being a dominant characteristic. Fences around Rosslyn Park FC, the A205 and Roehampton Lane (timber fence/palisade fence and netting) detract from the sense of place.
- Busy traffic along Roehampton Lane.
- Some newer development lacks the balance and sense of proportion, with disparate building heights and orientation disrupting the sense of coherence of the area.
- Some of the newer developments such as Bader Way lack mature trees and vegetation, with few front gardens, although there is an attractive public space within it.
- Some areas feel disconnected from easy to access green open spaces.

### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Detached;
- Villas;
- Low-rise estates;
- Mid-rise estates; high-rise estates;
- Mid-rise flats;
- Community/leisure.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

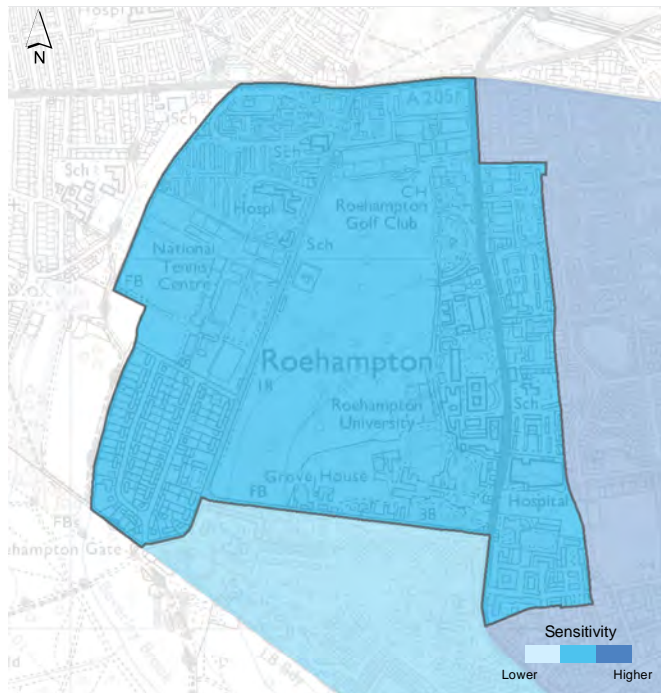


Fig. 164: E1 Roehampton Parkland Estates sensitivity plan

Overall Roehampton Parkland Estates has a medium sensitivity to change: there are some features of high value and others where change could result in enhancing the overall character.

Any change should:

- conserve and respect the historic character and the heritage assets and their settings including the nationally and locally listed buildings and landscapes;
- maintain the openness provided by the areas of MOL;
- preserve the LVMF view corridor.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve

Restore

Improve

Transform

Although there is an underlying historic character to the area, this has become lost under layers of disparate change and development. Therefore the strategy is to **improve** and **conserve** character, aiming to establish a new coherent character which incorporates and protects the valued assets and addresses the negative qualities.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Maintain and enhance street trees and front gardens, including keeping pavements clear of overhanging planting. Retain mature trees wherever possible.
- Increase green infrastructure through street tree planting and front gardens as part of any new development. Ensure planting has native and wildlife friendly species.
- Maintain and encourage the use of traditional materials - primarily brick. Ensure special attention to choice of materials and architectural details to reflect the rich townscape of the area, avoiding the use of timber or plain façades. Larger buildings should incorporate architectural details to avoid a 'monolithic' effect.
- Conserve the historic character and the heritage assets and their settings including the nationally and locally listed buildings and landscapes.
- Conserve and enhance the character and appearance of the Registered Park and Garden including restoration of the rock cascades and grotto and the mausoleum.
- Opportunities exist for development of generally up to 5-6 storeys as long as it is balanced with a commensurate proportion of planted open space around the building, and the wider major roads.
- Enhance the quality, functionality and biodiversity of green spaces e.g. around Beverley Brook and the public estates by incorporating wildlife-friendly planting and management.

## E2 Roehampton Residential Estates



**Fig. 165:** Grade II\* listed Denmead House in Alton Conservation Area, surrounded by Scots Pine trees part of the parkland setting

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Roehampton Residential Estates encompasses the buildings and parkland occupied by the Alton East and Alton West Estates, a pioneering 20<sup>th</sup> century social housing scheme set in a historic landscape and interwoven with historic buildings.

**A palimpsest landscape, made up of different layers of historic developments from the Georgian, Victorian and post-war eras. Historically the area was occupied by Georgian country estates** reflecting Roehampton's popularity as a destination for aristocratic summer residences close to Richmond Park. Several of the country estates in their landscaped parkland settings still survive (notably Mount Clare, grade I listed, Parkstead House, also grade I and Downshire House, grade II\*), although many have been altered, extended or part demolished. The Victorian era saw development of villas, in subdivided estate plots.

In the late 1940s the London County Council acquired the land for a major flagship housing scheme. The Alton Estate is one of the largest council estates in the UK, built to rehouse people as part of slum clearance schemes in Hammersmith, Fulham and Shepherds Bush. The scheme was interwoven with the existing historic landscape. Alton East was constructed between 1952-1955 and Alton West between 1955-1959.

**Parkland setting within which buildings have been carefully sited and harmoniously arranged.** The

design vision for the Alton Estate was for elegant and harmonious clusters of residential accommodation set within generous parkland; retaining the maximum amount of parkland possible was a key principle of the masterplan, to give residents the impression of 'living in the park'. Land rises to the north east towards the plateau of Putney Heath. The tallest buildings were designed to form distinctive landmark elements on the skyline in views from Richmond Park. Mature trees and undulating landform create a distinctive parkland landscape, although much of the areas have become overgrown, with sections of impermeable wilderness, and spaces which are dark, unsafe and unusable. The Alton West slab blocks took their inspiration from Unite d'habitation in Marseilles with a flowing landscape, while the Alton East point blocks are influenced by Scandinavian housing and landscape.

**A deteriorating sense of place:** the vision for the masterplan of distinct neighbourhoods, was not completely realised, and has suffered from additions and demolitions over the years, obstructing views, and infilling 'spare' bits of land with poor quality developments. The incremental change and redevelopment has eroded the overall vision and sense of place, compromised the openness, harmony and connectivity of the estate, and resulted in a confused place. This has led to much of the area being identified in the Local Plan as an area for regeneration.

**Residential land use,** with a mix of heights and styles. The residential use is mixed with community, education and retail uses, with a local shopping parade, community facilities and Roehampton University colleges occupying the former Georgian villas. However, the area lacks a distinct centre or focus such as entertainment facilities or restaurants. **Materials are primarily light and grey coloured concrete.** The buildings of the Alton Estate comprise five basic types:

- slab blocks (11 storeys), now listed grade II\* and described by Pevsner as the most important innovation of the estate;
- point blocks (12 storeys);
- maisonettes (4 storeys);
- terraced houses (2-3 storeys); and
- bungalows (1 storey).

**A landscape of socio-economic challenges:** the area has a low level of public transport accessibility, particularly for rail transport. There are also high levels of economic inactivity and unemployment, and higher than average crime levels.



**Fig. 166:** E2 Roehampton Residential Estates character area plan

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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- The unique historic and cultural interest of the Alton Conservation Area and its setting, comprising the surviving layers of development from successive eras (Georgian, Victorian and post-war) in a parkland setting. The special sense of place resulting from the atmospheric landscaping, historic layout and architectural quality of the buildings.
- Mount Clare and Parkstead House, both grade I listed Georgian villas, and Downshire House, grade II\* listed, valued for their historic interest.
- The scenic, cultural and historic value of the locally listed gardens at Parkstead House and Minstead Gardens at Mount Clare, designed by Capability Brown, bringing a feeling of tranquillity.
- The parkland landscape and mature trees which provides a strong setting to the large tower blocks, important for maintaining a sense of openness.
- Cultural value of the Alton Estate, a highly influential post-war conception of towers in a parkland setting and important reference in the history of housing development in the UK, particularly the landmark slab and point blocks (grade II\* listed), and the grade II listed registered parks and gardens designated in June 2020.
- The relationship with and the role the area has in providing a setting to Richmond Park (grade I registered park and garden), including views from the park to the character area (noting the design vision of the Alton Estate blocks to form distinctive landmark elements on the skyline).
- The relationship with Roehampton High Street including setting of the conservation area and the view to the spire of the Church of the Holy Trinity.
- Recreational and access value provided by the strategic cycle network routes along Danebury Avenue and Clarence Lane; and proximity to open spaces like Richmond Park.



**Fig. 167:** Bland amenity grass in front of the 4 storey block creates a poor frontage to Danebury Avenue



**Fig. 168:** Visual connectivity to Roehampton High Street from Danebury Avenue



**Fig. 169:** Danebury Avenue local centre has public spaces which lack function

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Poor condition of buildings and public realm, including the 'square' at Danebury Avenue.
- Lack of legibility and focal points, with incremental additions over the years having eroded the area's coherence. Some buildings, such as along Danebury Avenue, do not address the street positively.
- Dominated by cars and parking which detracts from sense of place.
- Perception of a lack of safety due to dead ends, retaining walls, and lack of natural surveillance.
- Absence of interest and vibrancy along the main 'centres' such as the shopping and community amenities at Danebury Avenue and Portswood Place.
- The estate is cut off from its surroundings, by the A3 and Roehampton Lane and continuous fences along Richmond Park/golf course which prevent access for residents.
- Lack of efficient transport infrastructure.
- Poor circulation and connectivity: the road barrier by Alton Primary School limits access from the estate to Richmond Park via bus and creates a cul-de-sac effect which is confusing for visitors.
- Number of buildings are too tall for their position next to Richmond Park, including the towers but also blocks such as Allbrook House (10 storeys).

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Low-rise estates;
- Mid-rise estates;
- High-rise estates;
- Villas.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

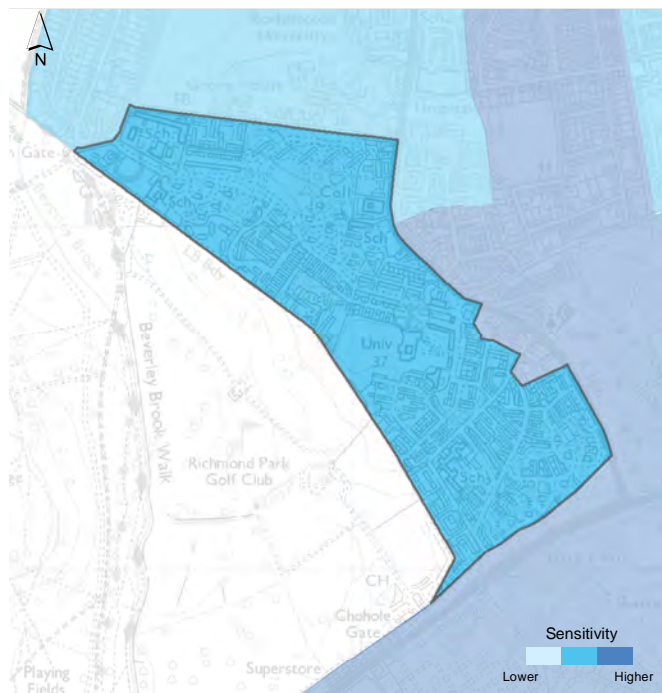


Fig. 170: E2 Roehampton Residential Estates sensitivity plan

Overall, Roehampton Residential Estates has a medium sensitivity to change, with the potential for positive change which is sympathetic to the area's valued features.

The existing extensive areas of open space and the existing built features in the area indicate the area may be able to accommodate growth. However, there are also numerous valued features including the heritage assets, historic landscape character and sense of openness. Therefore any change should respect the area's valued assets and susceptible characteristics, including:

- the parkland setting and the role it provides in providing a spacious setting to the taller buildings on the site;
- the special qualities of the conservation area and listed buildings;
- the setting to Richmond Park.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

**Conserve**

*Restore*

*Improve*

**Transform**

Although containing many features of cultural and historic interest, the character of the area has become degraded over time and unrecognisable from its former state. Therefore the strategy is to **transform** the character of the area, into a landscape with a strong, new character. At the same time, the strategy should **conserve** and enhance the many positive assets of the area, namely the parkland setting, historic character and heritage assets.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Refer to the Alton Masterplan documents.
- Respect the heritage assets and their settings, including the listed buildings, gardens and conservation area.
- Maintain and enhance the parkland character of the landscape, ensuring that any new development in the area does not diminish the openness and harmonious balance of buildings and open spaces.
- Enhance focal points, local centres and amenities, including community facilities.
- Enhance the biodiversity, scenic and recreational amenity function of the green and public open spaces.
- Improve building maintenance and condition.
- Improve provision of public realm and spaces for sitting and socialising.
- Improve connectivity with the surroundings, particularly Richmond Park. Incorporate a movement strategy across the area.
- Ensure buildings have positive active frontages to surrounding buildings, streets and open spaces.
- Improve use and function of public realm and public spaces, reducing dominance of the car.
- Ensure any new tall buildings respects the pattern of existing built development, valued views and settings of heritage assets.

# F Tooting



Fig. 171: Tooting has a mix of property styles but many are quite large and have a suburban feel

## Summary

Tooting has pre-Saxon origins and was a focus for trade with its position on the Roman Road Stane Street. Historically it developed as Lower Tooting and Upper Tooting which centred on the Broadway, where Tooting Bec and Broadway stations now sit. Furzedown, to the south of Tooting Common was developed by local builders in 1900 within the extensive grounds of Furzedown House with a grid pattern of streets.

The 89 hectare Tooting Bec and Tooting Graveney Commons represent the remaining open space that once stretched to Mitcham.

Today, the demographics of Tooting are shifting towards young professionals with a vibrant town centre.

## Key changes and trends

- Tooting Broadway station is being considered by TfL as a station on the Crossrail 2 development. This could potentially relieve congestion on the Northern Line, and provide Tooting with a rapid and direct connection to major London station such as Clapham Junction, Victoria, Tottenham Court Road and Euston. However, it also brings the town centre under threat as the new station would require the demolition of a significant central space on the high street.
- As an important town centre, Tooting will be a focus for redevelopment and increased housing provision where appropriate to do so. The plan for future change in this area is set out in the Area Strategy in the Local Plan.





# F1 Tooting Town Centre



**Fig. 173:** Tooting Market (locally listed) - vibrant colours and a diverse range of food cultures

## *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Tooting Town Centre stretches along two main roads: the long Tooting High Street, and Mitcham Road. Its focus is around Tooting Broadway, although there is also a local centre at Tooting Bec.

**The character of the town centre can be divided into four zones:** the focus around Tooting Broadway underground and the high street; Tooting Bec local centre (small scale shops); Tooting Graveney at the southern end of Tooting High Street (small scale shops); and Mitcham Road (entertainment, pubs and restaurants).

**The town evolved along Tooting High Street,** a Roman road (Stane Street) which linked Chichester to London. Tooting Bec takes its name from the abbey of Bec in France, which once owned the land in the area, and was a separate place from Tooting Broadway. In 1926 transport links were improved with the extension of the Northern Line, followed by the opening of the Tooting Granada in 1930, which gave cultural life in Tooting a major boost. Much of the high street is within an Archaeological Priority Area.

**Distinctive architecture and landmark buildings.** Upper storeys of buildings are particularly distinctive, with a high proportion of listed and locally listed buildings, particularly in Tooting Broadway. Across the town centre key buildings include:

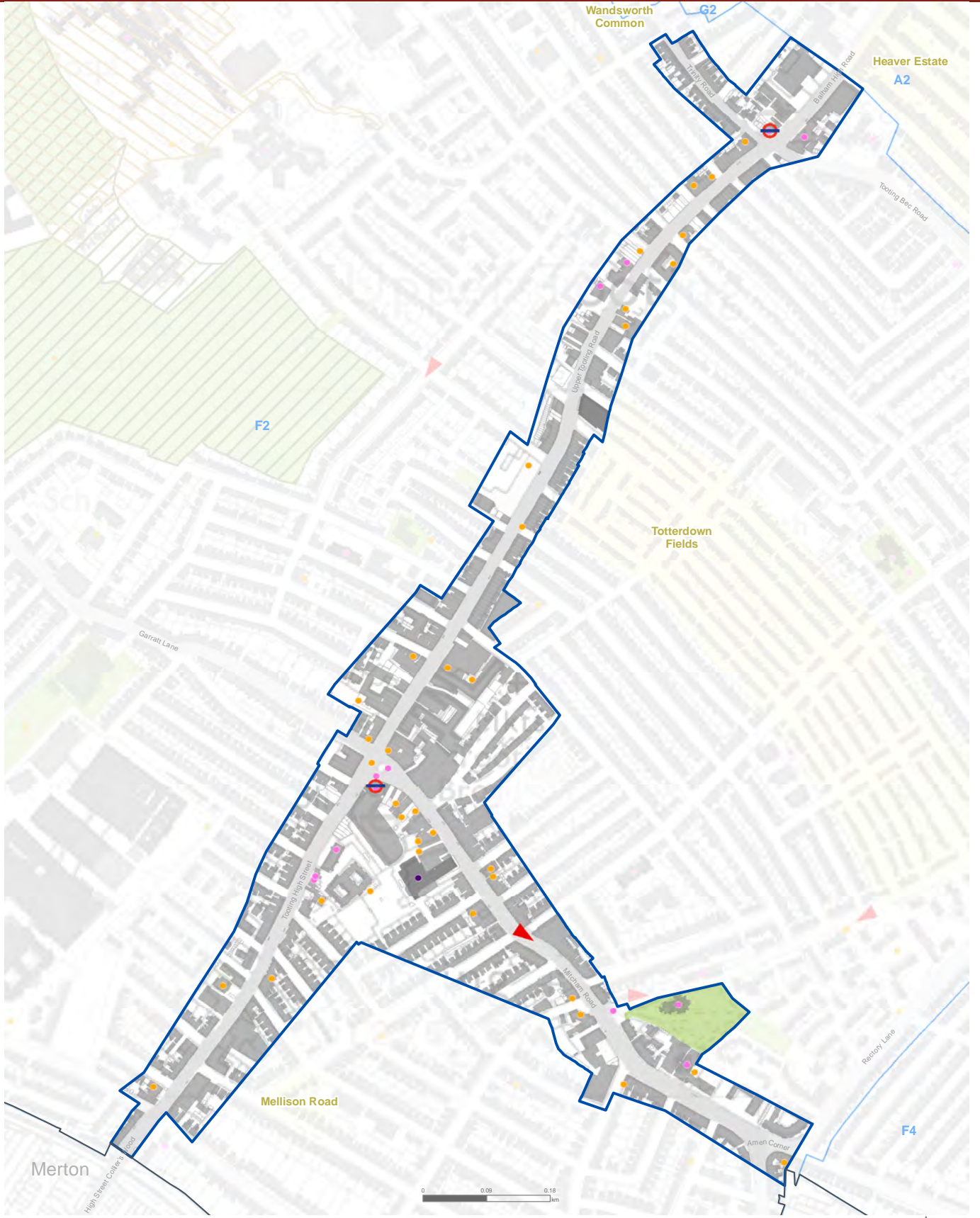
- underground stations (both grade II listed);
- former Granada Cinema (grade I listed);
- locally listed Tooting Market ([Fig. 173](#));
- memorable iron gas lamp standard outside Tooting Broadway ([Fig. 175](#));
- Kings Head pub (grade II listed), the Castle pub (locally listed) and the Wheatsheaf pub a positive frontage to the junction at Tooting Bec;
- Tooting Library (locally listed, [Fig. 176](#));
- RACS Building (locally listed);
- Churches of St Boniface and St Nicholas (both grade II) and their green churchyard settings;
- the curved form of Amen Corner parade at the end of Mitcham Road (locally listed).

**Balanced proportions, with building heights of between 2-4 storeys.** This provides a human scale and ensures the landmark buildings remain landmarks.

**A vibrant destination for food and nightlife,** and town centre functions such as shops, restaurants, pubs and public services. It has excellent public transport accessibility, with Northern Line stations at Tooting Bec and Tooting Broadway, and a National Rail line at nearby Tooting train station (outside of the borough). Tooting's entertainment was once prolific, with dancing at the RACS building, famous music concerts at the Granada theatre and numerous cinemas. A few of these distinctive buildings survive in different guises but commercial entertainment has a low profile.

**An urban feel, exacerbated by a busy, congested high street, lack of green and public space.** The town centre is vehicle dominated, with poor air quality at Tooting Broadway crossroads. There is also a predominance of garish Shopfronts. The busyness and clutter detracts from the historic interest of the built fabric. However, the open space around the Church of St Nicholas on Mitcham Road, provides a welcome respite to the hard urban realm.

**Diversity, colour and interest.** The proportion of residents of mixed/multiple ethnic group is relatively high for the borough, close to the London average. A high percentage of Asian residents has brought about many much-loved curry houses. Tooting Market, as well as food stalls and independent shops along the high street create a vibrant and exciting place. Diverse faiths and cultures are a defining and cherished feature of Tooting. Churches and other faith buildings (including those converted from previous uses) are prominent and positive features of the high street.



**Fig. 174:** F1 Tooting Town Centre character area plan  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Landmark buildings, particularly those at junctions for creating structure, legibility and sense of place.
- Buildings of high architectural quality, particularly historic buildings, including pubs (listed and non-listed).
- The balanced proportions along the two high streets, which creates a human scale to the street scene.
- Tooting Market, a rich and exciting destination, although a bit hidden behind street clutter. The market epitomises the cultural diversity and food culture that Tooting has become renowned for.
- The town centre functions including community facilities, shops and local independent services as well as restaurants/food, entertainment and places of worship, which helps create a sense of vibrancy.
- The distinctive character, green, trees and openness of the churchyard of St Nicholas Church.
- Views and vistas of interest, including the view south

- along Mitcham Road towards the trees and spire of the Church of St Boniface.
- Proximity to the nearby Commons, the Wandle Trail and other open green spaces.
- Diversity of people and cultures creating a vibrant, bustling and friendly area.
- Strong transport links including the London Underground and bus routes.



**Fig. 175:** The landmark grade II listed cast iron candelabrum at Tooting Broadway station



**Fig. 176:** The handsome terracotta Tooting Library on Mitcham Road (locally listed), opened in 1902



**Fig. 177:** Street trees enhance the view south along Mitcham Road with the spire of St Boniface Church

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Buildings are in a mixed state of repair, which overall, results in a fragmented feel to the streetscape. There is mixed management and maintenance of Shopfrontages along the high street, with some blank façades, which are detrimental to the experience of the town centre. In many cases historic features of upper storeys have become degraded. The built fabric deteriorates towards Tooting Bec.
- Some large retail units dominate due to their mass and footprint (rather than height).
- Heavy traffic, combined with narrow pavements creates a busy, and noisy environment along the High

Street.

- Absence of a distinct identity to Tooting Bec, with some examples of poor frontages to the street (e.g. industrial units on Balham High Road).
- South Thames College and Sainsbury's detract from the activity and proportions of the High Street in their key location.
- Absence of public open space including benches, poor air quality, street clutter, garish Shopfronts and high traffic noise diminishes the quality of the townscape for pedestrians.
- Presence of a large ground level car park behind the Granada building on Mitcham Road.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Shopfront terraces;
- Town centre retail;
- Mid and high-rise mixed use.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

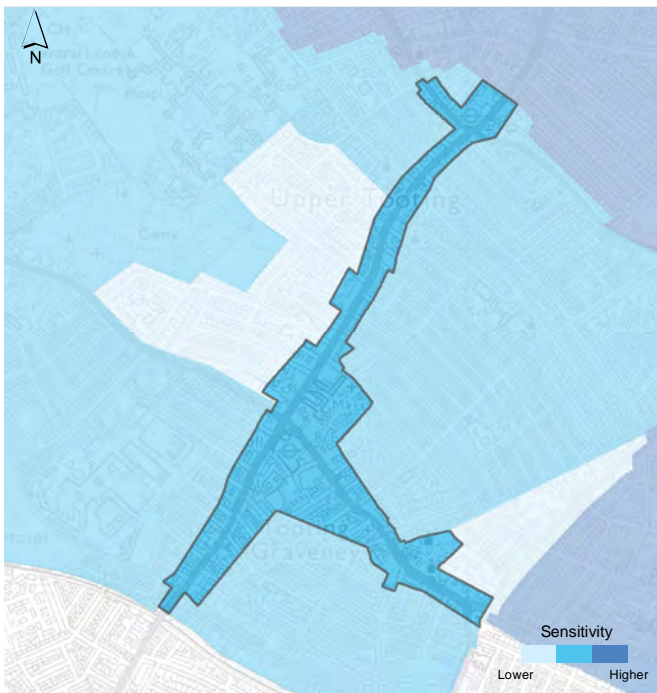


Fig. 178: F1 Tooting Town Centre sensitivity plan

Overall, Tooting Town Centre has a medium sensitivity to change, with the potential for targeted development.

The character area has the potential to incorporate change and new development, as long as this respects:

- the proportion, scale and material quality of the remaining period buildings;
- the existing landmark buildings, preserving their setting and contribution to the skyline; and
- the function of the streetscape as a shopping and leisure destination.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve

**Restore**

Improve

Transform

Tooting has a strong character in terms of community and culture, although its streetscape could be improved to enhance the condition and setting of the high quality landmark buildings.

Therefore the strategy is to **restore** the character to enhance the quality of public realm, the maintenance of remaining characteristic features and ensure future growth reinforces existing character.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Refer to the [Tooting Bec and Broadway Neighbourhood Characterisation Study](#).
- Establish guidelines and standards for ground floor Shopfrontages to ensure they contribute to the overall quality of the street scene.
- Ensure good maintenance of building façades and public realm, de-cluttering and litter.
- Widen pavements and introduce further traffic management including changes to Tooting Broadway junction to reduce the dominance of cars and improve pedestrian / cyclist safety.
- Highlight pubs and civic building landmarks through public realm improvements and protecting their setting and proportion.
- Establish more green infrastructure, particularly street trees, within the public realm. Improve provision and quality of public green space to counteract the dominance of traffic.
- Support independent retailers to provide a diversity of commercial offerings.
- Re-introduce and retain historic features, enhancing their prominence in the street scene.
- Create new destinations to enhance the cultural experience inspired by its past e.g. theatre/music.
- Enhance sense of place around Tooting Bec, accentuating the sense of arrival from Balham.
- Opportunities for buildings 5 storeys and over are restricted to some areas around the larger Tooting Market plot, due the small-scale character.

## F2 Tooting Residential



Fig. 179: Two storey period terraces on Totterdown Street

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Tooting Residential is characterised by period terraced streets surrounding Tooting Town Centre. The large footprint of St George's Hospital in the south west of the area is complemented by large green cemeteries nearby. There is a gradual transition in character to Furzedown to the east, Earlsfield to the west and Balham to the north.

**The area grew rapidly in the Victorian period**, with rows of terraced housing laid out in proximity to the main road junctions of the High Street with Mitcham Road ('Tooting Graveney') and Trinity Road ('Tooting Bec'). Between 1901-11 London County Council built a pioneering new cottage estate for working families: the Totterdown Fields Estate. Its development grew further following the extension of the Northern Line to Tooting Bec and Tooting Broadway in 1926.

**Residential character, of 2-3 storey period terraced and semi-detached houses in fair condition.** The area is characterised by densely packed, tight-knit streets of houses, many of which have been converted to flats. House sizes and quality increase around Tooting Bec in the north, and east of the town centre, where street trees create more of a green feeling. West of the town centre houses are modest in scale; standard Victorian terraces, generally lacking in details and utilitarian in style. Materials are sometimes poor quality and have become degraded. Various modifications (such as removal or rebuilding boundary walls, façade repairs, pebbledash

and paint) over time have led to a lack of cohesiveness. Typical materials are London stock and red brick.

**Balanced proportions**, where narrow streets have modest scale houses. Around the hospital in the south west of the area, large green spaces and larger footprint mansion blocks make a coarser urban grain. The 3-4 storey mansion blocks such as Bellamy House help to define the wider Garratt Lane; here architectural details such as stone work above main entrances and the high quality red brick, add real texture and distinctiveness to this part of Garratt lane. Similarly, the 2 storey Anderson House on Fountain Road provides strong presence; its mass broken up by courtyards and green space, and the 4 storey Moira Court on Trinity Crescent.

### **The 'cottage feel' in the Totterdown Fields**

**Conservation Area** - which is particularly distinctive, attractive, rich and special. The streetscape is relatively green, with mature street trees and green front gardens. There are a mix of building styles but a sense of cohesiveness from the high quality and historic character. Totterdown Fields has historic significance, being the first 'cottage estate' in London.

**Local parades of shops are important in providing community function and sense of place in the dense terraced streets.** For example, local shops at the western end of Garratt Lane (near Khartoum Road).

**Open spaces are located towards the periphery of the character area**, including Lambeth Cemetery and Streatham Cemetery, as well as Tooting and Wandsworth Commons (adjacent to, but outside of, the character area). There are few public open spaces within the residential areas themselves. Although there are some private back gardens, these are being lost to residential extensions and conversion into flats.

**A strong sense of community**, but a population that is changing fast, with the influx of young professionals priced out of more central London locations, and signs of gentrification. Tooting's population has a diversity of faiths, cultures and backgrounds, and is known for its British Asian community.

Landmarks include:

- Tooting Primary School (grade II listed), a well-preserved example of a London board school c1900
- Grade II listed St Nicholas Parochial Hall;
- Church of All Saints (grade II listed);
- The red brick mosque on Gatton Road.



## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Remnant historic details, such as the 'Heating Plumbing Merchants' painted sign on the corner of a building at Rostella Road/Garratt Lane.
- Totterdown Fields Conservation Area, valued for its distinctive cottage-style character and as a fine example of properties influenced by the Garden City and Arts & Crafts movement.
- The unusual and attractive use of knapped flint in the Mellison Road Conservation Area, on the façades of houses and for boundary walls.
- Church Lane: views of the Church of St Nicholas and adjacent churchyard (although within Tooting Town Centre character area), grade II listed St Nicholas Parochial Hall (Fig. 182), and the red brick walls, piers, gates and postbox features marking the former location of the former St Benedict's Hospital (locally listed). The combination of mature trees and historic features give Church Lane a sense of richness, local distinctiveness and tranquillity.
- Street trees enhance views, soften the dense urban realm, and provide environmental resilience. Groups of trees which add special value include those in front of St Anselms Convent School.
- The green character of the cemeteries and green spaces, and their value for relaxation and recreation including Hebdon Road playing fields, Garratt Green, Figges Marsh and Springfield Hospital.
- High quality materials including red brick which add a sense of place and rich texture.
- Landmarks and individually distinctive and high quality buildings, many of which are locally or nationally listed for their historic value.
- Views downhill towards the hospital from the northern end of Fishponds Road, along the tree-lined street to St Nicholas Church.
- Springfield Hospital and its heritage assets.

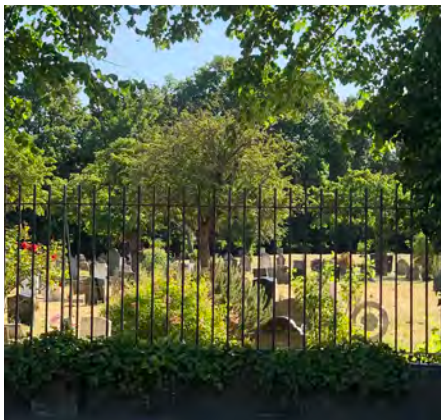


Fig. 181: View into Lambeth cemetery from Blackshaw Road



Fig. 182: Grade II listed St Nicholas Parochial Hall on Church Lane



Fig. 183: Red brick cottage-style house on Derrington Road in the Totterdown Fields Conservation Area

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Some maintenance issues, degraded front boundaries and gardens, including degradation of period features over time which has resulted in a mixture of different coloured painted frontages and walls which fragment the character of the street scenes (predominantly to the west of the High Street). Issues with fly tipping in some locations.
- Simple, plain and uninteresting frontages e.g. along Fountain Road, the 2 storey houses lack interest, whereas the wide road could benefit from more imposing frontages (3-4 storeys) such as Anderson House.
- Modern infill estates e.g. south and east of Church Lane, lack sense of place, are dominated by hard standing and car parking and lack connectivity with the surrounding area, with poor pedestrian environments and little green. Beville Allen Close housing estate lacks active frontage to the main road.
- Hard and urban feel with a lack of green and public open space.
- Ernst Bevin College which detracts from the residential character of Beechcroft Road.



## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Semi-detached;
- Low-rise estates;
- Mid-rise estates;
- Low-rise flats;
- Mid-rise flats.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

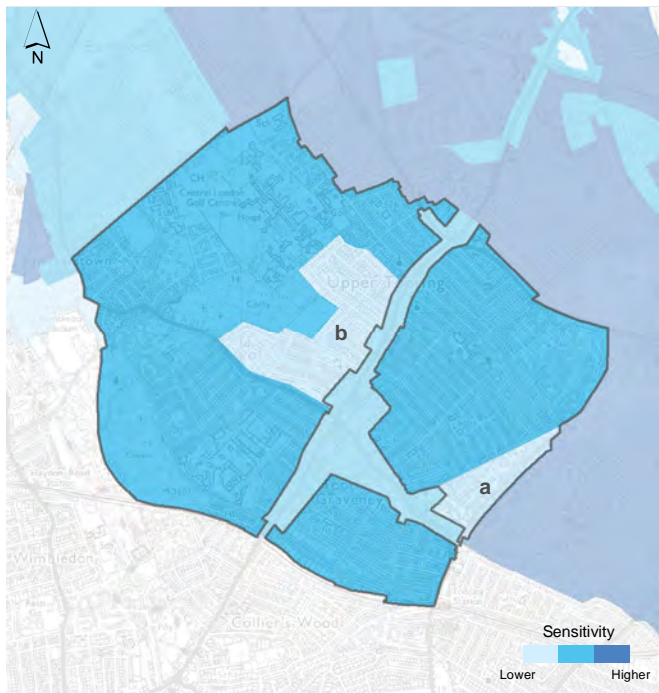


Fig. 184: F2 Tooting Residential sensitivity plan

Overall, Tooting Residential has a medium sensitivity to change.

More highly valued and cohesive areas have higher sensitivity; these include:

- the conservation areas;
- the terraced streets east of the town centre which are in good condition;
- the open spaces.

Positive change should be encouraged in areas of lower sensitivity where this can enhance character, including:

- along main roads;
- the modern estates around Church Lane, sub area (a);
- the terraced streets west of the town centre which are in poorer condition, around sub area (b).

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.



Overall the character of Tooting Residential is fragmented, with poor condition of built fabric in some places and some areas lacking sense of place. Therefore, the strategy is to **improve** character, particularly along the main roads, where character could be enhanced through street tree planting and buildings with improved frontage to the street.

Any new development should use historic references from the Tooting Residential area wherever possible to ensure they build upon existing character.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Refer to the [Tooting Bec and Broadway Neighbourhood Characterisation Study](#).
- Encourage upkeep of boundaries, façades and planted front gardens, and increase street trees.
- Respect the scale and proportions of existing buildings and streets. Most streets would not be able to accommodate buildings over 3 storeys.
- Enhance legibility of the area through adding high quality landmarks along main roads and key junctions which respect prevailing building heights and street widths (e.g. 3-5 storeys may be appropriate fronting wider roads with some potential for 6 and 7-8 storey buildings in the west of the area near the hospital and Lambeth Cemetery).
- Enhance the quality, functionality and biodiversity of green spaces in estates e.g. by incorporating play equipment, seating and management regimes which encourage wildlife.
- Conserve, repair or reinstate original period features such as windows, boundary walls, traditional materials and front doors.
- Retain existing local parades and local facilities.
- Respect the settings, proportions, scale and details of landmark features.
- Encourage use of characteristic materials such as high quality red brick and London stock brick.
- Improve definition of the edge of Tooting Bec town centre.
- Refer also to masterplans for the listed hospital.

## F3 Tooting Common



Fig. 185: Tooting Common

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Tooting Common character area encompasses the large public open space of the Common, and the adjacent residential area within its setting, dominated by the Streatham Park Conservation Area.

**Tooting Common is historically two commons:** Tooting Bec Common and the smaller Tooting Graveney Common, which were once part of much larger medieval manors. The commons were protected through an Act of Parliament in 1875 in response to local petitions against its enclosure by a wealthy landowner, and managed for recreation and sport. Over time the common land became more formalised, with laying of paths, development of amenities and formation of the artificial lake following gravel digging. During the World Wars the common was an essential resource for the local and metropolitan population as a site of recruitment, training, bomb shelters, temporary prefab homes and food production.

**Sense of openness** provided by the Common, a well-maintained public open space and designated Metropolitan Open Land with mature trees. The sense of space is continued through into the Streatham Park residential area (inside and outside the Conservation Area), with detached houses and estate blocks up to 6 storeys well-integrated into the landscape, surrounded by green spaces and mature trees. There is a **leafy,**

**green feel** over the whole area, and although roads pass in proximity they are not very apparent.

**A wildlife haven** with a mosaic of habitats including acid grassland, woodland, tree avenues, and open standing water. Tooting Common is designated as a Site of Metropolitan Importance for Nature Conservation. It also contains a number of veteran trees and historic pollards which can be associated with its historic management as wood pasture. The lake on Tooting Common supports a range of invertebrates and aquatic plants, waterfowl and bats.

**Historic character**, including the tree avenues: one planted in 1600 along Dr Johnson Avenue. There are remnants of the 19<sup>th</sup> century estates including Furzedown Lodge (Fig. 188), one of the last remaining gate-lodges in the Streatham and Tooting area. Other non-listed features in the common include an art deco style drinking fountain and fossilised tree stump.

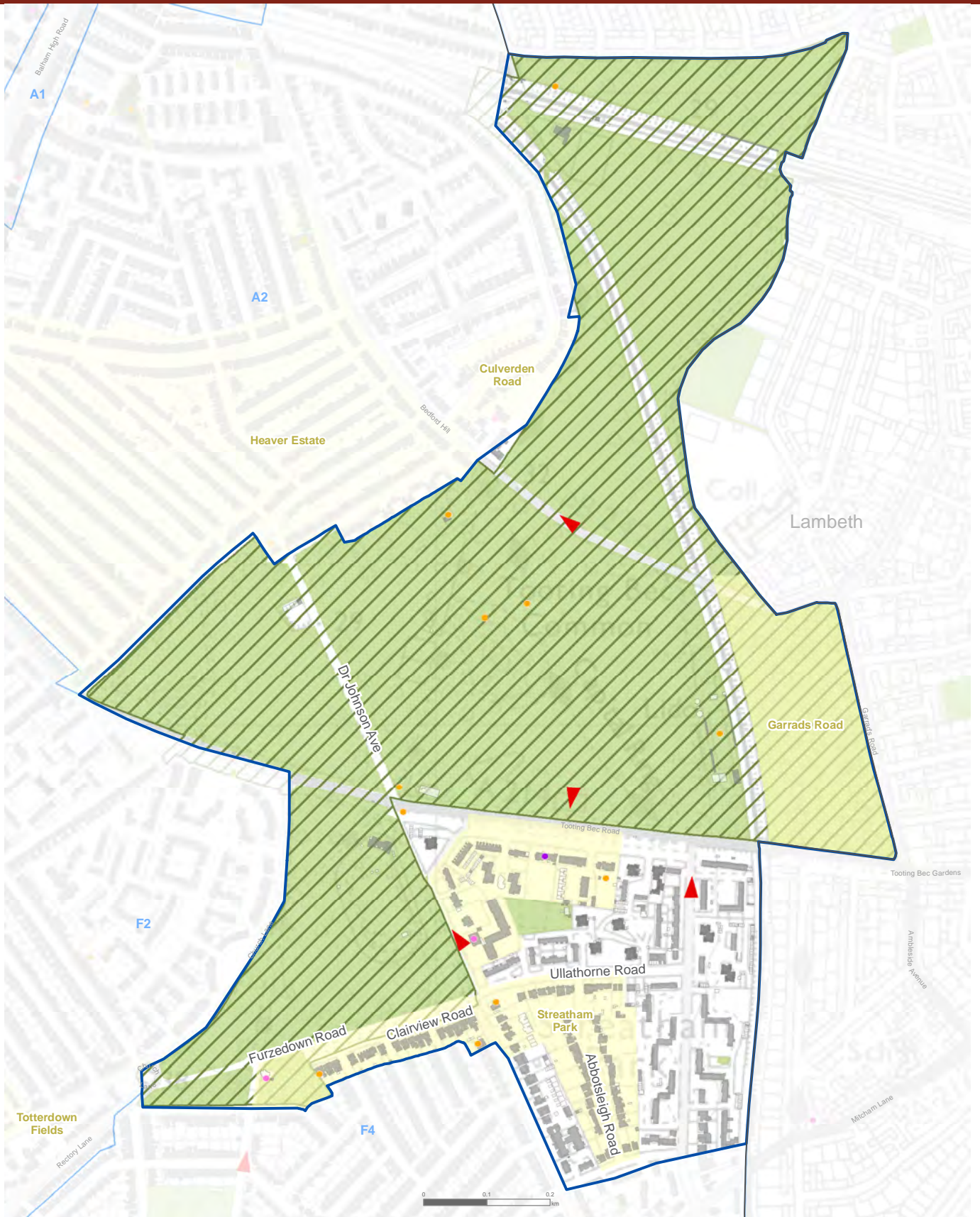
**A destination for sport, leisure, socialising and relaxing** with facilities including playground, Tooting Bec Lido, football pitches, car parks, tennis courts, open areas of grass and fishing in the lake. **Tooting Bec Lido**, dating from 1906, the largest open-air freshwater swimming pool in England at just over 90m.

**Sense of calm, relative tranquillity and peacefulness** as a result of mature trees screening traffic and reducing perception of noise and pollution. Areas of dense planting, such as around the lake, add richness, sense of wildlife and enclosure to parts.

**Streatham Park Conservation Area**, its special character a result of the relationship between the groups of detached and semi-detached late 19<sup>th</sup> century and early 20<sup>th</sup> century houses, particularly the frontage to Tooting Bec Common, as well as the historic landscape and vestiges of woodland surviving from large estates that were sold for development around the end of the 19<sup>th</sup> century. This gives the area a very green suburban character of low density and pleasantness. The margins of Tooting Bec Common in Clairview Road and North Drive and the grass verges with trees to Ullathorne and Abbotsleigh Roads give the impression of the common flowing into the area reinforcing its landscape character. Houses are generally 2 storey and materials predominantly red brick.

### **Relatively few landmarks, including:**

- Furzedown Lodge, grade II listed;
- Tooting Bec Lido (locally listed).



**Fig. 186:** F3 Tooting Common character area plan  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Tooting Common, designated Metropolitan Open Land and a locally listed garden. It is valued for recreation, leisure and its role in providing space for socialising and relaxation.
- Mosaic of habitats on the common including acid grassland, woodland and standing water habitats (priority biodiversity action plan habitats) in an otherwise densely developed area. Open standing water represents one of the most diverse of all ecological habitats in London.
- Sense of openness provided by the Common, a well-maintained public open space with mature trees. The sense of space is continued through into the Streatham Park residential area, with buildings in spacious plots, allowing views through, and their surrounding green spaces, gardens, grass verges and mature trees.
- Tooting Bec Lido.
- Veteran trees, particularly on the common, and street trees which make an important visual contribution in the residential areas and along main roads such as Furzedown Road, providing a perceptual link to the common.
- Streatham Park Conservation Area including the listed and non-listed buildings and features and landmarks contributing to its special character.
- Historic character including Furzedown Lodge, the former gatehouse to Furzedown House (within Furzedown Residential character area).
- Valued views including those looking out from the roads that approach the common and from the common towards the buildings in Streatham Park Conservation Area.
- Local shops and diverse selection of restaurants in the local area.



**Fig. 187:** Tooting Bec Lido with the coloured doors of the changing cubicles



**Fig. 188:** Listed Furzedown Lodge in Streatham Park Conservation Area, formerly the lodge to Furzedown House



**Fig. 189:** Busby House on the Streatham Park Estate set in green open grounds

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Some maintenance issues on houses edging the Common in the conservation area.
- Traffic noise in some places.
- Palisade fencing around Tooting Bec Lido provides a harsh visual edge from some views.
- The railway bridge underpass in the northern part of Tooting Common suffers from poor maintenance and feels unsafe.
- Width of the roads in some places, which make it difficult to accommodate multiple different user types.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Semi-detached;
- Detached;
- Mid-rise estates.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

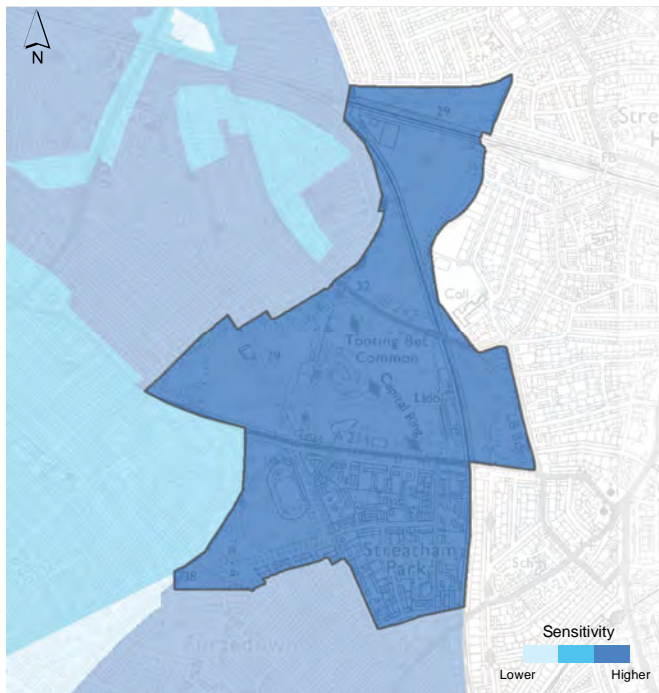


Fig. 190: F3 Tooting Common sensitivity plan

Overall, Tooting Common has a high sensitivity to change with no potential for extensive growth.

The estates may be able to accommodate some intensification including infill of undercroft spaces and increase in height of low-rise elements but their sense of openness should be retained.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

**Conserve**

Restore

Improve

Transform

The existing character of the area is strong, with many valued features and distinctive sense of place. Therefore the strategy is to **conserve** the character, elements and features, whilst enhancing existing features where appropriate.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Refer to the [Tooting Common Management and Maintenance Plan](#), [Tooting Common Conservation Plan](#) and the [Streatham Park Conservation Area Appraisal and Management Strategy](#).
- Protect the area's green character and sense of openness. Green buffers to the roads are particularly important in reducing the perception of noise, pollution and traffic.
- Maintain the green skyline, paying particular attention to new development in other character areas which may affect this.
- Preserve and maintain street trees and green front gardens. Preserve historic tree avenues on the common, planning for replacement of trees as they reach the end of their life.
- Enhance biodiversity on the Common e.g. grassland management regimes, improving connectivity between habitats, tree management.
- Ensure any future house extensions retain the sense of spaciousness between houses.
- Protect and reinstate historic features including listed buildings, and their settings.
- There are limited opportunities for buildings of 5 storeys - limited to the area of existing mid-rise estates close to Tooting Commons.

## F4 Furzedown Residential



Fig. 191: Two storey terraced houses with cross-gabled roofs on Mitcham Lane

### Key characteristics

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Furzedown Residential encompasses the residential streets south east of Tooting, towards London Boroughs Merton and Lambeth. It includes Mitcham Lane local centre.

**The name 'Furzedown' dates from the 17<sup>th</sup> century, and refers to the gorse that grew there.**

**The origins of Furzedown lie with Furzedown House** (grade II listed, now a school), which was one of the only buildings in the character area until around 1900, aside from a couple of farms and a focus of settlement around Mitcham local centre associated with the railway to the east. The existing residential roads were laid out on the former estate's grounds - on land that had been parkland and a golf course - between 1900 and 1930, after it was sold.

**Topography in the area slopes down towards the River Graveney, a tributary of the Wandle, to the south west.** Furzedown House occupies a local high point. The regular pattern of north east/south west orientated roads gives rise to distant views to the south towards Mitcham, with the gas holder a recognisable feature (Fig. 194). The long views with wooded horizons emphasises the more spacious, suburban character compared with Tooting to the west.

**Residential, semi-suburban character, with period terraces of consistent heights** of 2 storeys and

detached/semi-detached houses laid out in a regular, consistent grid pattern. Façades are a mixture of render, pebbledash and brick with a consistent roofline giving a sense of openness. Architectural details on Victorian terraced houses above windows and doors add visual interest. Original features such as sash windows and cross-gabled roof give a richness to the streetscape.

Small inter-war and post-war estates include taller buildings such as the 11 storey tower next to Furzedown House. Although the tower detracts from the setting of the historic building, it does not detract from wider character as it is well integrated into its surroundings with trees and open space.

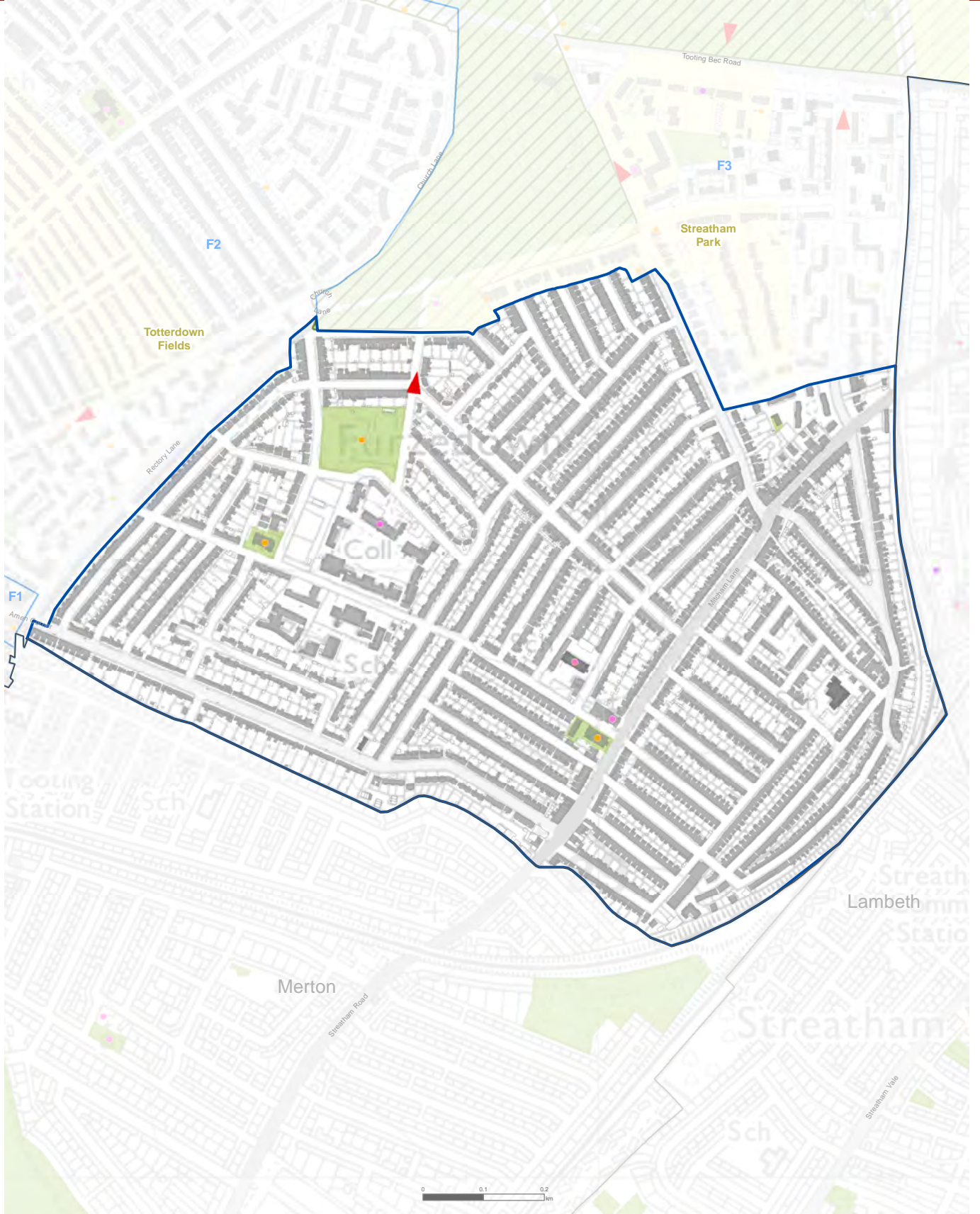
**Relatively frequent street trees and back gardens contribute to a reasonable proportion of green cover in the area.** There are few public green spaces with the exception of Furzedown Recreation ground. The green space was created in 1924 by the Council as part of the new housing scheme having previously been attached to the historic Furzedown House. It still contains some remnants of 19<sup>th</sup> century planting.

**Mitcham Lane local centre**, provides shopping and community facilities. Streets are fronted by 3 storey consistent Shopfront terraces in red brick and the Furzedown pub occupies a prominent corner location. Local parade of shops on Moyser Road provide shopping and community services.

**Relatively poor public transport accessibility**, although Tooting Station (in LB Merton) and Streatham Common Station (in LB Lambeth) provide rail access.

Landmarks include:

- Penwortham School (grade II listed), a 3 storey building of yellow stock brick with red brick dressings, built in the Queen Anne style for the London County Council in 1907-8. It occupies a prominent position on a hill overlooking the valley of the River Graveney which the verticality of its design emphasises;
- Furzedown House (grade II listed);
- the pair of churches on Mitcham Lane (Mitcham Lane Baptist Church, grade II listed and St James Church, locally listed);
- Goldfinch Primary School;
- The Furzedown pub on Mitcham Lane.



**Fig. 192: F4 Furzedown Residential character area plan**  
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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Listed buildings including Furzedown House (grade II), dating from 1794, now a teacher training college.
  - Architectural details on Victorian terraced houses which give a richness and visual interest to the streetscape.
  - Intact front boundaries, street trees and green front gardens which soften the public realm.
  - Furzedown recreation ground, a locally listed garden with remnants of 19<sup>th</sup> century planting.
  - Furzedown Christmas Market on Moyser Road, a locally valued community event.
  - Open views along streets and downhill, as a result of the topography in combination with the regular street pattern. Views to wooded horizons creates a feeling of space and a semi-suburban, peaceful character.
- Local amenities including schools, nurseries and churches creating a family centred community spirit.
  - Proximity to Tooting Common with its extensive facilities including the lido.



**Fig. 193:** Locally listed St James Church on Mitcham Lane



**Fig. 194:** Distant view downhill from Nimrod Road to a wooded horizon and gas holder in Mitcham



**Fig. 195:** Typical period terraces on Welham Road, with intact front boundary walls and hedges

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Some areas could benefit from better maintenance, including pavements and boundary walls, which have been replaced by timber fences, although overall the condition is fair.
  - Modifications to houses such as doors, porches and modern windows detract from the character of the streets.
- Poor condition of some of the Shopfronts in Mitcham Lane which has been raised by representatives of the local community.
  - The 11 storey tower on Spalding Road which detracts from the setting of the listed historic Furzedown House.



### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Semi-detached ;
- Detached.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

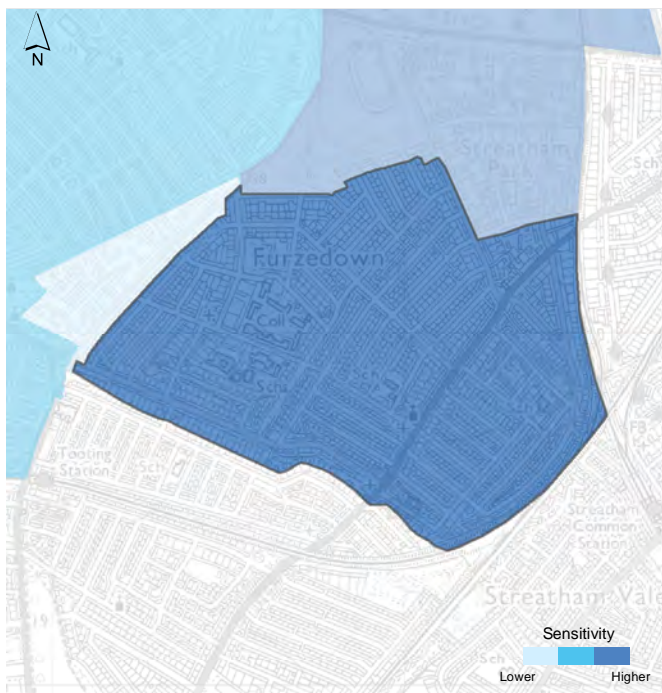


Fig. 196: F4 Furzedown Residential sensitivity plan

Overall, Furzedown Residential has a high sensitivity to change. This is owing to the high susceptibility of consistent building heights and sense of spaciousness in the borough which may be negatively affected.

Therefore, any new change should carefully consider its design to fit into its surroundings.

Wider, busier roads such as Mitcham Lane can more easily accommodate denser development, but not particularly taller.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.



The existing character of Furzedown Residential lacks distinctive sense of place, therefore the strategy is to improve character by retaining positive features, restoring historic elements and adding new positive features to enhance local distinctiveness.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Encourage upkeep of boundaries, façades and planted front gardens.
- Increase street tree planting.
- Respect the scale and proportions of existing buildings and streets. Most streets would not be able to accommodate buildings exceeding the prevailing height.
- Enhance legibility of the area through adding landmark buildings along main roads and key junctions. New landmark buildings should be high quality design and respect the prevailing building heights of the area and be in proportion to the width of the streets (e.g. 3-5 storeys may be appropriate fronting wider roads).
- Enhance the quality, functionality and biodiversity of green spaces in estates e.g. by incorporating play equipment, seating and management regimes which encourage wildlife.
- Conserve, repair or reinstate original period features such as windows, boundary walls, traditional materials and front doors.
- Retain existing local parades and local community facilities.
- Respect the settings, proportions, scale and details of landmark features.
- Encourage use of characteristic materials such as high quality red brick and London stock brick.
- No capacity for buildings of 5 storeys and over has been identified.

# G Wandsworth Town and Common



Fig. 197: Wandsworth Common has long been at the heart of the area, but feels quite detached from the busy town centre

## *Summary*

The Manor of Wandsworth appears in the Domesday book. The town's original centre was at All Saints Church on the High Street, and industry at the confluence of the River Thames and Wandle. Houses and industry developed together in a tight knit format. Wandsworth Common has been recorded as such since 1741, although eroded over time by development and the construction of the railways.

Significant redevelopment of Wandsworth occurred in the 1970s and 80s, including the Arndale shopping centre and Swandon Way. This was the first introduction of towers in the centre of the area. The housing around the common remains highly intact period Victorian and Edwardian properties.

## *Key changes and trends*

- Wandsworth town centre is a major redevelopment zone within the borough, with potential development areas along the High Street, along the Thames frontage and through the Wandle Valley.
- There is also a large economic use intensification area around the A3 and railway lines, where commercial premises will continue to be encouraged and increased density explored.

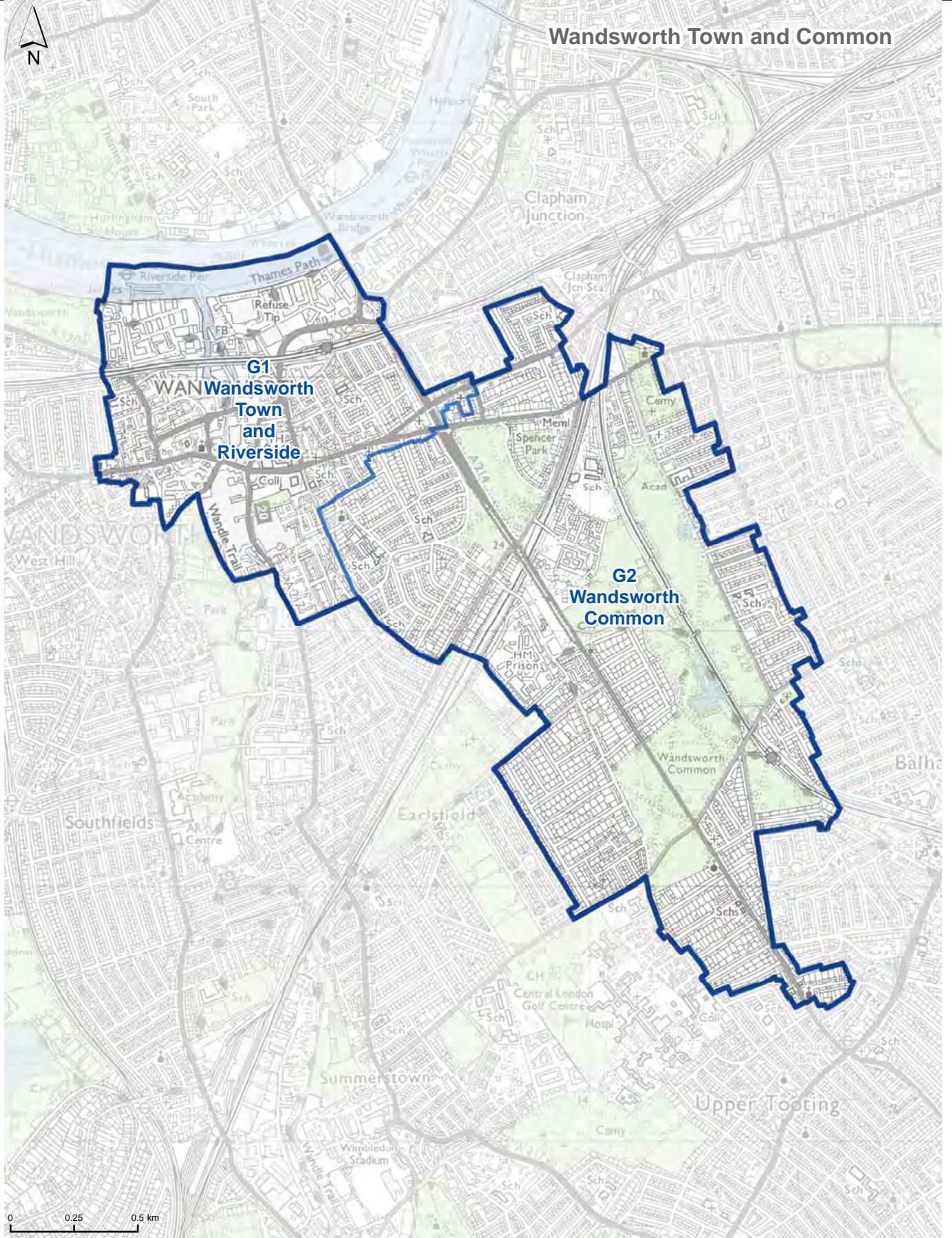


Fig. 198: Place G: Wandsworth Town and Common character areas plan

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# G1 Wandsworth Town and Riverside



**Fig. 199:** Town Hall (grade II listed) forms a grand civic landmark at the junction of Wandsworth High Street/East Hill/Fairfield Street/St Ann's Hill

## Key characteristics

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

Wandsworth Town and Riverside encompasses the town centre, the River Thames and Wandle frontages, Wandsworth Town station and adjacent Old York Road, and period terraced streets.

**Historically Wandsworth was a centre of industry, using the power generating capacity of the River Wandle.** Industry has been a prominent part of Wandsworth's history since the Middle Ages, including brewing, and flour and snuff milling. Craft industries were fashioned by the Huguenots in the 17<sup>th</sup>-18<sup>th</sup> centuries. The Ram Brewery complex is a prominent feature of the town centre. The Wandle and Thames are a fundamental part of the area's historic evolution, but have little connection to the town centre today.

**A town centre dominated by roads, primarily the ancient main road (the current A3), rising up the valley sides.** The loop of roads comprising the A3 (West Hill/Wandsworth High Street/East Hill), Wandsworth Plain, Frogmore and Putney Bridge Road, mark the historic road layout. These, alongside other main roads including Armoury Way and Swandon Way, result in a busy vehicle-dominated town centre.

**Valley topography gives rise to high levels of intervisibility across the character area.** Wandsworth lies in a valley, with the pronounced topography giving rise to interesting, sweeping and long-ranging views rare in a built-up area, including from East Hill and the

residential areas towards the town centre and riverside.

**All Saint's Church (grade II\*) forms the centre of the town's historic core, at the heart of Wandsworth Town Conservation Area.** Early Metropolitan houses (1723) in Church Row (grade II\*) are of special architectural and historic importance, although its prominence has been diminished by new development.

**Historic buildings, of civic, industrial and residential use, create a rich townscape and distinctive character.** Buildings of interest include the barber shop, churches and town hall. Characteristic materials include yellow stock brick, red brick details and occasional use of ceramic tiles. Historic buildings are grouped together, with houses and industry cheek-by-jowl. The juxtaposition of land uses is sometimes awkward, such as the tyre shop on the high street and the storage unit on Putney Bridge Road.

**Building heights range between 2-4 storeys and towers up to 27 storeys.** Towers have recently become a feature of the skyline, sometimes uncomfortably relating to adjacent residential areas.

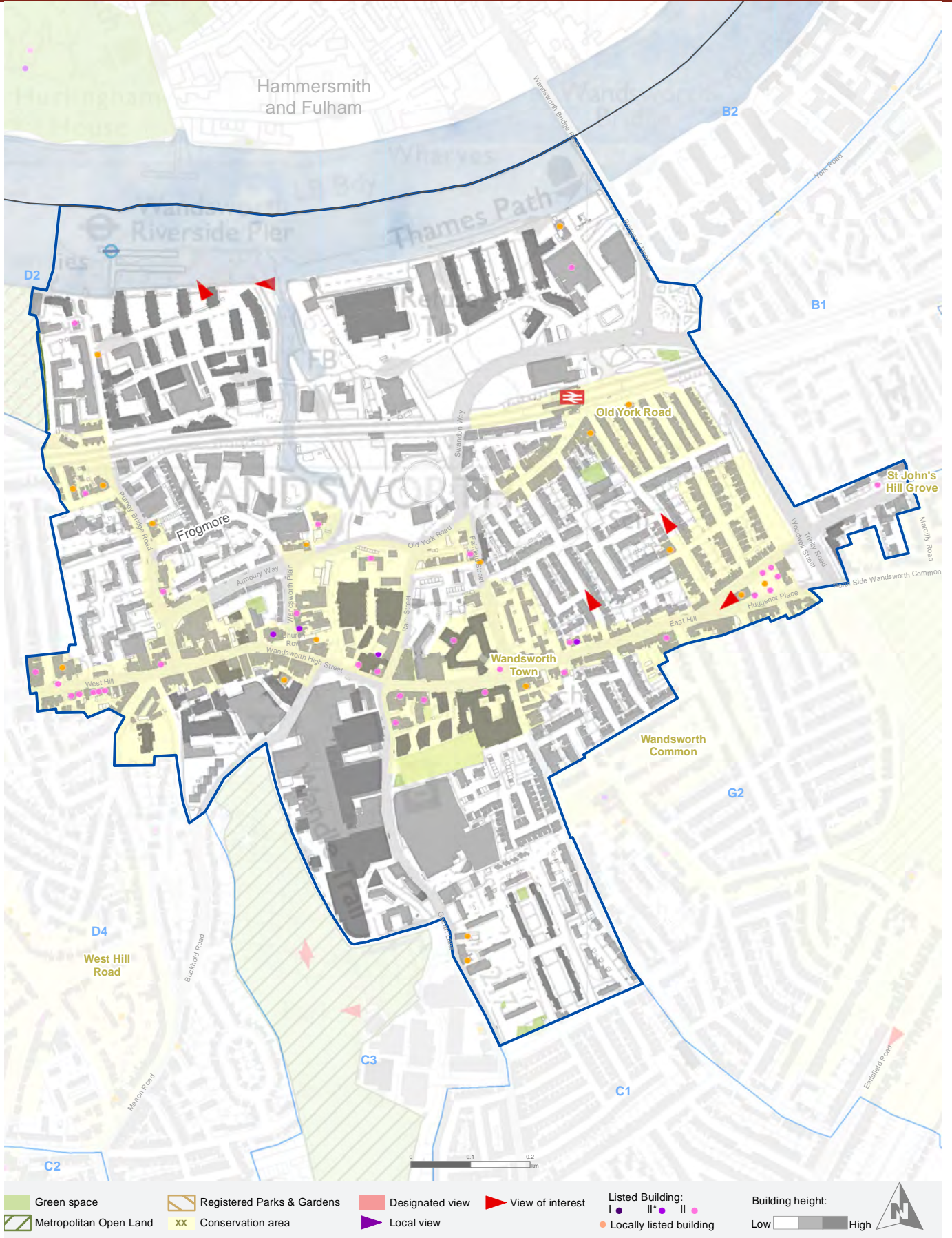
**Old York Road forms a 'mini high street'** with a distinctive character and sense of place, with consistent 2-3 storey London stock terraces creating a human scale pedestrianised streetscape.

**The area is undergoing substantial regeneration** which includes new development along the River Thames and the River Wandle.

**Landmarks at key junctions are important to the legibility and framework of the town.** The presence of grand civic buildings is important to the sense of place as the borough's civic centre. **Book House/Mount Nod triangle** on East Hill and **St Thomas's Church** (grade II listed) on West Hill form gateway markers to the town and Conservation Area. The Town Hall and South Thames College mark the transition from East Hill to the High Street. Other landmarks include:

- All Saints Church and Church Row;
- East Hill United Reformed Church (grade II);
- Ram Brewery and its distinctive chimney;
- Brewer's Inn, including its distinctive turret;
- Spread Eagle pub.

**Residential terraced streets east of the town centre** in regular grids to the north of East Hill. The streets are in good condition and well maintained. Houses are 2-3 storey; materials include Gault brick with Gothic Revival detailing.



**Fig. 200:** G1 Wandsworth Town & Riverside character area plan

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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- High quality historic buildings, including surviving parts of the old town, particularly those within Wandsworth Town Conservation Area such as Church Row (grade II\*), Ram Brewery (grade II\*), those in the Putney Bridge Road area including Prospect Cottages and the Arts and Crafts group in Oakhill Road, and other moments such as Wandsworth Bus Garage.
- River Thames - the sense of openness, views along the river, vibrancy of local boat traffic and the interest of the piers and moorings.
- The Thames Path, which is highly valued for leisure.
- Residential terraced streets in East Hill which are in good condition and well-maintained. The consistent front boundaries and retained details are important to the character of these streets.
- Old York Road's distinctive, high quality parade of shops and restaurants including the consistent terraces and the vibrant street scene.
- Green open spaces, including the nearby King George's Park, Garratt Lane burial ground and Huguenot burial ground.
- Landmark buildings, as described above.
- Views and vistas of interest, including rare long-ranging views, and:
  - views from East Hill towards the town centre;
  - views from the terraced residential streets towards the town centre and towards industry by the river;
  - views west along the Thames Path towards the green edge of Wandsworth Park (the park is within Putney Riverside character area).
  - northern bank of the River Thames including Hurlingham Park and Fulham Palace.
- The ecological value of the River Wandle in Wandsworth, designated as a Site of Importance for Nature Conservation (SINC).



**Fig. 201:** Green ceramic tiles on the Alma pub lend richness and texture to Old York Road streetscape



**Fig. 202:** Towers in the town centre appear above the rooftops of terraced houses on Tonsley Road



**Fig. 203:** Buildings in Wandsworth Riverside Quarter dominate the view of the River Wandle and Thames

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- The busyness, noise, clutter and pollution caused by traffic, significantly detracts from character and are obstacles to safe movement. It visually detracts from the character of historic assets such as the grouping of All Saint's Church and Church Row, the Ram Brewery and the River Wandle.
- There is a lack of clear, legible routes connecting the town centre to the River Thames and Wandle.
- Southside shopping centre has a negative effect on the town centre at a key junction.
- Dominance of large industrial buildings is unwelcome, including palisade fencing, blank façades and the presence of heavy good vehicles/waste trucks with smell, noise and pollution. These combine to make a harsh pedestrian environment and poor legibility. Industrial buildings dominate views.
- Signs of degradation with poor maintenance.
- New development is often poorly integrated, creating discordant juxtapositions between old and new development. Tall buildings dominate views.
- The Riverside Quarter lacks design quality. The character of the River Thames and path lacks interest, with inappropriate landscape treatments, including areas that feel private.
- Tall buildings by the narrow River Wandle make it feel dark and unpleasant, with cleanliness issues.
- Incongruous land uses in the town centre.

## Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Low-rise flats; mid-rise flats; high-rise flats;
- Shopfront terraces; town centre retail;
- Mid and high-rise mixed use;
- Small scale light industrial;
- Large floor plate retail/industrial.

## Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

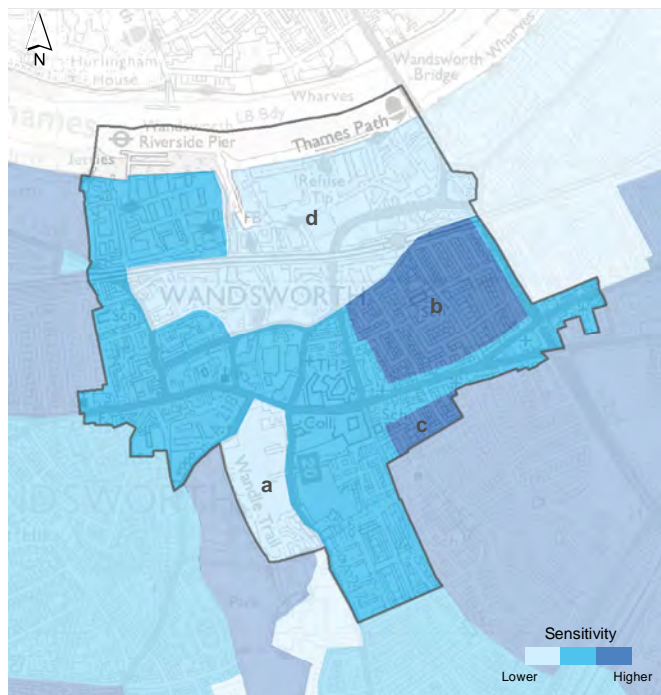


Fig. 204: G1 Wandsworth Town and Riverside sensitivity plan

Overall, Wandsworth Town and Riverside has a medium sensitivity to change, as the capacity for further growth - particularly towers and tall buildings - is nearing its limit.

The cumulative effect of existing and recent development has affected the character to a significant extent. Therefore there may be limited change for very tall buildings although growth in the right locations could enhance character.

- Southside, sub area (a) has a low sensitivity and potential for taller buildings around King George's Park, as long as they positively address the park and softened with trees. Tall buildings should be considered as part of a masterplan for the area.
- East Hill, sub areas (b) and (c) have high sensitivity due to their condition, consistency and character.
- Sub area (d), is a planned area of change with a low sensitivity to change. New development should positively enhance character.

## Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.

Conserve	<b>Restore</b>	Improve	Transform
----------	----------------	---------	-----------

Wandsworth Town and Riverside has many elements of strong character and distinctiveness, however, there are significant detractors which affect its sense of place, notably the busy roads.

Therefore the strategy is to restore the existing character by improving maintenance of remaining characteristic features, restoring historic characteristic features that have been lost over time, and ensure the future growth of the area reinforces existing character.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Enhance the pedestrian quality around both riversides. Prepare a movement strategy that enables clear public routes from the town centre, including along Swandon Way.
- Improve walking and cycling, including delivering the Wandle Trail and enhancing the Thames Path. Reduce the dominance of traffic, especially in the core of the town centre.
- Improve public open spaces and play provision.
- Retain and enhance the prominence of existing landmark buildings at key junctions. Enhance Fairfield Street as a core town centre street.
- Restore historic character by preserving and enhancing historic buildings and their settings.
- Ensure development respects the small scale of the Wandle river corridor.
- Ensure active ground floor uses.
- Respect the prevailing building height within the old town area (3-4 storeys), ensuring taller buildings behind them do not adversely impact on views, character or heritage value.
- Enhance the public realm around the Southside shopping centre including access to the Ram Quarter and river.
- Create distinct characters for the 'old town' and 'new town'.
- New development should consider views from residential areas and historic buildings and protect the character of the conservation areas.

## G2 Wandsworth Common and Residential



**Fig. 205:** Wandsworth Common is a well-used open space for running, socialising and activities

### *Key characteristics*

The combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.

**Wandsworth Common and Residential** encompasses the Common and surrounding residential streets of consistent period houses. Almost all of the area is designated as conservation area, reflecting its high quality townscape.

**Wandsworth Common** creates a unifying feature and visual backdrop to the character area, with mature trees around the perimeter, tree avenues, open grassland and lakes. It provides an excellent wayfinding landmark, and an important resource for leisure, recreation, relaxation and gathering. It is designated MOL, and its openness is of particular value in the urban context.

**The Common was once a gorse-covered heathland and twice the size it is today.** Like the other commons, it has a history of providing for the local community: during the 19<sup>th</sup> century it was dug to provide sand and gravel, and during the World Wars for food production.

**Semi-natural value of the Common:** whilst much of it has the character of a formal public park, some areas feel semi-wild. Wandsworth Common is a Grade I Site of Borough Importance for Nature Conservation for the mosaic of habitats including woodland, acid and neutral grassland, scattered gorse scrub and wetlands.

**A distinctive, intact and well-managed townscape** with historic properties, trees and open spaces in good condition. The **relationship between the fine-grained**

**pattern of development surrounding the common and the open space of the common** gives the area a 'village green' setting, most apparent in Bellevue Road and around St Mark's Church. Its high value is reflected in much of the area being within Wandsworth Common Conservation Area, and the smaller St John's Hill Grove Conservation Area in the north.

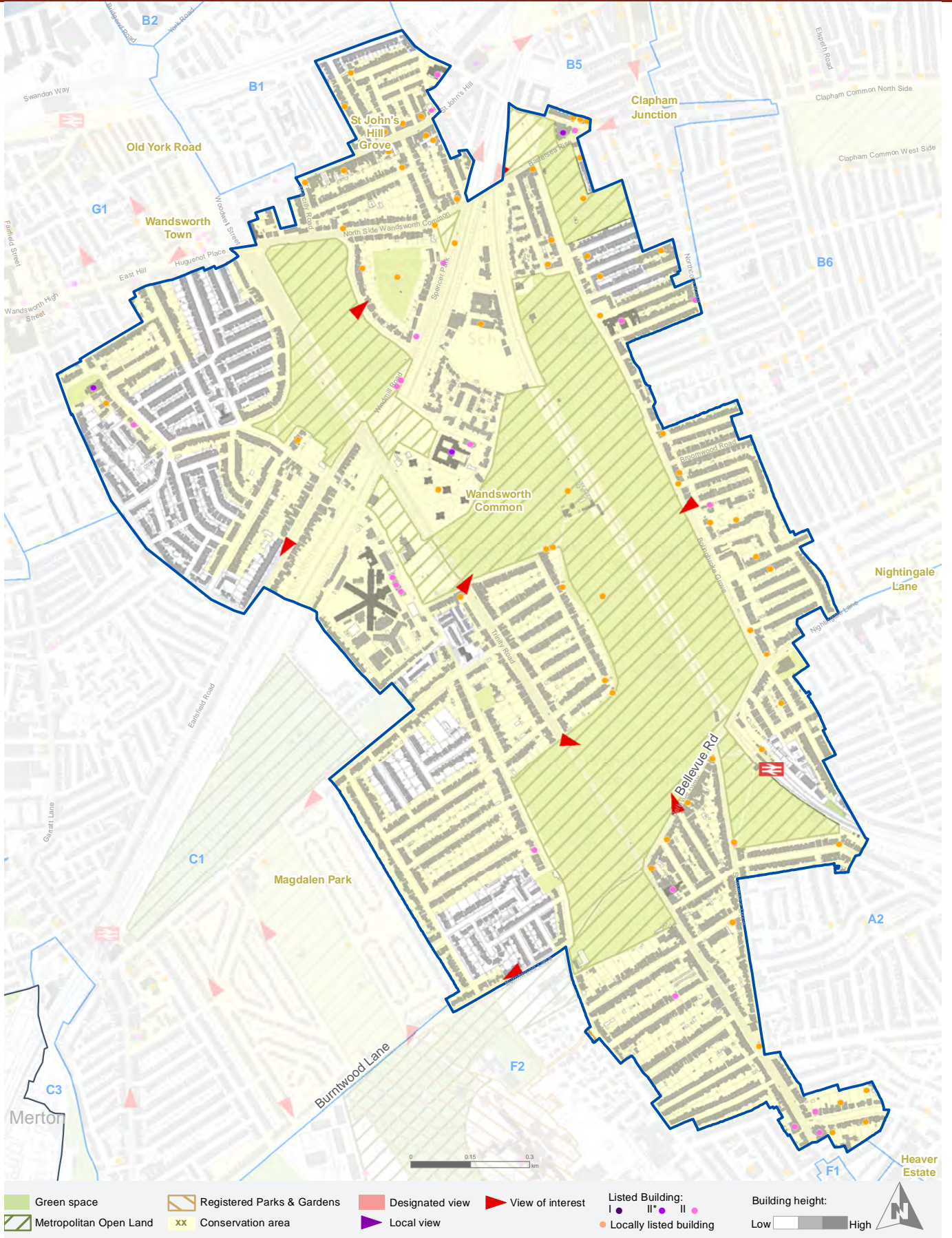
**Sense of coherence despite different architectural styles**, as a result of high quality buildings and consistent use of architecturally detailed **red and yellow London stock brick**, and occasional gault brick. Characteristic front boundaries include brick piers, low walls and railings. Surviving historic details lend character at a local scale, including original Victorian railings, granite setts, cast iron bollards and retained setts to the highway edges in St John's Hill Grove Conservation Area. Streets in regular grid patterns create a **formal character**, lined with Victorian/Edwardian 2-3 storey properties, punctuated by landmark buildings. Substantial detached villas e.g. Spencer Park, set back in large plots have a spacious and grand feel. **Taller 3-3.5 storey buildings front the wider main roads** including St John's Hill and Earlsfield Road.

**Scenic, green quality** of the Common which permeates down the surrounding streets, with trees, front gardens, and views to green skylines. Mature trees help define the boundary of the common and screening from the road. Alongside green space, they also screen and integrate the small number of taller buildings (e.g. 11 storey point blocks on Trinity Road) into the landscape.

**Local parades of shops** bring activity to the area, including Bellevue Road, Earlsfield Road and Burntwood Lane. However, there is also a **sense of tranquillity, openness and enclosure** in parts. Although there are some busy roads, they don't adversely affect the character. Landmarks include:

- Highview Primary School (grade II listed);
- St Paul's Church, St John's Hill (now residential); St Mark's (grade II\*), Holy Trinity (grade II), Trinity Road Chapel, St Mary Magdalene Church (grade II) and St Ann's Church (grade II\* listed);
- Royal Victoria Patriotic building;
- Emmanuel School;
- Wandsworth Prison (grade II listed), a dominating building although screened behind dense vegetation.
- Individual mature trees act as landmarks e.g. at the corner of Battersea Rise/ Boutflower Road.





**Fig. 206:** G2 Wandsworth Common and Residential character area plan

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## Valued features

An overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different people for a variety of reasons.

- Wandsworth Common (MOL, Conservation Area and locally listed garden), highly valued for recreation, leisure and play, as a space for gathering, tranquillity and sense of openness, particularly important in the urban London context.
- The special relationship between the Common and the houses fronting it which lend a 'village green' character.
- Landmarks, listed and individually attractive buildings, valued for aesthetic and historic quality. Architectural details on buildings including tiles, front doors, windows etc. give a rich and textured character which is particularly distinctive.
- Scenic, green quality including along streets, and green skylines. Street trees and front gardens provide biodiversity value, whilst gaps between houses allow views to greenery in back gardens. Trees in the central reservation of Trinity Road are important for minimising its impact, whilst trees around the prison are important for screening.
- Natural heritage value of the Common, supporting a mosaic of habitats and important connection with nature for local communities. Historic interest of tree avenues on the Common.
- Local parades of shops including Bellevue Road for their visual interest and vibrancy.
- Views and vistas of interest, including:
  - of St Mark's Church from Battersea Rise (Clapham Junction Town Centre character area);
  - views across the Common and across Spencer Park;
  - views into the Common from surrounding streets, including views of the Common from Bellevue Road and Trinity Road;
  - long green vistas down Earlsfield Road and Burntwood Lane.



Fig. 207: Landmark Grade II\* listed St Ann's Church



Fig. 208: Red brick terraced houses on Wandsworth Common West Side, viewed from the Common



Fig. 209: Grand terraced houses on Oberstein Road and the landmark Highview Primary School (grade II listed)

## Negative qualities

Qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.

- Front gardens which are paved over or are missing boundary walls detract from the generally coherent streetscape.
- On-street parking diminishes the quality of some streets.
- Roads and railways that cut through and fragment the Common. The presence of busy roads affects the character of the Common to a small extent but only in isolated areas.
- Unsympathetic alterations to historic buildings e.g. modern windows instead of traditional timber sash, changes to the size of original window openings, and installation of satellite dishes.
- Some poor quality Shopfronts e.g. at Bellevue Road.
- Properties with ground floor garages e.g. Westover Road contribute little to the streetscape.
- Some signs of poor maintenance e.g. Earlsfield Road and opposite Wandsworth prison.
- Unsympathetic use of materials such as timber façades.
- The modern church next to St Mark's obscures the latter from some views.

### Building types

These are the general predominant residential building types. Other uses are noted only where they are a dominant feature of the character e.g. town centres

- Period terraces;
- Semi-detached;
- Villas;
- High-rise estates;
- Shopfront terraces.

### Sensitivity

An assessment of the character area sensitivity considering its relative value and susceptibility to the types of changes likely to occur in the area.

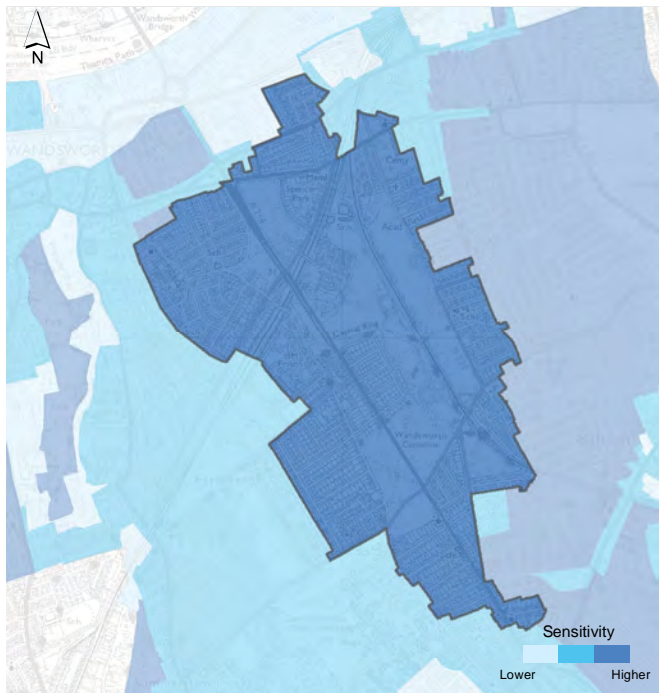


Fig. 210: G2 Wandsworth Common and Residential sensitivity plan

Overall Wandsworth Common and Residential has a high sensitivity to change, and extensive change is not appropriate.

There may, however, be small areas of lower sensitivity where the townscape is less intact and does not reflect the character described in the key characteristics.

### Strategy

Based on the current state of the townscape character, this is the broad strategy for forward planning and management.



The existing character of the area is strong, with many valued features and distinctive sense of place. Therefore the strategy is to **conserve** the character, elements and features, whilst enhancing existing features where appropriate.

### Character area design guidance

An overview of design principles to help achieve the strategy above. See Appendices A and B for guidance on tall buildings and small sites.

- Refer to the relevant conservation area appraisals and [Wandsworth Common Management and Maintenance Plan](#).
- Ensure any new development uses high quality materials. Utilise interesting façades and key elements of detail such as windows and doors.
- Respect the scale and proportions of existing buildings and streets (e.g. some taller buildings (up to 4 storeys) exist on main roads and fronting the Common);
- Preserve or reinstate historic architectural details and avoid unsympathetic alterations to buildings.
- Preserve and maintain street trees, green front gardens and boundary walls.
- Actively manage the trees on the Common to ensure replacement of trees nearing the end of their life. Selective thinning and tree removal of naturally regenerating woodland.
- Enhance biodiversity on the Common e.g. grassland management regimes, improving connectivity between habitats, tree management.
- Protect the openness of the Common and wooded skylines and vistas.
- Restore the Smock Mill on Wandsworth Common, including its setting.
- Install further cycleways and other measures to manage traffic and improve air quality.
- There are limited opportunities for buildings of 5 storeys or over, restricted to the existing area of existing towers by Wandsworth Common.



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Bus lane  
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## Section 4

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### Capacity for growth

# Section 4

## Capacity for growth

### 4.1 Introduction

This section of the report considers the capacity for growth in the borough (specifically in relation to tall buildings) using the findings of the characterisation study.

This study has been prepared to provide evidence for the Local Plan update and associated site allocations. One of the main issues the Local Plan is having to address is how many new homes are needed to meet the demands to those seeking accommodation in the borough and where they are to be built. Therefore, the assessment of land and development capacity is an important component of enabling future growth as described in the introduction (Section 1).

The capacity for growth has been established through a process of comparing sensitivity to change alongside the probability of change in line with the methodology described in [Appendix C](#).

Section [4.2](#) summarises and maps the sensitivity of the different character areas and sub-areas to change, with reference to the findings of the characterisation study.

Section [4.3](#) describes and maps the probability of change analysis, overlaid by the character areas.

Section [4.4](#) brings these two sets of analysis together to map out development capacity.

The development capacity of different areas has then been tested using a series of tall and mid-rise building development scenarios (presented in [Appendix A](#)). This analysis has informed opportunity maps for tall and mid-rise buildings, described in more detail below.

#### Tall buildings for Wandsworth

Part A of Policy D9 of the London Plan, 2021 states:

*"Based on local context, Development Plans should define what is considered a tall building for specific localities, the height of which will vary between and within different parts of London but should not be less than 6 storeys or 18 metres measured from ground to the floor level of the uppermost storey."*

In line with the London Plan, the borough of Wandsworth has developed a local definition of a tall building to be applied across the borough. A tall building is defined as:

**"Buildings which are 7 storeys or over, or 21m or more from the ground level to the top of the building, whichever is lower".**

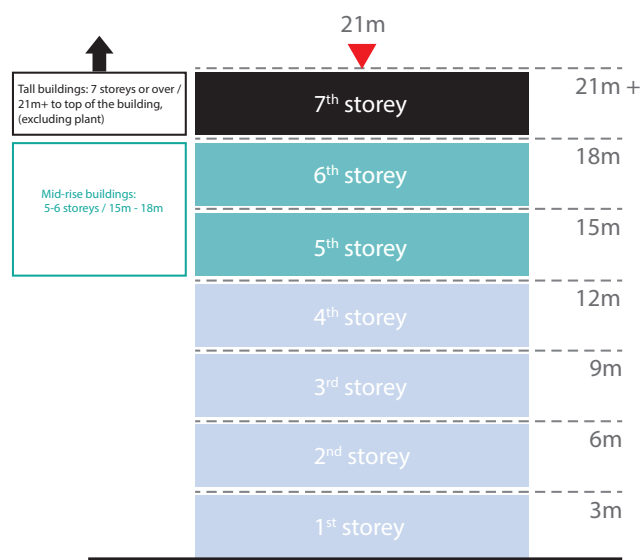


Fig. 211: Definition of a tall and mid-rise building for Wandsworth

This definition has been developed in response to the extensive parts of the borough which have a small-scale character which are sensitive to any increases in height. Therefore the minimum definition of a tall building in accordance with the London Plan has been applied to the borough. Providing a policy which had different definitions of "tall" and different appropriate heights in different parts of the borough was considered to risk introducing confusion to the implementation of the policy. With the adopted definition the study is able to provide less ambiguity and more guidance for developments which would be tall within their local context but not within a tall building zone. This is crucial for protecting the character of Wandsworth. Buildings will be considered tall where they meet the definitions set out in Part A of policy LP4 of the Local Plan and Part A of Policy D9 of the London Plan. However, where the height of a building measured from ground to the floor level of the uppermost storey does

not trigger the London Plan definition, but the height of the uppermost storey exceeds the average storey height of the remaining storeys and the total height of the building exceeds the definition set out in Part A of Local Plan policy LP4, the building will be considered tall.

Further explanation on the definition is as follows:

- Ground level is the surface of the ground immediately adjacent to the building in question and would not include any addition laid on top of the ground such as decking. Where ground levels are not uniform (for example if the ground is sloping), then the ground level is the highest part of the surface of the ground next to the building.
- The height restriction does not include for typical structures and plant found at roof level including railings, plant equipment, maintenance cranes or elevator shafts. However, the placement, scale and design of these must follow good practice in addition to the design principles in [Appendix A](#).
- It is recognised that some developments will have differing floor heights, particularly at the ground floor. The height restriction applies to whichever is the lower between the number of the storeys and the metres above ground level.
- Heights are not expressed above ordnance datum (AOD) as the ground level of the tall and mid-rise building zone differs across each individual zone. Therefore the appropriate heights defined would vary if expressed in AOD for every individual location. Heights in AOD have been considered in the development of the appropriate building heights (for example, prominent plots on the tops of hills may have a lower height restriction than those at a lower AOD, depending on the site specific context).

Where this definition is applied to a building being assessed as part of a planning application, consideration should be given to the probability, sensitivity and development capacity plans and the criteria accompanying the tall building zones plans within this document, and importantly, the building's design quality.

It should be noted that this study has been prepared in the context of a new London Plan (2021) and the new emerging Local Plan for Wandsworth. The study follows a design-led approach to identifying tall buildings and is not intended to provide evidence to support buildings which have gone through previous planning processes.

Refer to Section [4.5](#) for tall building zones and further details on appropriate building heights, in principle, for these areas.

### **Mid-rise buildings for Wandsworth**

Separate from the London Plan definition of tall, the borough of Wandsworth recognises that there may be mid-rise buildings in the borough which, whilst not defined as 'tall' in line with the London Plan, are of an increased height to their surroundings. Such buildings have the potential to result in significant impacts to the roofscape, skyline, or townscape character.

Therefore, such buildings have been described as 'mid-rise' buildings within this study. A mid-rise building is defined as:

**"Buildings of 5-6 storeys or 15-18m from the ground level to the top of the building, whichever is lower."**

This definition is based on the prevailing building heights in the borough, determined through the site survey. Many areas in Wandsworth borough contain 2-3 storey buildings. In such cases, a new development of 5-7 storeys would be of an increased height to its surroundings, although would not be classified as a 'tall building'. The borough therefore classifies such buildings as 'mid-rise'. Mid-rise buildings zones are not required for the London Plan. However, this study has defined these in the same way as the tall building zones, in order to direct appropriate growth in the right locations in the borough.

Refer to Section [4.6](#) for plans of the mid-rise building zones.

### **Small site opportunities**

Opportunities for development of small sites has also been assessed, presented in Section [4.7](#).

## 4.2 Sensitivity

[Fig. 212](#) opposite illustrates the sensitivity of different parts of the borough.

The sensitivity of each character area (or parts thereof) has been assessed through the characterisation process reported in [Section 3](#), using the method described in [Appendix C](#) section [C.5](#). The sensitivity is assessed in relation to each character area's (or sub-area's) relative sensitivity to tall and mid-rise buildings.

Some of the most sensitive parts of the borough (shown in the darker shade of blue) include its large open spaces, such as (from west to east):

- Wimbledon Common and Putney Heath;
- Wimbledon Park;
- Wandsworth Park;
- King George's Park;
- Wandsworth Common;
- Battersea Park;
- Clapham Common; and
- Tooting Common.

Other notably high sensitivity areas include (from west to east):

- conservation areas across much of West Putney Residential with extensive intact period properties;
- Putney Riverside area with strong cultural associations with river uses and an interesting historic fabric;
- Southfields Residential area with its highly consistent period terraces;
- period residential properties around Wandsworth Common, with the Wandsworth Common Conservation Area;
- residential streets around much of the Battersea and Balham Residential character areas; and
- Furzedown Residential with its rich and consistent streetscape.

Areas with a lower sensitivity to change (shown in the paler shade of blue) include (from west to east):

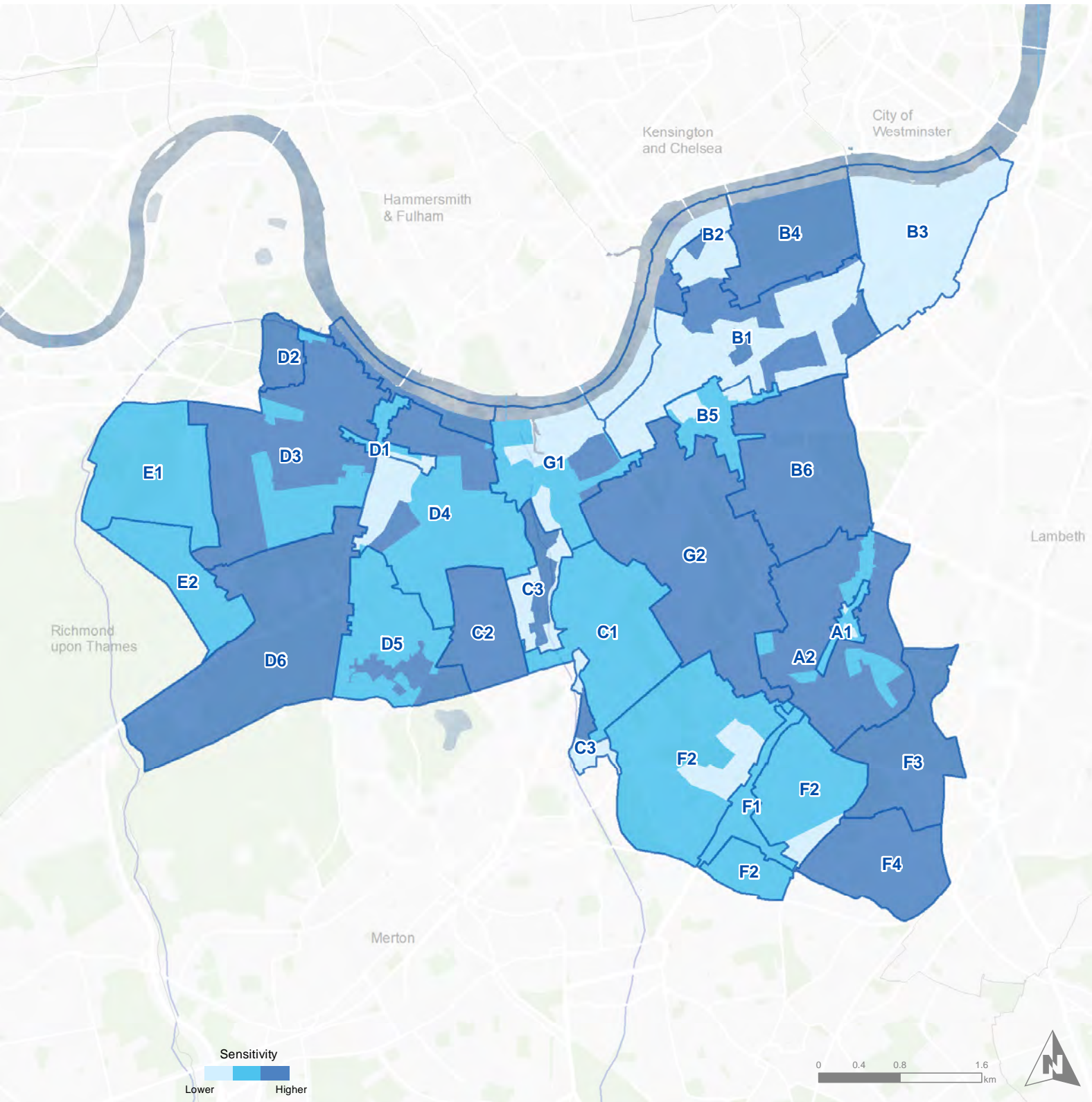
- estates within East Putney Residential;
- a section of Upper Richmond Road within Putney Town Centre;
- parts of Wandsworth Riverside and the Wandle Valley;
- industrial areas along King George's Park and further to the south along the River Wandle and borough boundary;

- estates in the Battersea Residential character area;
- areas around St George's Hospital near Tooting;
- modern estates around Church Lane in Tooting Residential;
- the supermarkets and car parking within Balham Town Centre; and
- the Nine Elms Opportunity Area.

It should be noted that the sensitivity assessment has been undertaken at a borough-wide scale and is therefore necessarily broad-brush in its application. Within each of the areas identified there may be specific sites with a higher or lower sensitivity than illustrated. Additionally, sensitivity has been assessed to a generic principle of a building that is approximately 50% higher than the existing average building height. Specific sites would need to consider sensitivity to specific development types including their land use and design quality.

There may be smaller scale highly sensitive areas (including for example locally listed parks and gardens) which are not illustrated in the borough-wide map but would be sensitive to change. These are generally noted in the valued features descriptions in the character area profiles.





**Fig.212: Sensitivity plan**  
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### 4.3 Probability of change

Across the borough, some sites and locations are more likely to come forward for development or redevelopment than others.

The findings of the probability of change assessment are shown in [Fig.213](#). The methodology for undertaking this assessment - including the definition of low, medium and high probability - is set out in [Appendix C](#) section [C.6](#).

A borough-wide assessment considers the likelihood of areas coming forward for development. Factors which give rise to a **higher probability of change** include:

- areas which are already designated for development (through an existing allocation or opportunity area status), including areas with likely forthcoming masterplans and major planning applications at:
  - part of Balham town centre;
  - the Clapham Junction and the York Road/Winstanley Regeneration area;
  - Nine Elms (including Battersea Design Quarter);
  - parts of Putney town centre;
  - parts of Roehampton, focused around the Alton Estate;
  - parts of Tooting Town Centre;
  - parts of Wandsworth Town, riverside and the Wandle Valley.
- areas which have a high Public Transport Accessibility Level (PTAL) score, or are in proximity to a town centre or station, including:
  - areas along the suburban National Rail line including Putney Town Centre, Wandsworth Town and Queenstown Road
  - areas around East Putney and Southfields London Underground District line;
  - areas around National Rail mainline stations including Earlsfield and the major interchange at Clapham Junction;
  - areas around other suburban National Rail lines, including stations at Battersea Park, Wandsworth Common, Balham and Tooting;
  - areas along the London Underground Northern Line including Tooting Broadway, Tooting Bec, Balham, Clapham South;
  - the Nine Elms and Battersea areas where close to the Northern Line extension; and
  - areas around Wandsworth Road London Overground station.

Generally, this covers much of the northern and south eastern extents of the borough.

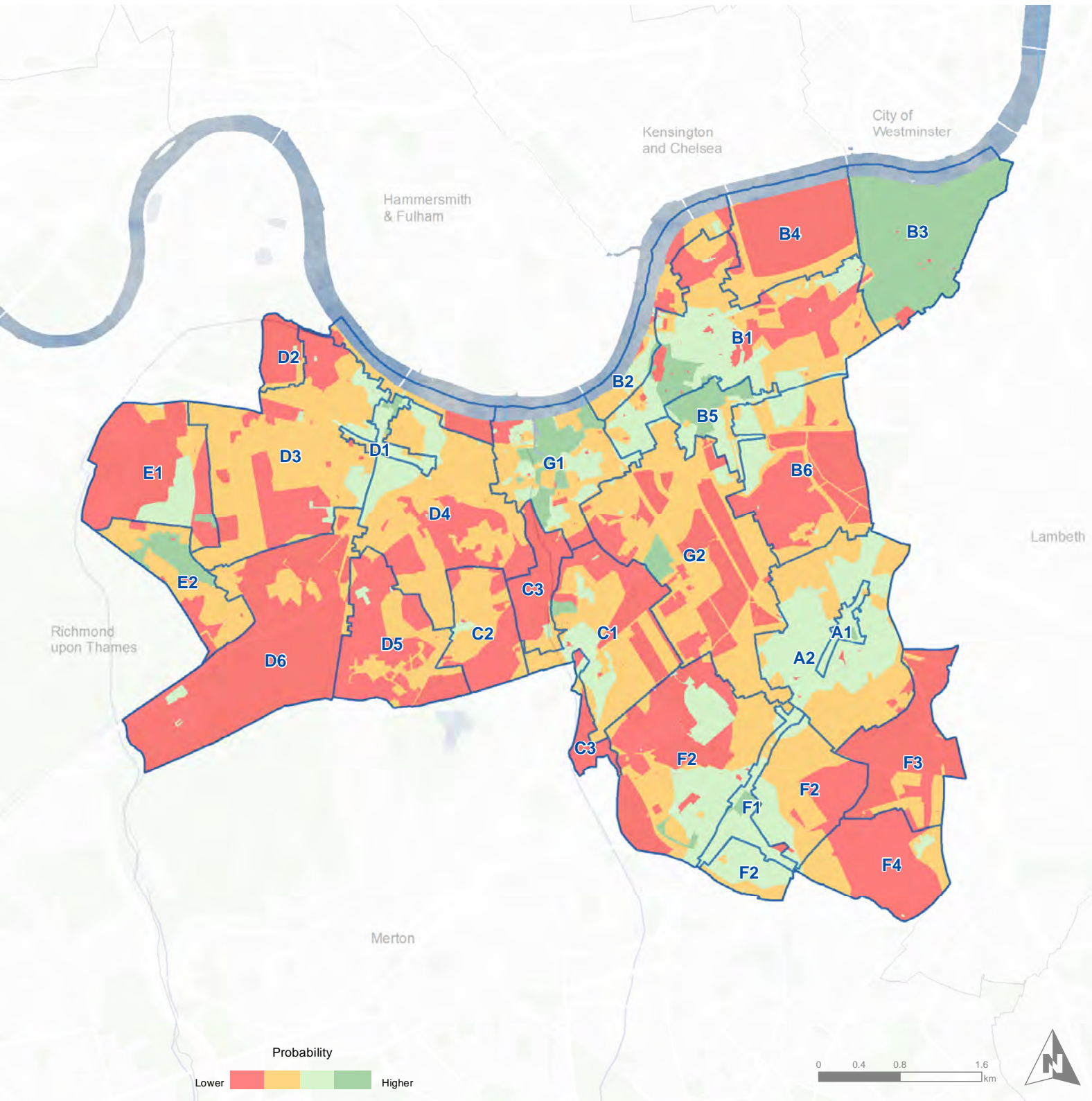
This is not to suggest that all sites within this areas are acceptable for development; rather, that as a whole the likelihood of change is higher.

Factors which give rise to a **lower probability of change** include:

- Areas which are designated in their existing use (such as Strategic Industrial Location), including:
  - strategic industrial units within Battersea and Nine Elms; and
  - industrial units along the Wandle Valley.
- areas of open space, such as:
  - Wimbledon Common and Putney Heath;
  - Wimbledon Park;
  - Wandsworth Park;
  - King George's Park;
  - Streatham Cemetery;
  - Wandsworth Common;
  - Battersea Park;
  - Clapham Common; and
  - Tooting Common.
- areas with a low PTAL score, including:
  - Roehampton;
  - Putney Heath;
  - areas to the west of Wandsworth Common; and
  - Furzedown Residential.

Again, this is not to suggest that no change will occur in these areas; however, as a whole the likelihood of change is lower.

On a site-by-site basis, there will be a number of factors that influence probability of change, including: existing use; quality, fitness-for-purpose and vacancy; site size and configuration; and ownership.



**Fig. 213:** Probability of change plan

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## 4.4 Development capacity

The development capacity of different parts of the borough to tall buildings has been assessed by combining the analysis of sensitivity and probability of change.

The matrix in [Table 1](#) illustrates how sensitivity and probability of change are combined to indicate development capacity. These are mapped in [Fig. 214](#).

		Probability			
		Very high	High	Medium	Low
Sensitivity	High				
	Medium				
	Low				

**Table 1** Development capacity matrix

The red areas represent the parts of the borough with a high sensitivity to tall buildings alongside a low probability of change (e.g. existing large public green spaces). Areas with the least likely development capacity include:

- Wimbledon Common and Putney Heath;
- the westernmost parts of West Putney Residential;
- Barnes Common;
- Wimbledon, Wandsworth and King George's parks;
- the open spaces of Wandsworth Common;
- Battersea Park; and
- Clapham and Tooting Commons.

Orange areas have either a lower sensitivity but still a low probability of change (e.g. development areas with a low PTAL score), or a high sensitivity but only a medium probability of change.

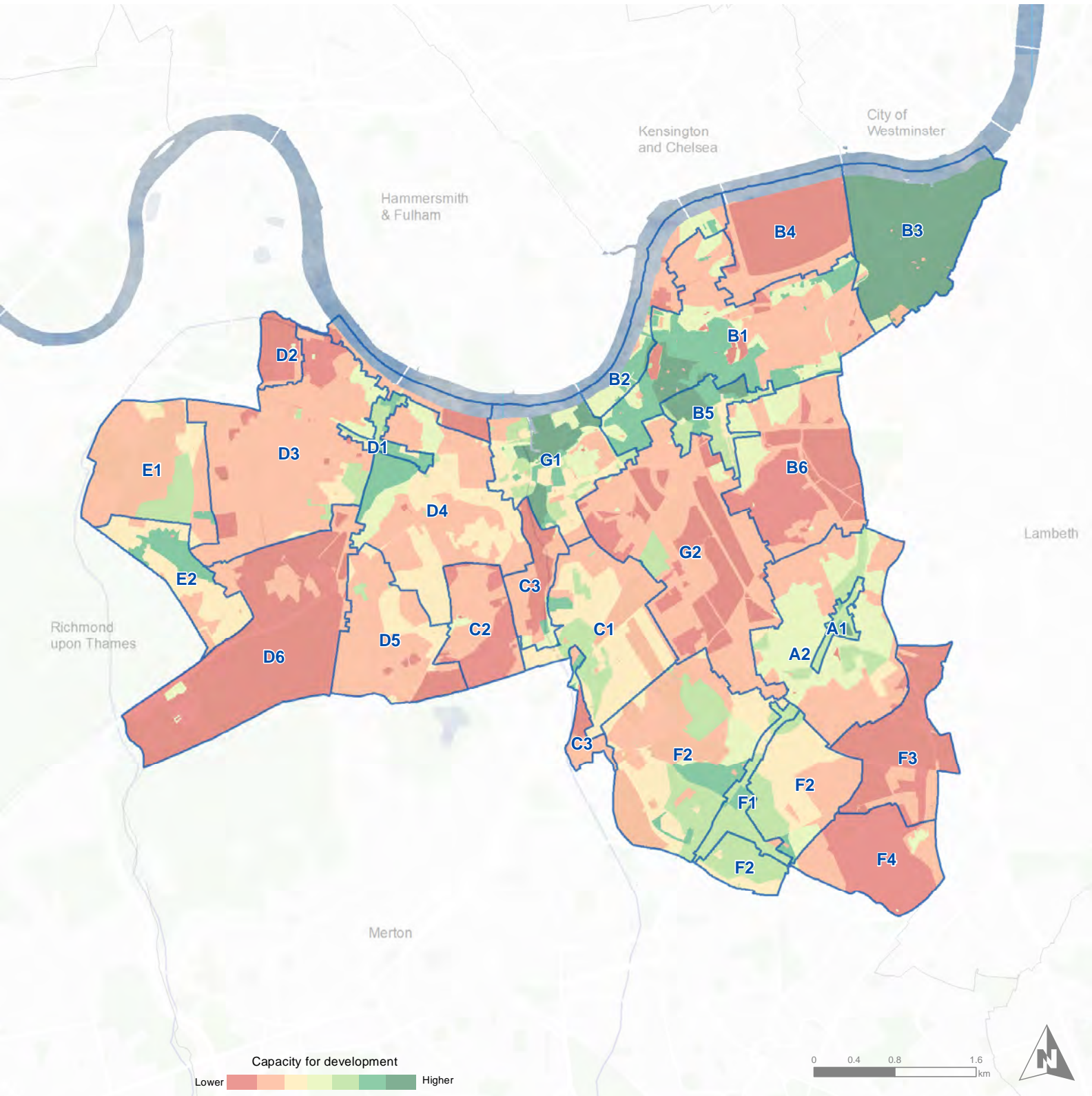
Green and pale yellow areas generally have a high probability of change or a lower sensitivity to change. Dark green areas are the most likely to have the greatest development capacity, with a high probability of change compared against a low sensitivity, including areas such as:

- estates in East Putney Residential;
- Wandsworth Town Centre including the Wandle Delta;

- some industrial plots within the Wandle Valley;
- estates in Battersea Residential;
- parts of Clapham Junction Town Centre;
- some parts of Tooting Residential closest to the town centre and estates around Church Lane;
- the supermarket and car parking plots in Balham Town Centre; and
- Nine Elms Opportunity Area.

The following pages considers this analysis alongside areas of existing and consented tall buildings across the borough. This then identifies the overall strategy for tall and mid-rise buildings which in turn defines the broad areas of search across the borough (see [Fig. 218](#)).

The individual areas are identified in the Tall Building and Mid-Rise Building Opportunity Maps set out in Sections [4.5](#) and [4.6](#). The tall building zones have been further assessed in [Appendix A](#), which includes presentation of a number of development scenarios designed to test the capacity for increased height.



**Fig. 214: Development capacity plan**  
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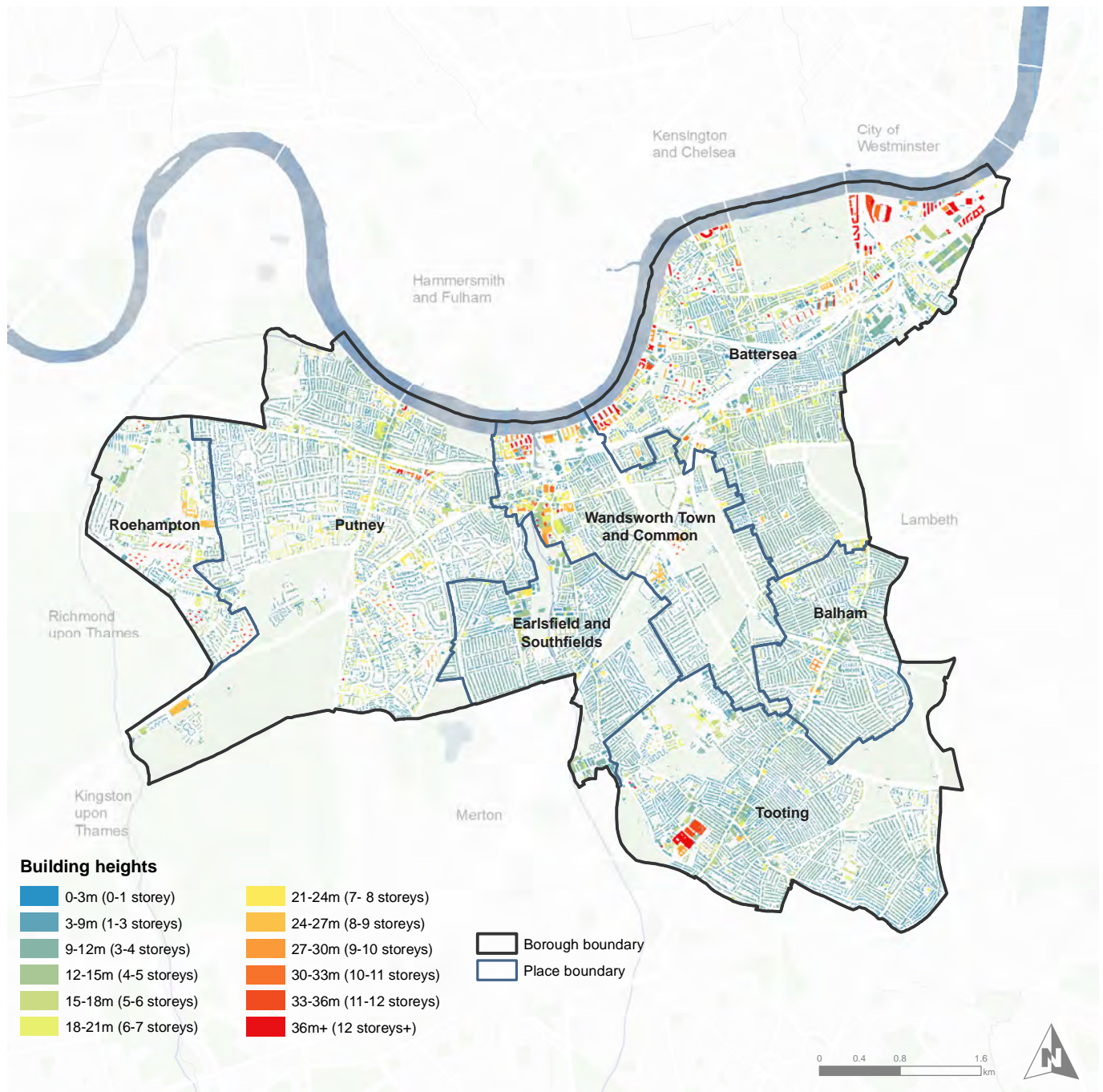
#### 4.4.1 Existing building heights

Development capacity has been considered alongside existing building heights across the borough. [Fig. 215](#) below shows existing building heights in the borough based on available data. The adjacent [Fig. 216](#) shows just those buildings which fall within the Wandsworth definition of tall (i.e. 7 storeys or 21m or higher).

Note that the number of storeys shown is given as an estimate based on available building height data and is not a record of an actual storey count. In some cases new development may not be shown and when one part

of a building is at a certain height this may sometimes extend across the whole building due to the high level nature of the data used. These inconsistencies have not influenced the identification of tall building zones and appropriate height ranges. There is a clear distribution of existing tall buildings within key areas such as:

- Nine Elms Opportunity Area;
- the Thames riverside through Wandsworth and Battersea; and
- the town centres of Putney, Wandsworth, Clapham Junction.



**Fig. 215:** Existing building heights across the borough

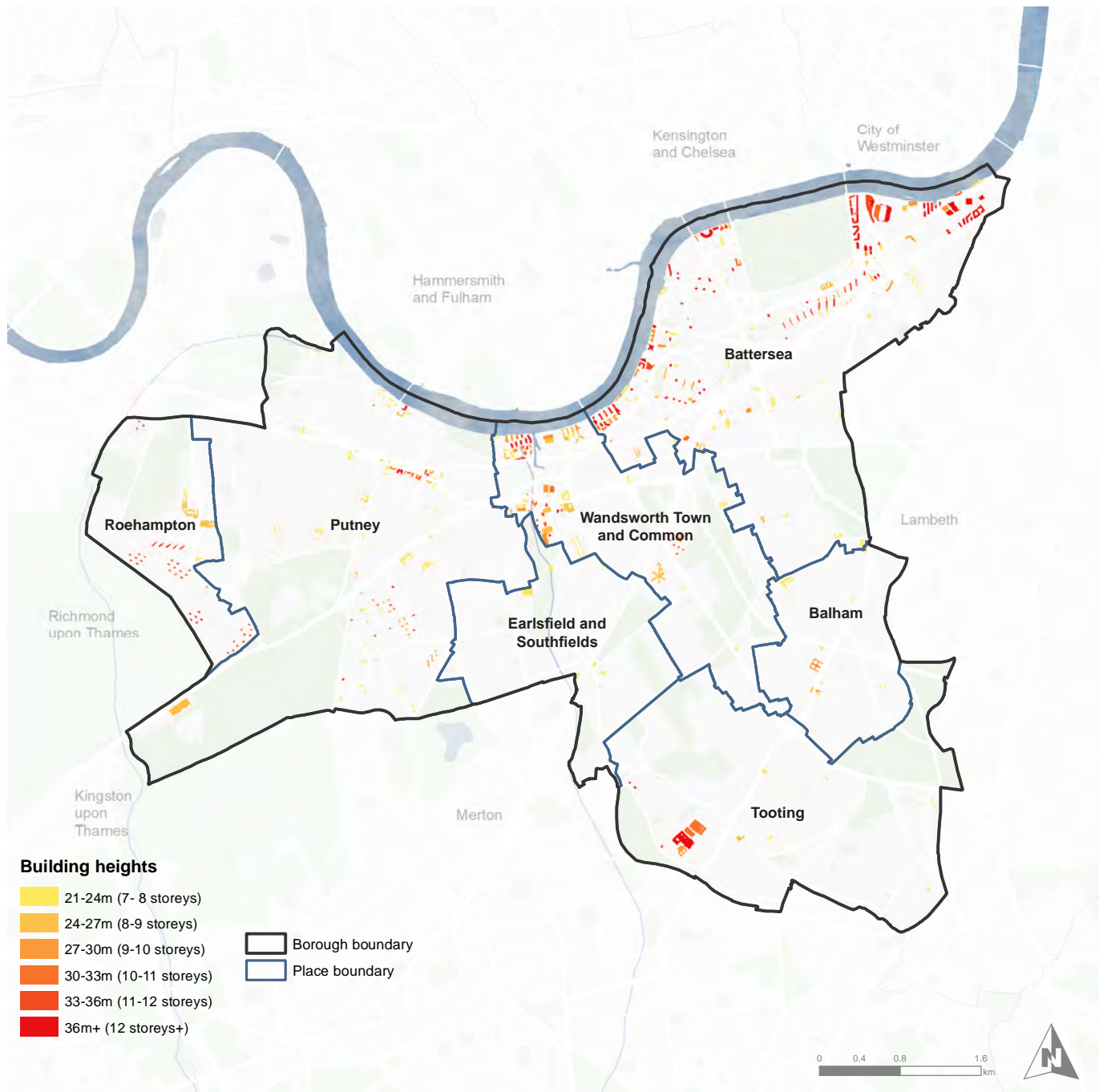
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There are also tall buildings within a number of estates across the borough, including across parts of :

- Battersea;
- West Putney;
- Roehampton;
- Wimbledon Parkside; and
- adjacent to Wandsworth Common.

Outside of these areas, tall buildings are intermittent and not typical of the overall character of the areas. This includes individual large buildings such as St

George's Hospital in Tooting, Wandsworth prison close to Wandsworth Common and the retail superstore adjacent to Putney Heath. Note that the map does not necessarily capture tall buildings which are under construction or recently completed at the time of writing, in particular with reference to the Nine Elms Opportunity Area. The plan also does not show where newly consented tall buildings are expected to be built in coming years, although these are generally aligned with the existing areas of tall buildings. However these inconsistencies have not influenced the identification of tall building zones and appropriate height ranges.



**Fig.216:** Location of existing tall buildings across the borough

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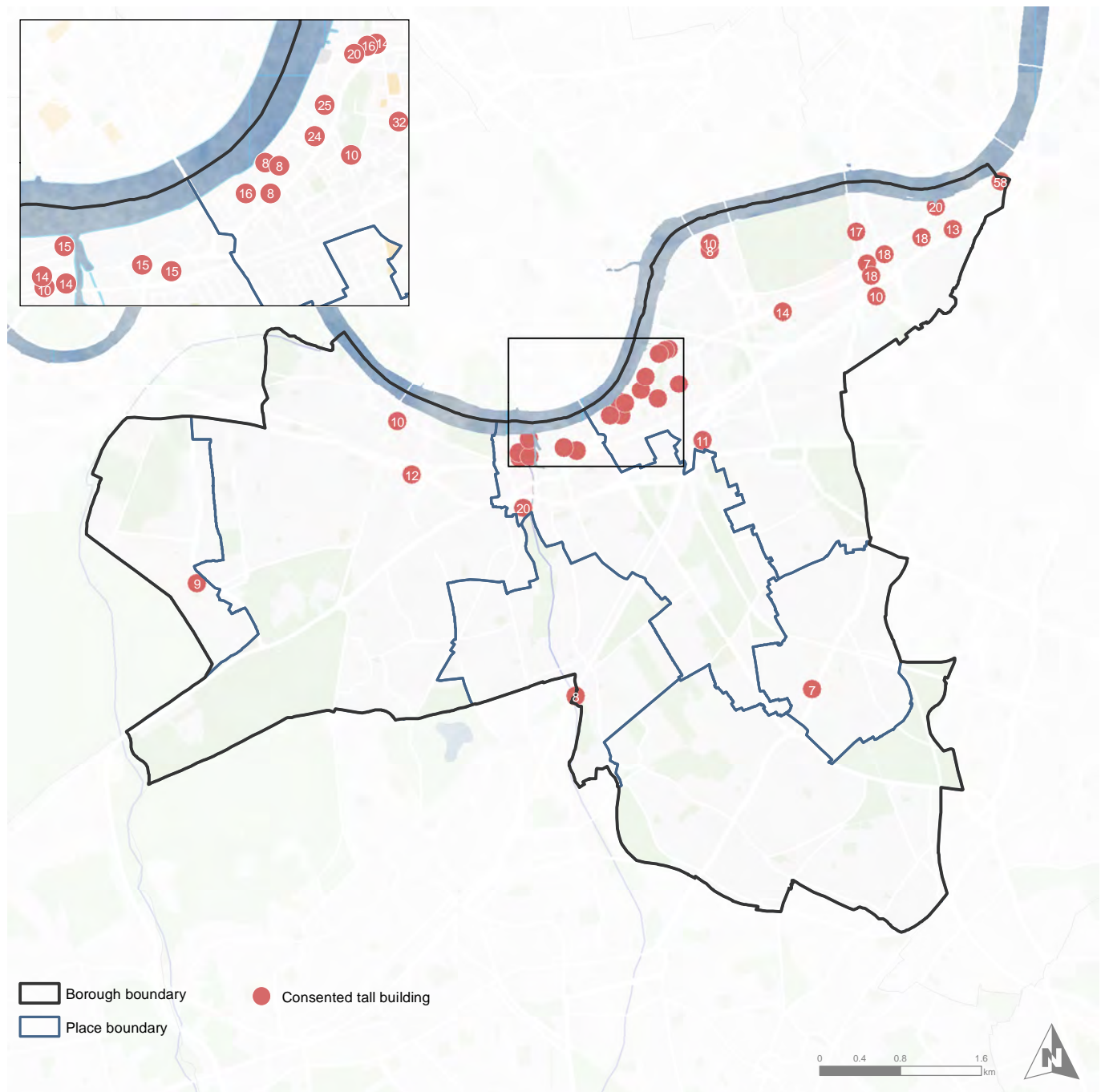
#### 4.4.2 Consented tall buildings

Parts of Wandsworth are undergoing rapid change and development. Therefore, the map of existing building heights does not give the full picture of the recent character of the borough. [Fig. 217](#) below shows the location and height of recently consented tall buildings across the borough, shown in the context of existing buildings over 7 storeys or 21m.

Note that the number of storeys shown is approximate based on available information about the consented schemes, and assumes a 3m storey height. The plan

indicates that the majority of consented schemes lie within the tall building locations identified in the adopted Plan, including:

- Nine Elms Opportunity Area;
- the Thames riverside through Wandsworth and Battersea;
- some wider parts of Battersea, north of the railway;
- the town centres of Putney, Wandsworth, Clapham Junction; and
- some opportunities along the Wandle Valley through Earlsfield.



**Fig. 217:** Distribution of recently approved new mid-rise and tall buildings across the borough

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### 4.4.3 Overall development strategy

The development capacity, existing tall buildings and consented tall buildings mapping helps to inform an overall strategy for development of increased height across the borough.

Fig.218 below identifies the broad areas considered in more detail in the following sections on tall and mid-rise buildings. Much of LB Wandsworth is characterised by highly sensitive areas with a consistently low height residential scale. Therefore these are scoped out of further consideration, with the

strategy for tall buildings focusing on the Nine Elms Opportunity Area, the five town centres (Balham, Clapham Junction, Putney, Tooting and Wandsworth), the River Thames frontage through Wandsworth and Battersea and known areas with emerging masterplans or major planning applications.

The study further reviews the potential for increases in height through mid-rise buildings along strategic road corridors including some existing areas with a less consistent built pattern or higher existing building heights which typically extend from these routes.

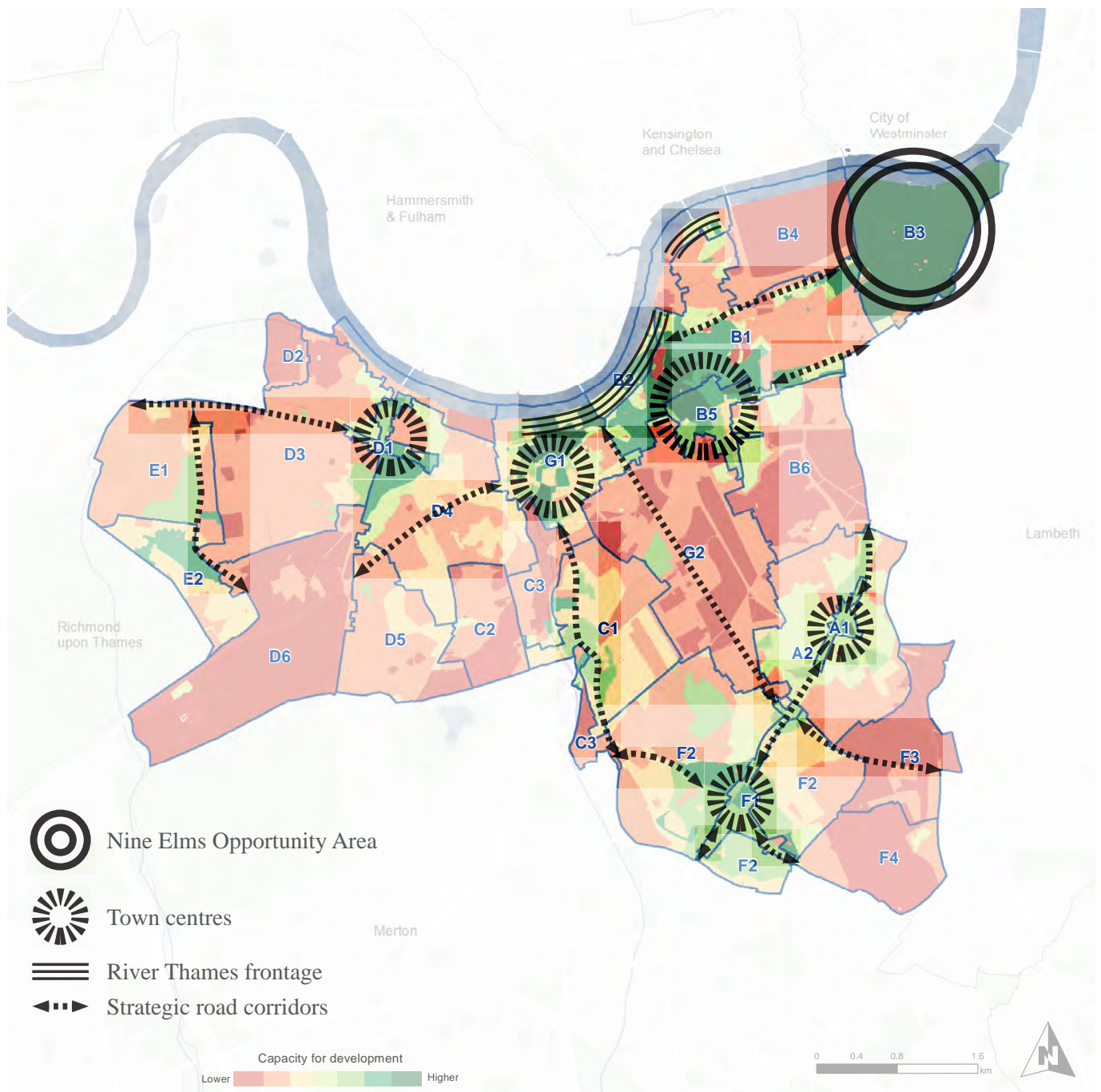
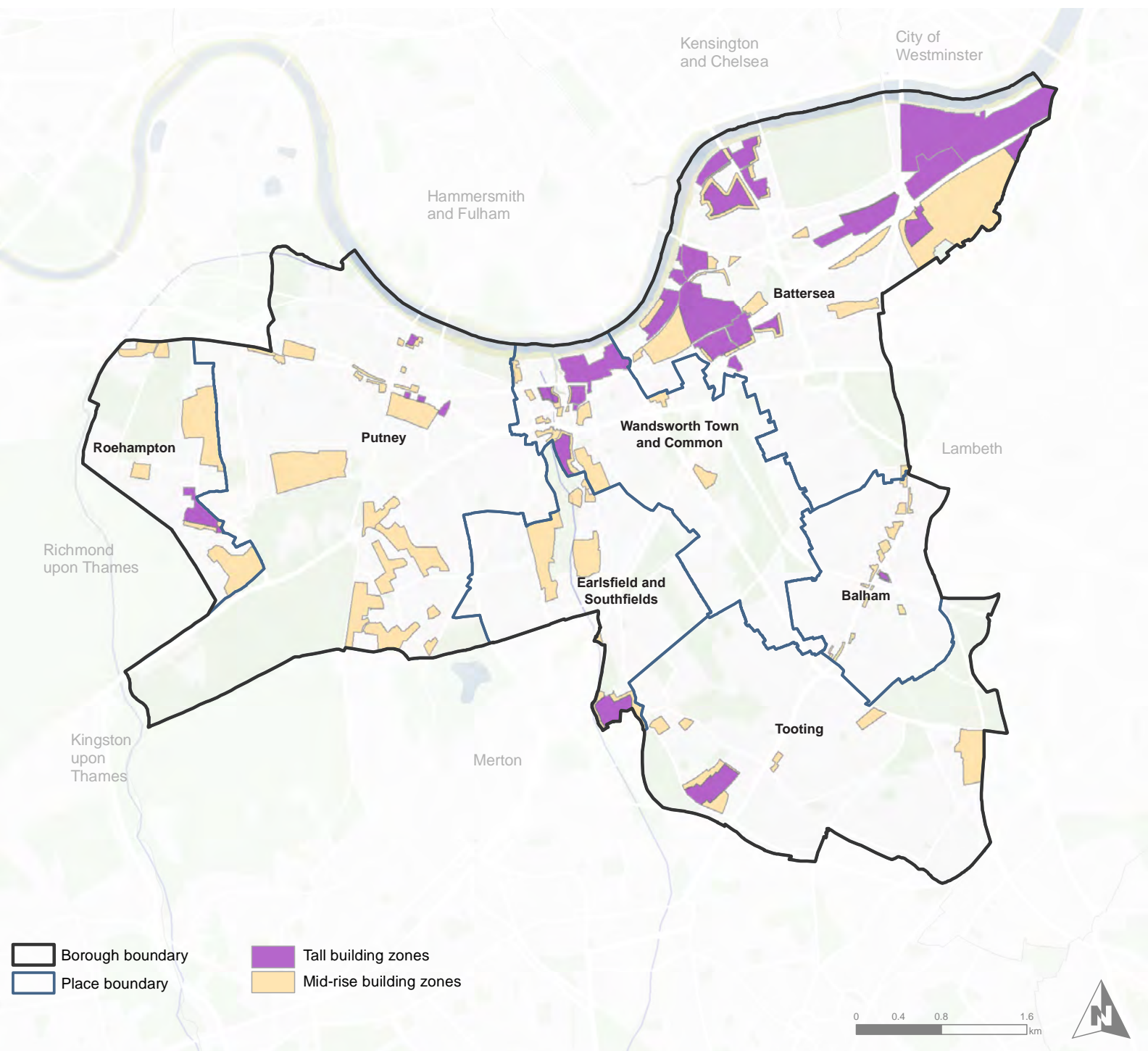


Fig.218: Strategy for mid-rise and tall building development across the borough  
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The following pages set out each of the identified tall and mid-rise building zones, mapped for each of the seven Places.

For each of the zones an appropriate height range is identified. For the tall building zones a further summary description is provided, and further evidence supporting the locations and their appropriate heights is provided in [Appendix A](#).

An overview of the tall and mid-rise building zones together is provided in [Fig. 219](#) below.



**Fig. 219:** Tall and mid-rise building zones borough overview map  
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## 4.5 Tall buildings

### 4.5.1 Tall building policy background

Part B of Policy D9 of the London Plan, 2021 states:

*"1) Boroughs should determine if there are locations where tall buildings may be an appropriate form of development, subject to meeting the other requirements of the Plan...*

*2) Any such locations and appropriate tall building heights should be identified on maps in Development Plans.*

*3) Tall buildings should only be developed in locations that are identified as suitable in Development Plans."*

[Fig. 220](#) on the following page presents an overview map of zones with potential to accommodate tall buildings, in line with the London Plan. Each zone is supported by a description of the appropriate tall building height range for that zone.

Evidence and information to support the conclusions is contained in [Appendix A](#). The tall building zones have been defined through an analysis of whether they would impact the townscape, local views and nearby heritage assets positively, negatively or neutrally. This assessment has been undertaken using three core types of information depending on the specific zone:

- analysis of existing tall buildings;
- analysis of consented tall buildings or area masterplans; or
- analysis of scenarios prepared specifically for this study.

### 4.5.2 Borough-wide findings for tall buildings

Overall, Wandsworth has capacity for tall buildings in a number of strategic and more local locations. An overview of these locations is shown on [Fig. 220](#). Opportunities for tall buildings are generally concentrated within three different types of area:

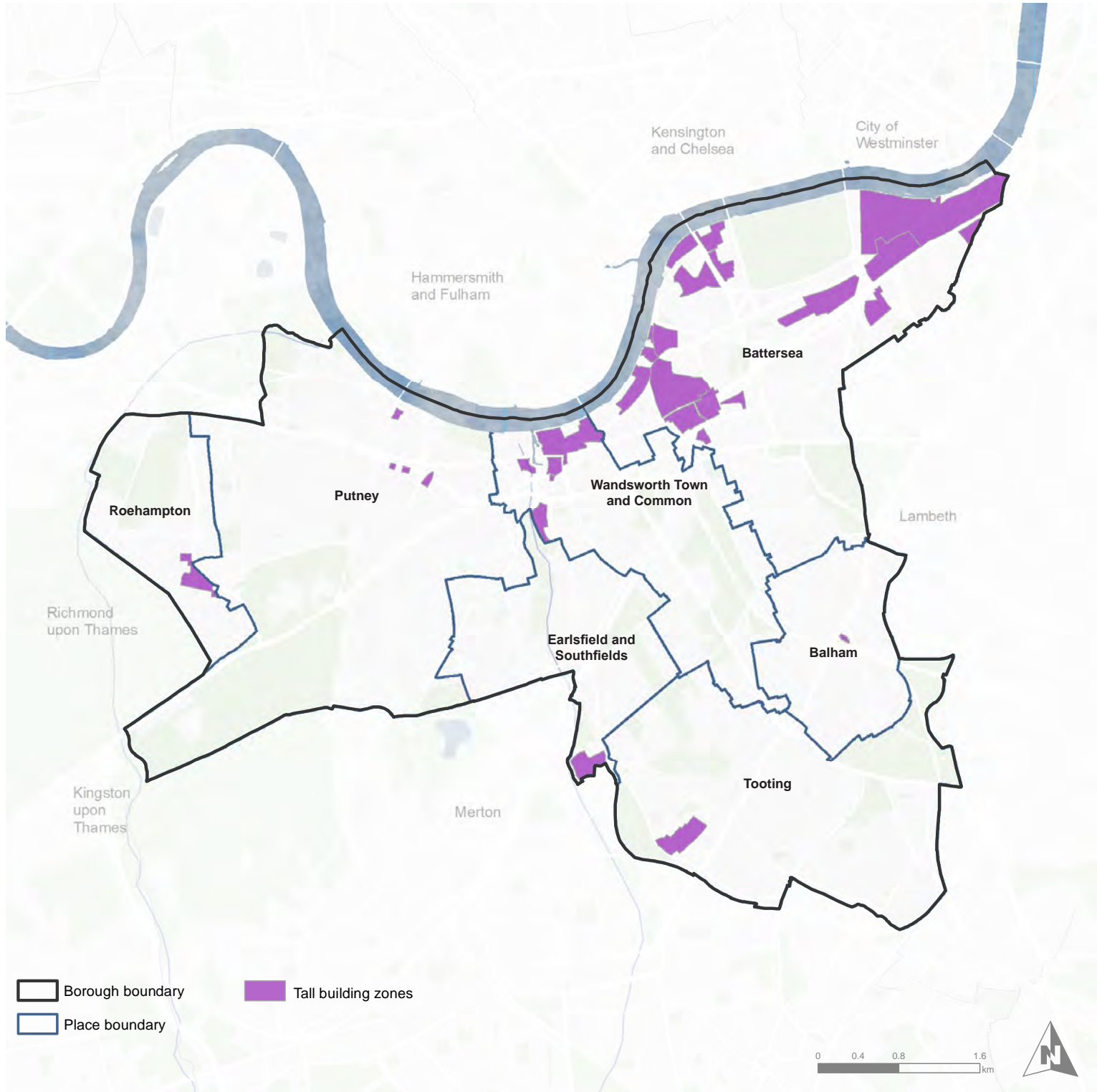
6. Along the **River Thames frontage** including within the **Nine Elms Opportunity Area**: Here, until recently, commercial and industrial uses have dominated. There are opportunities for tall buildings to respond to the large scale and width of the riverside. However, the impact of riverside development goes well beyond the borough boundaries and therefore must continue to be carefully planned to protect the character of both banks and the overall historic and cultural importance of the River Thames as a globally

recognised characteristic of London. Nine Elms is located within London's Central Activities Zone and is the site of significant tall building development in recent years. In particular any proposals need to assess and avoid impacts on the Palace of Westminster and Westminster Abbey including St Margaret's Church World Heritage Site in the nearby City of Westminster on the northern bank of the Thames. Tall building applications in the Wandsworth riverside area would need to have particular regard to historically sensitive sites on the north bank such as Fulham Palace Scheduled Monument. A policy on character and design in relation to the River Thames and the Thames Path (or a sub-policy contained within the general policy) would be beneficial considering its importance shown through some of the character area assessments.

7. Within **town centres**: There are opportunities for tall buildings within the town centres of Putney (including along Upper Richmond Road), Wandsworth, Clapham Junction and, to a lesser extent, Balham.
8. Within or adjacent to **existing estates and emerging major regeneration masterplans**: This includes opportunities within Roehampton, Battersea, the southern end of the River Wandle (near the Wimbledon AFC development in neighbouring LB Merton) and at St George's Hospital in Tooting.

In all cases, the acceptability of individual plots will depend on specific characteristics of the site. The tall building guidance provided in [Appendix A](#) is intended to help steer selection of appropriate sites and development of suitable building proposals, including in the context of consideration of the cumulative impact of tall buildings.

An area being designated as a tall building zone does not mean it has capacity to receive tall buildings within the appropriate range across the whole extent. Every new development will need to consider the specific context of the plot, existing buildings surrounding the plot and any other development proposals in the area including those going through planning, consented schemes and buildings under construction.



**Fig. 220:** Tall buildings zones map

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### 4.5.3 General tall building zone criteria

There are a number of criteria that apply generally to all of the tall building zones. Tall buildings shall:

- be carefully located and designed to step down to surrounding existing and proposed buildings (including within mid-rise building zones);
- respond positively and protect the setting of existing buildings in the surrounding area, including heritage assets such as conservation areas, listed buildings and locally listed buildings;
- respect the scale, width and proportion of adjacent streets and watercourses, and local character, as outlined in the character area profiles in [Section 3](#), including potential effects on key characteristics, valued features and sensitivities;
- respond to the character area design guidance provided in [Section 3](#) and the tall building design guidance in [Appendix A.9](#);
- deliver a varied and interesting roofline in response to surrounding architectural styles, avoiding long monotonous blocks of development;
- development within town centres should include active uses and frontages at ground level;
- incorporate an appropriate range of building heights and open spaces.

### 4.5.4 Character area summaries and criteria

The following pages provide the appropriate heights and a description of the tall building zones within each Place. This section should be read alongside the character area profiles provided in [Section 3](#).

The descriptions are based on the site observations of the team undertaking the study and are not intended to be exhaustive or detailed. All development proposals should make reference to the full suite of planning policy documentation available on the Council's website in addition to London Plan and national policy, a summary of which is provided in [Appendix D](#). All development proposals will need to show further detailed analysis at a specific site scale and should have sensitive consideration of the building's massing, form, style and materials.

Supporting evidence which underpins each tall building zone is provided in [Appendix A](#).

# A Balham

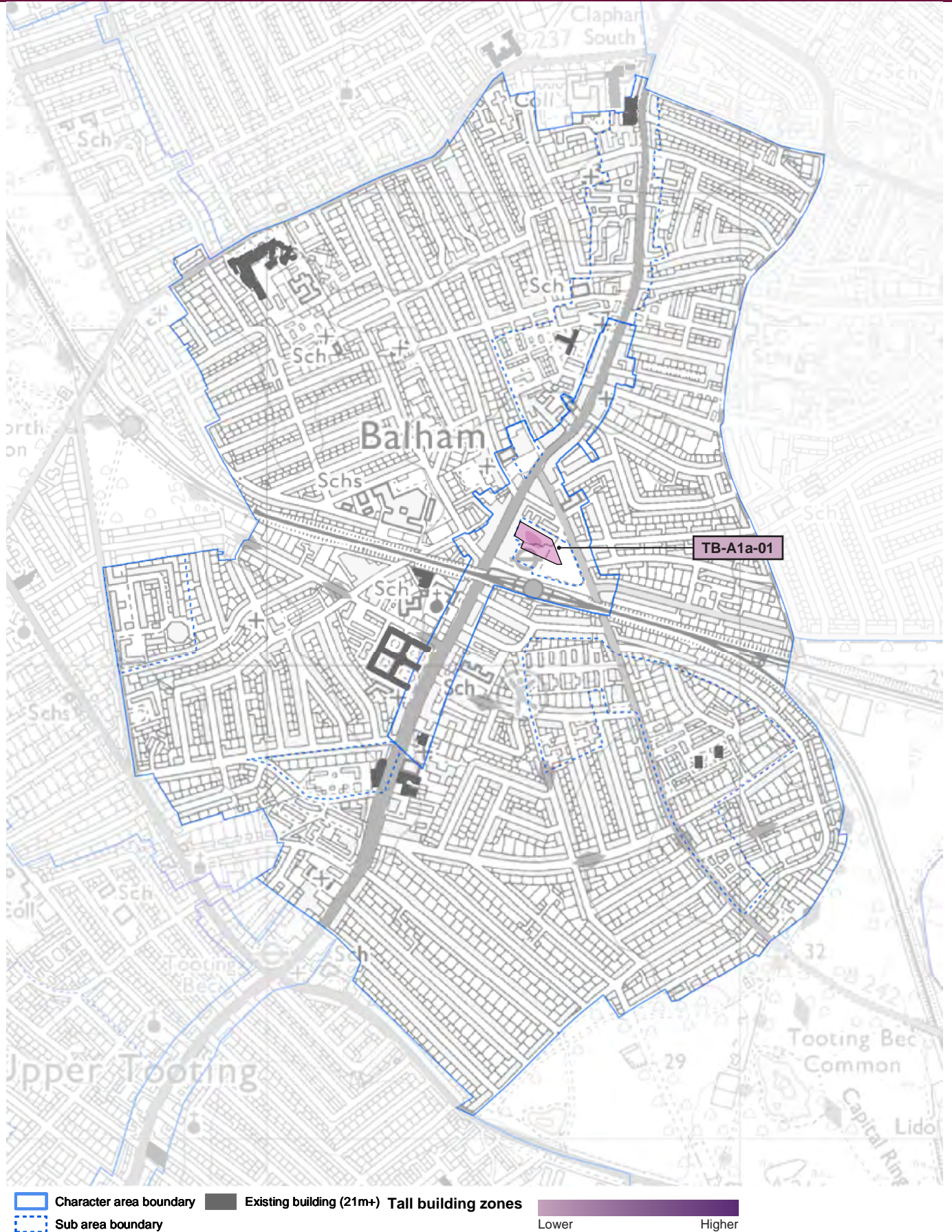


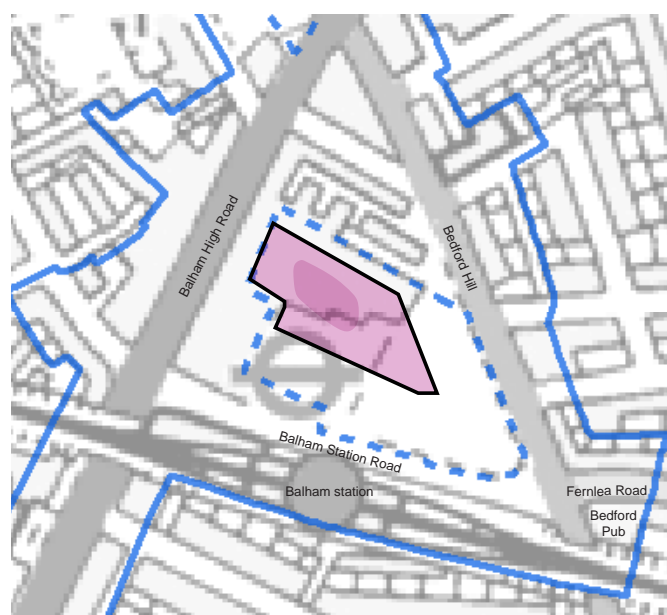
Fig. 221: Balham Tall Buildings Zone map

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Balham has a modest scale town centre amid extensive streets of 2, 3 and 4 storey Victorian and Edwardian houses. Although there is some potential for mid-rise development along Balham High Road, the majority of the area is not considered to have potential for tall buildings. There is one tall building zone identified, close to the tube and mainline railway station, on the site of an existing supermarket and associated car parking.

storeys is considered to be the absolute maximum this site could accommodate and this would depend entirely on a high quality design that is carefully placed within the plot in response to all the surrounding constraints and considerations.

### **TB-A1a-01**



**Existing prevailing height: 3 storeys**

**Appropriate height: 7-8 storeys (21-24m)**

The zone occupies the central part of the site, set back from the roads and other existing buildings, and towards Balham High Road, whilst allowing for public realm adjacent to Balham Station Road. The surrounding context of relatively low prevailing building heights and the scale of the roads, limits the capacity of the area to accommodate tall buildings. Particular sensitivities include the landmark Bedford Pub at the corner of Bedford Hill/Fernlea Road and the scale of Balham Station Road which has a blank façade to the south along the elevated railway line.

The tallest elements of any development should be set well within the zone respecting the scale of the surrounding streets and buildings. Buildings of 7-8

# B Battersea

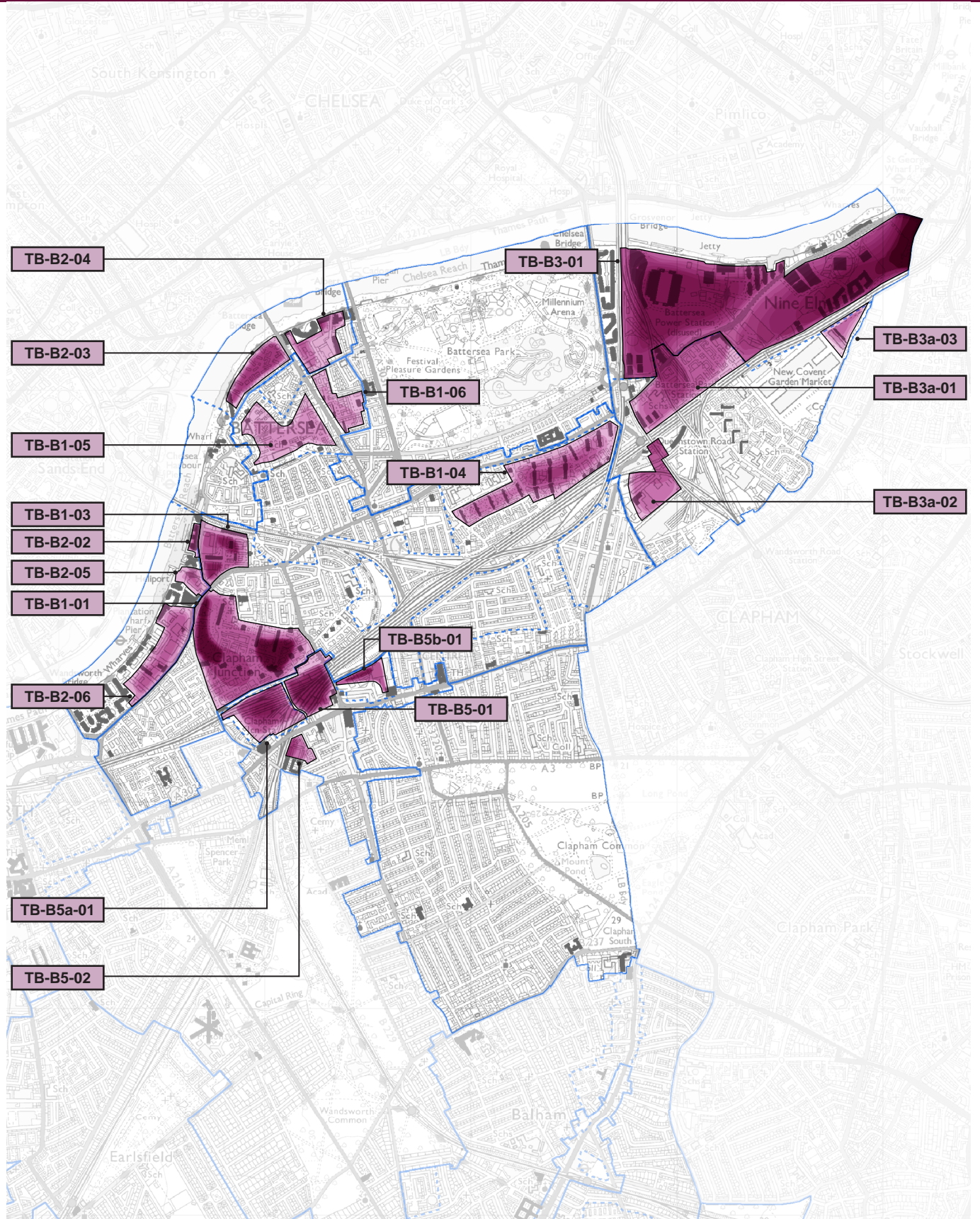


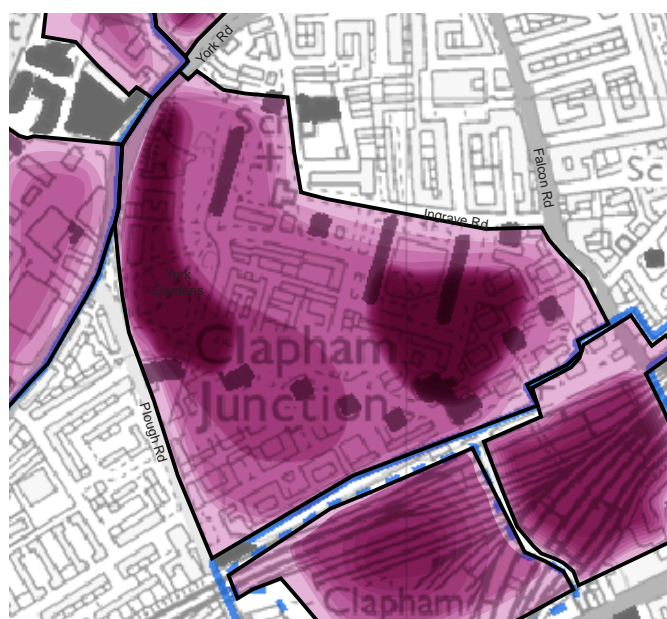
Fig. 222: Battersea Tall Buildings Zone map

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Battersea is home to the Nine Elms Opportunity Area (within the Central Activities Zone) and also the major town centre of Clapham Junction. The scale of the built environment provides potential for tall buildings across a number of different zones. However, to the south of the mainline railway there are limited opportunities as buildings become much more modest in scale leading towards Clapham Common. To the north of the railway, denser development must consider sensitive assets such as Battersea Park Registered Park & Garden and the Battersea Square Conservation Area. The River Thames is also an important consideration as an important characteristic of London as a whole, including the nearby Westminster World Heritage Site.

### TB-B1-01



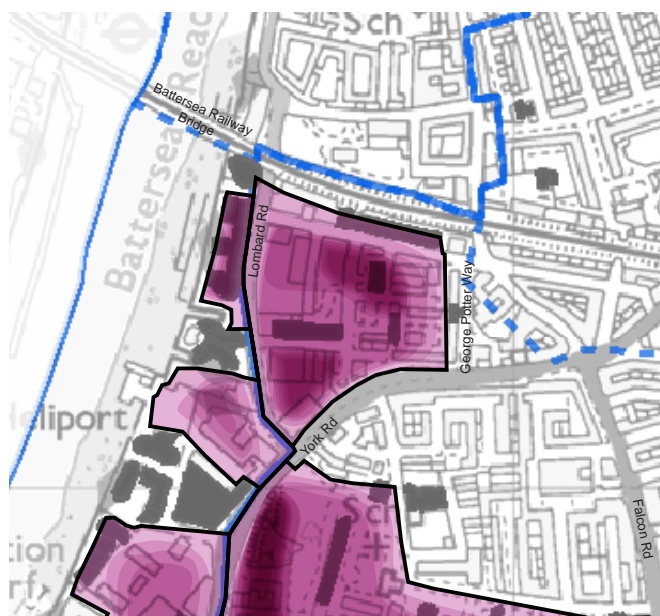
**Existing prevailing height: 3-23 storeys**

**Appropriate height: 7-20 storeys (21-60m)**

A mixed area which includes older post-war towers with more recent tall and mid-rise buildings particularly along the railway. Much of the area, including the existing site of York Gardens, is subject to a consented masterplan with buildings rising over 30 storeys tall set around a newly positioned large green space. There remains some capacity for new towers of around 20 storeys closer to the main road and the railway, when stepped appropriately from lower

buildings. Tall buildings need to protect the quality of the newly provided open space (i.e. by avoiding extensive overshadowing) and respond positively to surrounding low-rise development such as the terraced housing north of Ingrave Road. The majority of new development (beyond the consented schemes) would be expected to be 7-10 storeys with the character of the townscape having limited capacity for further tower elements.

### TB-B1-03



**Existing prevailing height: 2-20+ storeys**

**Appropriate height: 7-20 storeys (21-60m)**

A mixed area including older mid-rise and tall buildings set alongside modern tall buildings either side of the heliport site. There are some low-rise commercial uses which have consented tall building developments in progress. Development here needs to protect the character of the River Thames in addition to smaller scale buildings south of York Road and east of George Potter Way. The tallest buildings should be set within the plot and step down appropriately to adjacent roads, buildings and also the approach to the listed Battersea Railway Bridge. The consented tall building at 55-59 Lombard Road in the south should form the tallest building in this zone.

## TB-B1-04

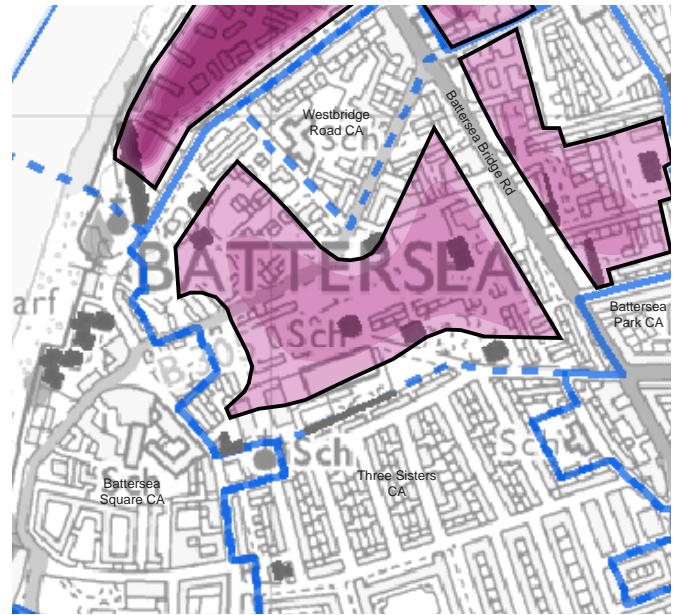


**Existing prevailing height: 2-20+ storeys**

**Appropriate height: 7-12 storeys (21-36m)**

An area of post-war estates including 2 storey short terrace blocks set amongst large scale 10 to 12 storey blocks facing east-west. The area includes a number of tower blocks over 20 storeys which are considered detractors on the surrounding sensitive character. Development within this zone needs to have particular regard to Battersea Park Registered Park & Garden which lies to the north beyond a series of grand terraced streets. Development should not introduce any new built elements into views from within Battersea Park. Development is further constrained by Parktown Estate Conservation Area to the east and south, Shaftesbury Park Estate Conservation Area to the south west and Latchmere Estate Conservation Area close to the western boundary of the zone. The tallest elements of any development should be set well within this zone, with the locations based on a detailed analysis of sensitive views. Development should also respect the small scale nature of residential properties along Blondel Street and Rowditch Lane.

## TB-B1-05

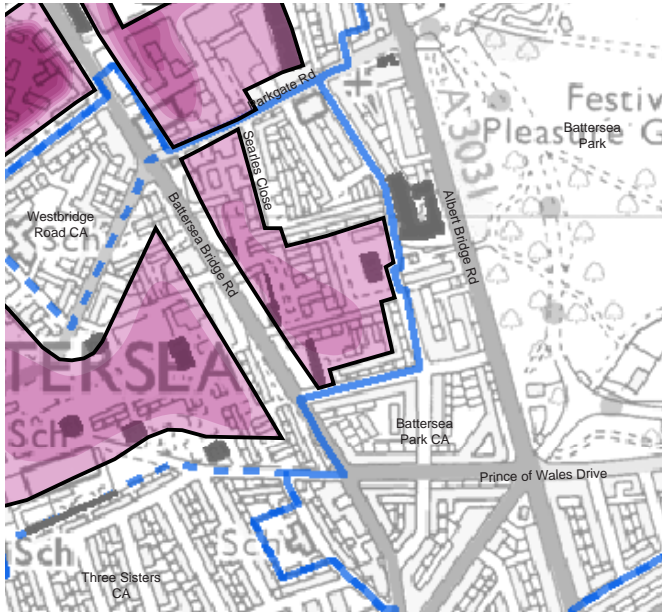


**Existing prevailing height: 4-20+ storeys**

**Appropriate height: 7-8 storeys (21-24m)**

These zones include a series of post-war estates which benefit from extensive mature trees filtering views of the main tower blocks which rise over 20 storeys. Development within this zone needs to have particular regard to Battersea Park Registered Park & Garden which lies to the east. Development should not introduce any new built elements into views from within Battersea Park. The area is surrounded by conservation areas which protect areas of period low-rise housing and also the historic Battersea Square at the heart of the old town which includes the riverside grade I listed St Mary's Church. Therefore, while there is some potential for 7-8 storeys, these should be positioned based on detailed townscape, visual and heritage assessments and considered cumulatively alongside existing and consented other developments. The approach to the listed Battersea Bridge is excluded from the tall building zone to avoid creating a canyon effect and preserve positive views towards this historic river crossing.

## TB-B1-06

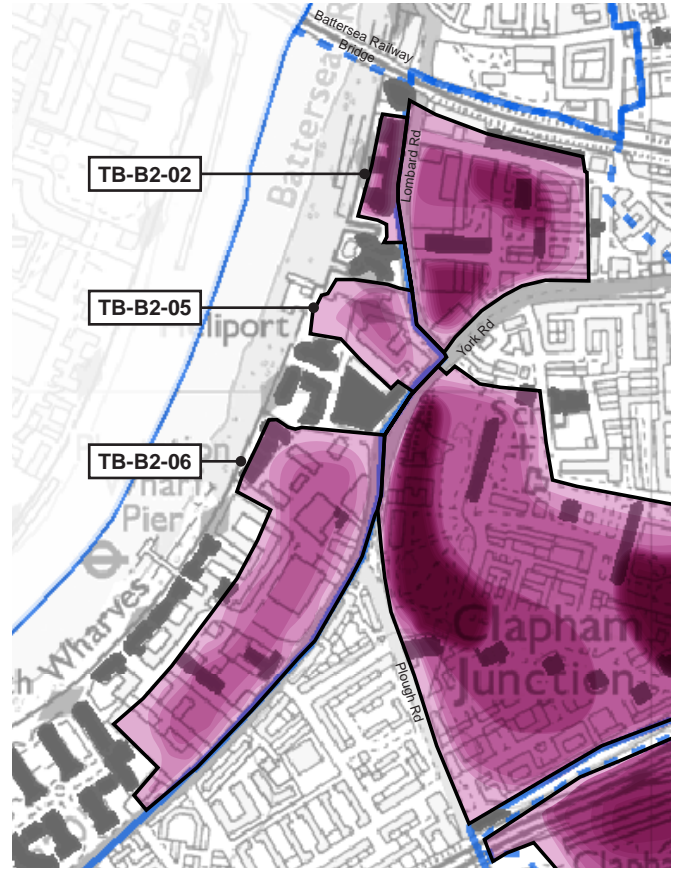


**Existing prevailing height: 4-17 storeys**

**Appropriate height: 7-8 storeys (21-24m)**

These zones include a series of post-war estates and 7-8 storey blocks generally set back from Battersea Bridge Road. The area includes a 17 storey tower towards the boundary with Battersea Park Conservation Area. Development within this zone needs to have particular regard to Battersea Park Registered Park & Garden which lies to the east. Development should not introduce any new built elements into views from within Battersea Park. While there is some potential for 7-8 storeys across the area, these should step down towards nearby smaller scale development such as the period terraces east of Searles Close. A mid-rise building zone provides a transition to the south and east of this zone. Buildings should also be set back from Battersea Bridge Road to protect the approach to the listed Battersea Bridge.

## TB-B2-02, TB-B2-05 and TB-B2-06



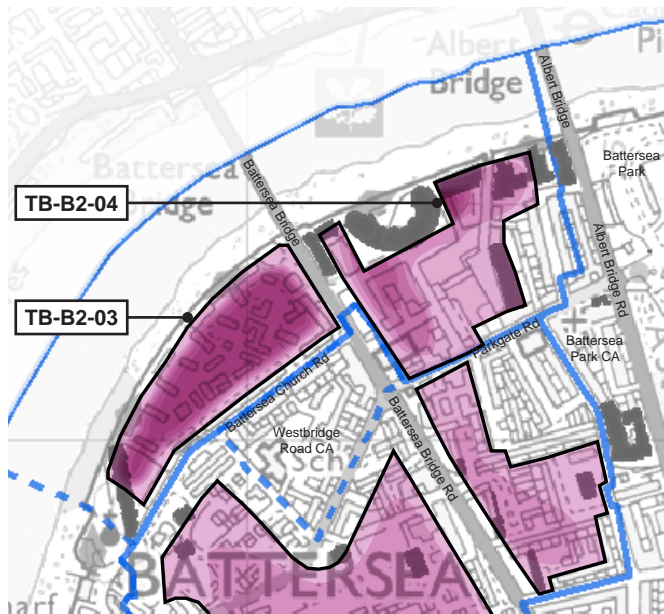
**Existing prevailing height: 2-20+ storeys**

**Appropriate height: 7-10 storeys (21-30m)**

A mixed area including older mid-rise and tall buildings set alongside modern tall buildings either side of the heliport site and extending south along the riverfront. Development here needs to protect the character of the River Thames in addition to smaller scale buildings to the south east of York Road, some of which fall within a mid-rise building zone. Buildings on the opposite bank of the river are similar in scale and style, set around the formal Imperial Park.

The area already contains a number of blocks and towers higher than 10 storeys, with a number of further developments consented. To preserve the character of the retained period buildings and to avoid over-development of the River Thames frontage, the appropriate height range for further buildings is considered to be 7-10 storeys.

## TB-B2-03 and TB-B2-04

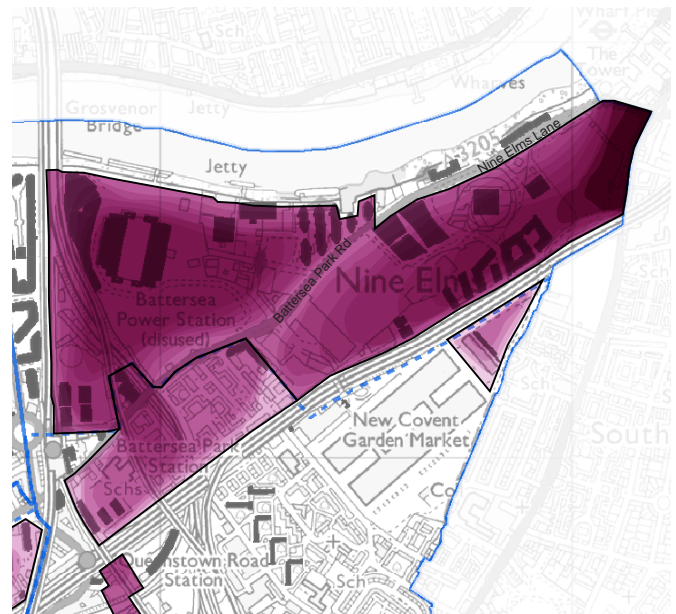


**Existing prevailing height: 3-18 storeys**

**Appropriate height: 7-12 storeys (21-36m)**

A riverside area dominated by two modern buildings rising to 12 and 18 storeys respectively. Existing buildings across the rest of the area are more modest in scale but could receive additional height provided the cumulative impact on the River Thames is fully considered, and the setting of key heritage assets such as Battersea Park Registered Park and Garden and the listed Battersea Bridge are protected. The 18 storey building in the west of zone TB-B2-03 detracts from the setting of the grade I listed St Mary's church and it is not considered that this height could be replicated anywhere else within this area. Apart from the recent Lots Road Power Station Development and the mid 20<sup>th</sup> century Worlds End Estate, the north bank of the river is relatively small scale and green in character. Development will need to respond positively to this. Development within zone TB-B2-04 should be planned carefully in relation to the small scale Ransomes Dock feature. Buildings should step down towards the approaches to the listed Battersea Bridge and Albert Bridge. Buildings in zone TB-B2-03 should also have regard to the small-scale character of the adjacent Westbridge Road Conservation Area.

## TB-B3-01



**Existing prevailing height: 6-40+ storeys**

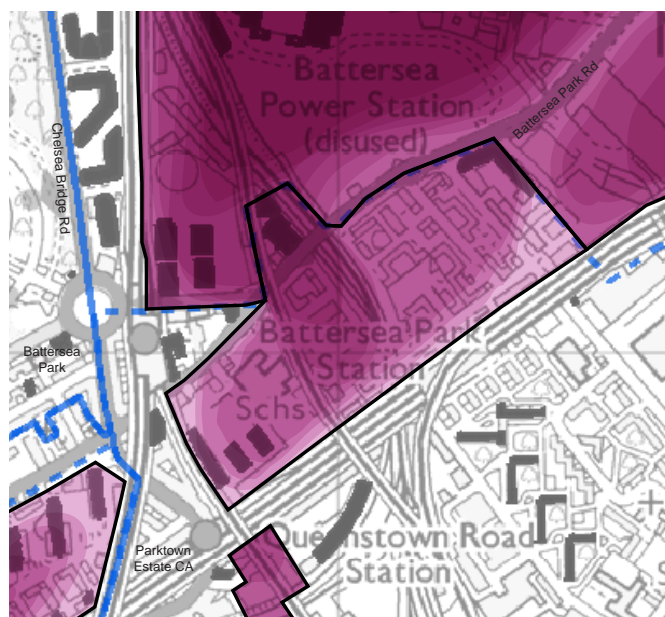
**Appropriate height: 8-25 storeys (24-75m)**

This is the main Nine Elms Opportunity Area development site focused around Battersea Power Station within the London Central Activities Zone. The zone supports clustered tall building development to the north and east of the mainline railway, provided the setting of Battersea Power Station and Battersea Park Registered Park & Garden are protected. Whilst the majority of this area is now developed or under development, any further proposals will need to assess and ensure no impacts to the nearby Palace of Westminster and Westminster Abbey, including St Margaret's Church World Heritage Site. However, further buildings within the appropriate height range for this zone would be considered not to harm the outstanding universal value of this World Heritage Site provided buildings are of a high quality design in line with the guidance provided in [Appendix A](#).

The existing and consented developments include a number of towers over 25 storeys, rising to over 50 (150m) in height. This are primarily clustered in the core of the Opportunity Area close to Vauxhall in neighbouring LB Lambeth. When these consented developments are built out there is considered limited capacity for further extra high towers, and further development should be consistent with the more modest scale 10-25 storey blocks already located along the riverside and the mainline railway. The heat map therefore shows the tallest form of development within this zone as to the far east within the area indicated in

the [Site Specific Allocations Document](#) (March 2016), which notes a zone which is not affected by World Heritage Site views. Outside of this zone development should not exceed 20 storeys. The form of development that will be supported in the Opportunity Area will be 8-10 storey high density development with tall buildings on key sites such as along the riverside and in prominent locations along strategic routes as set out in the public realm strategy. Tall buildings must respond to the specific restrictions set out in the London View Management Framework. Generally any development proposals which come forward in this area will need to respond to the detailed considerations set out in the [Site Specific Allocations Document](#) (March 2016) which includes more detailed local context, protected views and settings of heritage assets. Refer also to the [Local Views SPD](#) (2014) as an evidence base for protected views looking towards Battersea Power Station.

### TB-B3a-01



**Existing prevailing height: 2-12 storeys**

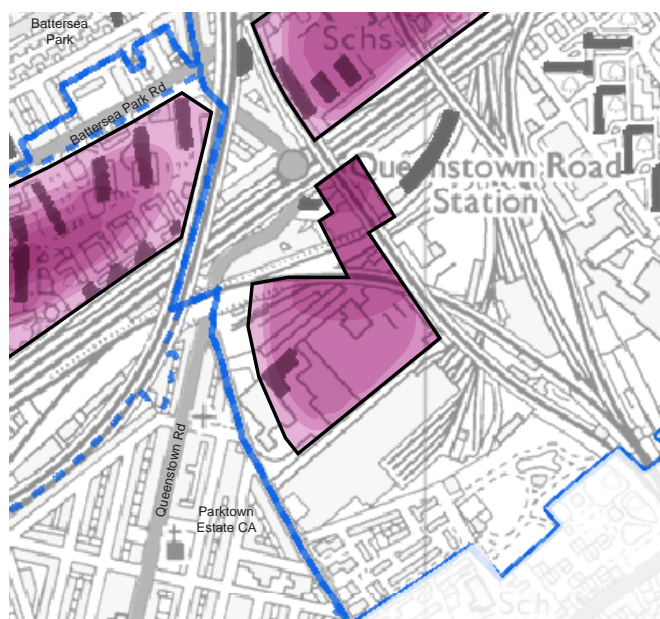
**Appropriate height: 7-13 storeys (21-39m)**

This zone is an area of mixed development ranging between two and 12 storeys, dissected by a number of wide railway corridors which fragment the overall built form. There is capacity for tall buildings along the railway corridors, but development requires careful placement to preserve views towards Battersea Power Station. A number of existing buildings are relatively modest in scale and new developments need to ensure appropriate stepping down to these where retained. The tallest buildings within this zone are likely to be most

suited to the north of the area where elevated railway infrastructure means additional height would sit most comfortably.

Tall buildings will need to respond positively to the adjacent Parktown Estate Conservation Area and the nearby Battersea Park Registered Park & Garden and Conservation Area. Refer also to the [Local Views SPD](#) (2014) as an evidence base for protected views looking towards Battersea Power Station.

### TB-B3a-02

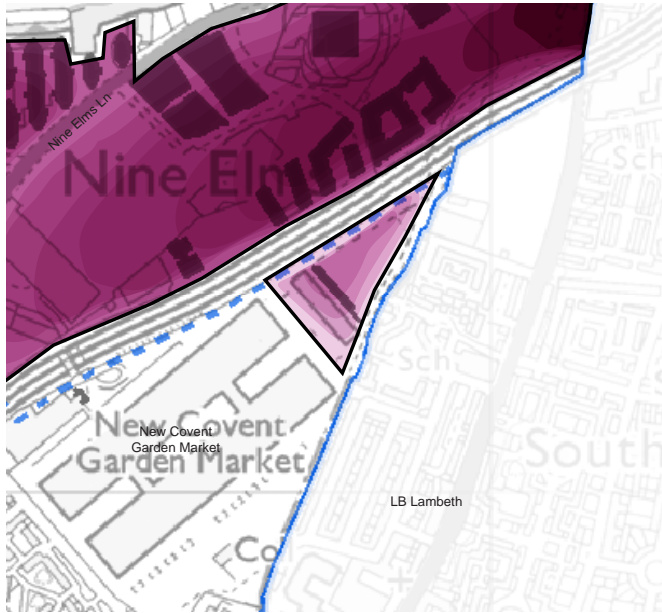


**Existing prevailing height: 2-5 storeys**

**Appropriate height: 7-11 storeys (21-33m)**

This zone is an area of largely low rise industrial and commercial uses among the network of railway lines converging as they come into London. There is capacity for tall buildings within the large development plots, along the railway lines and specifically around Queenstown Road train station. However, development requires careful placement to preserve views towards Battersea Power Station while also respecting the smaller scale character of the area, particularly to the south and west. In particular, development should step down towards the small scale and consistent Parktown Estate Conservation Area to the west. Tall buildings also need to respond positively to the nearby Battersea Park Registered Park & Garden and Conservation Area. Refer also to the [Local Views SPD](#) (2014) as an evidence base for protected views looking towards Battersea Power Station.

### TB-B3a-03

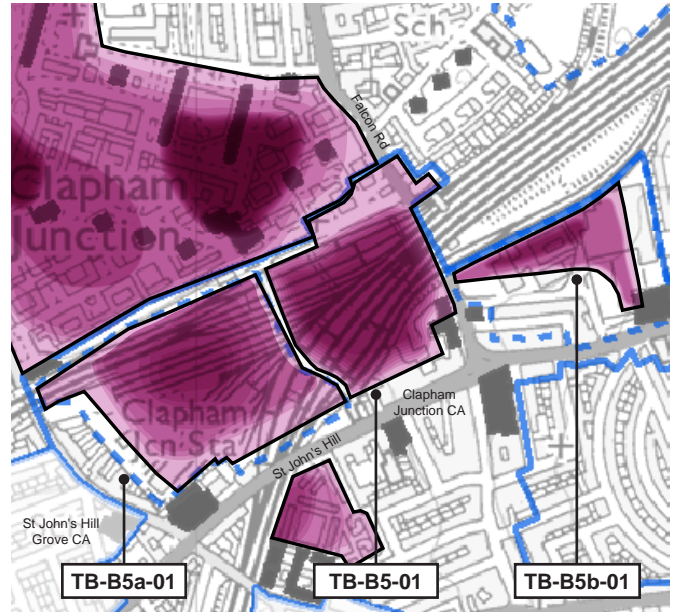


**Existing prevailing height: 0-4 storeys**

**Appropriate height: 7-10 storeys (21-30m)**

This zone is predominantly a cleared development plot with open hardstanding adjacent to Vauxhall Market, close to the new Nine Elms London Underground station on the recently extended Northern Line. There is capacity for tall buildings across the zone, stepping down from the adjacent very tall towers within LB Lambeth to the east.

### TB-B5-01, TB-B5a-01 and TB-B5b-01

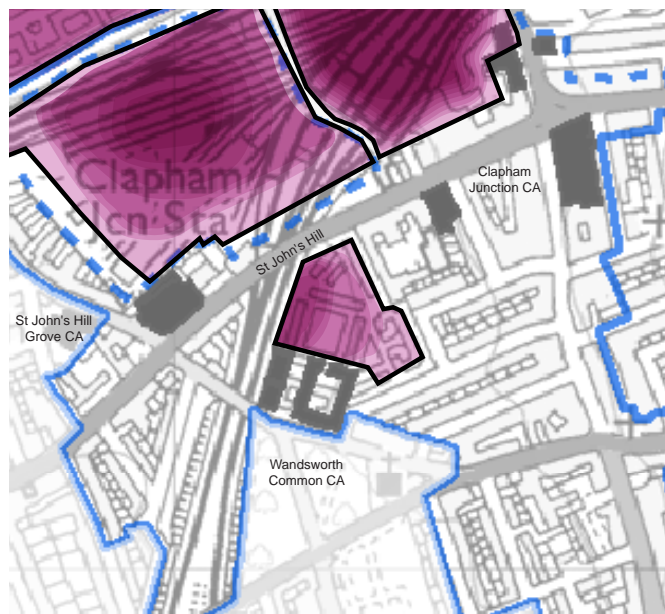


**Existing prevailing height: 2-8 storeys**

**Appropriate height: 7-15 storeys (21-45m)**

This zone is focused around Clapham Junction railway station and is dominated by the scale and width of the mainline railway tracks in this location. Development within this zone must be carefully planned to protect the setting and character of local landmarks and heritage assets such as the Arding & Hobbs building and also views from the train station railway bridge towards central London. The zone is in close proximity to small scale residential terraces and development must ensure appropriate stepping down to protect the character of these - particularly within Clapham Junction Conservation Area and St John's Hill Grove Conservation Area. Buildings exceeding 7-8 storeys in the southern parts of these plots are therefore unlikely to be appropriate. Development on the northern side of the railway within zone TB-B5-01 should also be no more than 7-8 storeys to respond to smaller scale existing buildings along Falcon Road. An adjacent mid-rise building zone provides a transition to the street frontage of St John's Hill in this location.

## TB-B5-02



**Existing prevailing height: 1-5 storeys**

**Appropriate height: 7-12 storeys (21-36m)**

This zone is adjacent to a completed phase of a wider masterplan for this area, focused along the mainline railway coming into Clapham Junction railway station which is in deep cutting at this point. Tall buildings within this zone should generally be located towards the railway line, stepping down towards the smaller scale period housing to the east and south of the zone. Development should also not overly dominate and overshadow the railway line passing to the north in cutting, and the appropriate heights have therefore been set accordingly. Development also needs to respond positively to the nearby Clapham Junction Conservation Area and Wandsworth Common Conservation Area.

# C Earlsfield and Southfields

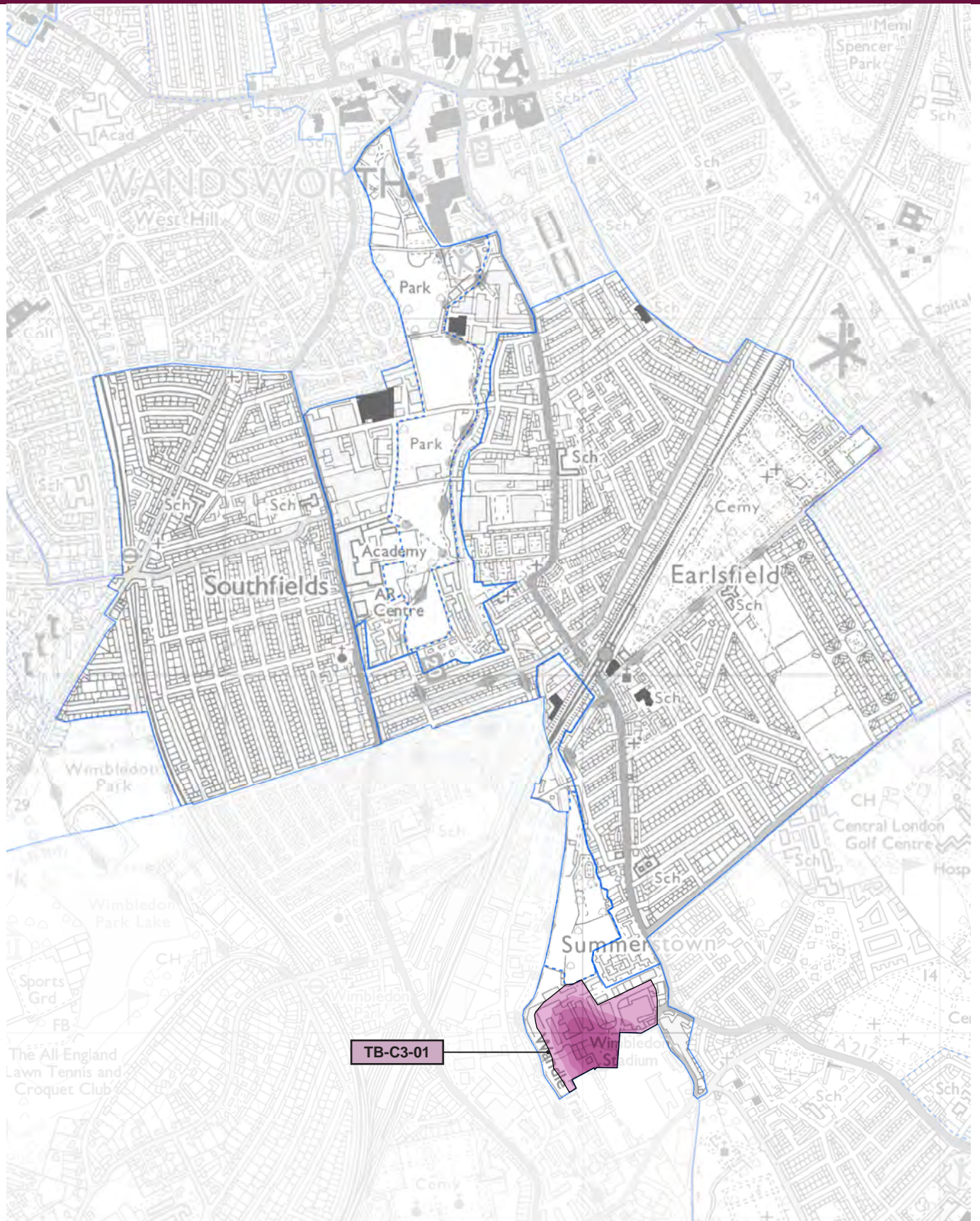


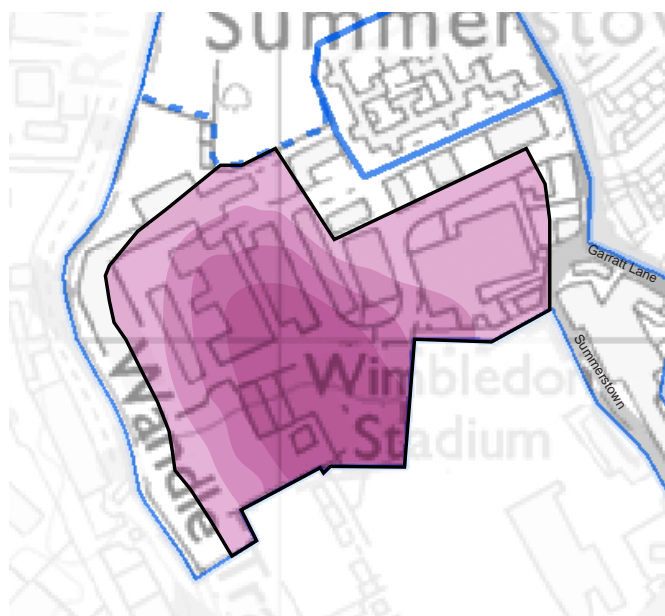
Fig. 223: Earlsfield and Southfields Tall Buildings Zone map

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Southfields has a highly consistent and small scale built environment which has no capacity to accommodate tall buildings without causing harm to the overall character of the area. While the development pattern and ages is more mixed across Earlsfield, it remains small-scale in character with narrow residential streets and relatively small open spaces. The majority of Earlsfield therefore also does not have capacity for tall buildings, although there are some areas with potential to accommodate mid-rise development. The only tall building zone within this Place is at the borough boundary adjacent to a tall ongoing development in LB Merton.

### **TB-C3-01**



**Existing prevailing height: 2-3 storeys**

**Appropriate height: 7-10 storeys (21-30m)**

The zone occupies an area of commercial and industrial uses at border with LB Merton, where a tall building development on the site of the former greyhound racetrack and around the AFC Wimbledon stadium is underway. The zone is offset from neighbouring small-scale buildings and the River Wandle corridor. The landscape setting of nearby Garratt Park will need to be assessed in the development of any tall building proposals to ensure the green character is not adversely affected.

# D Putney

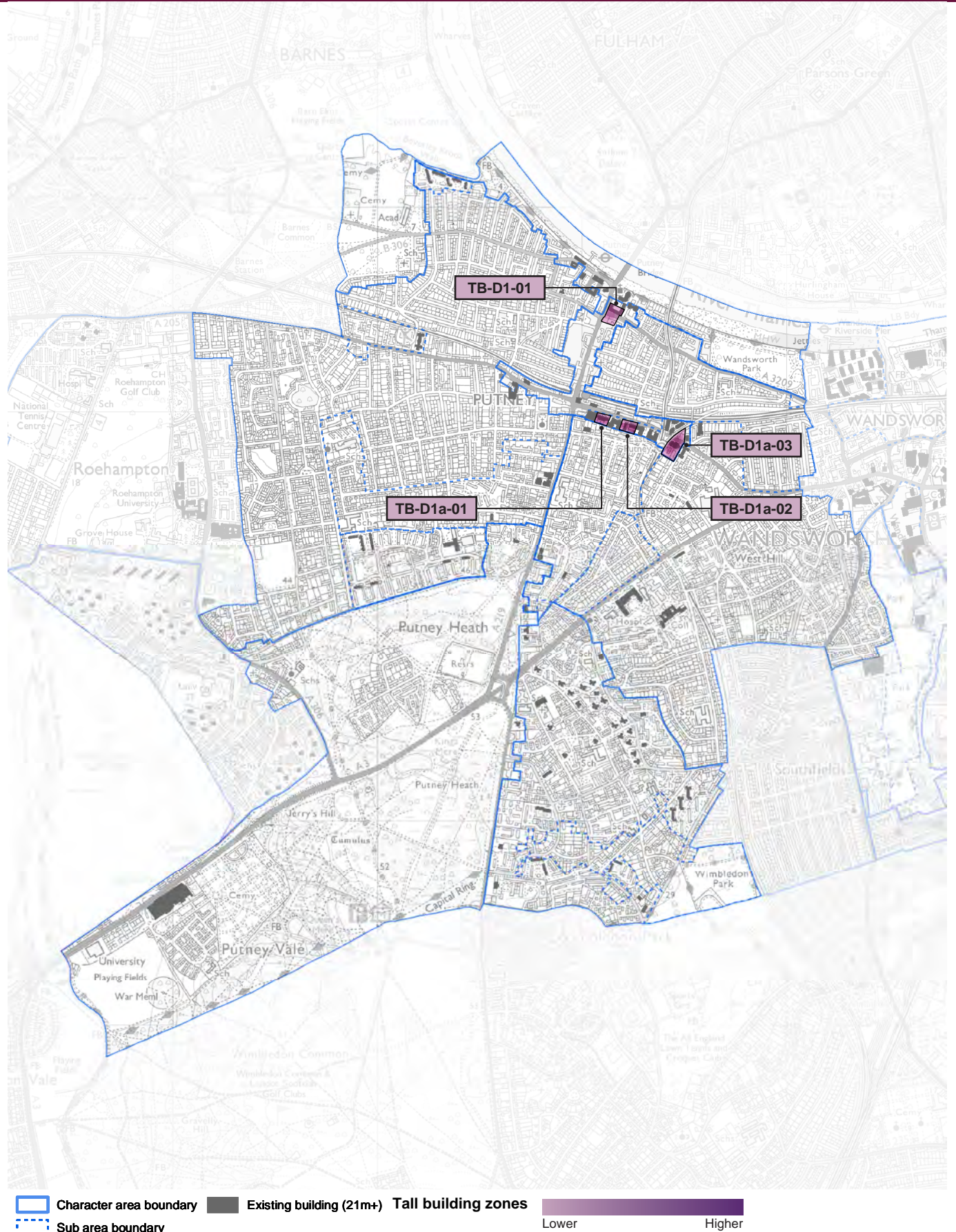
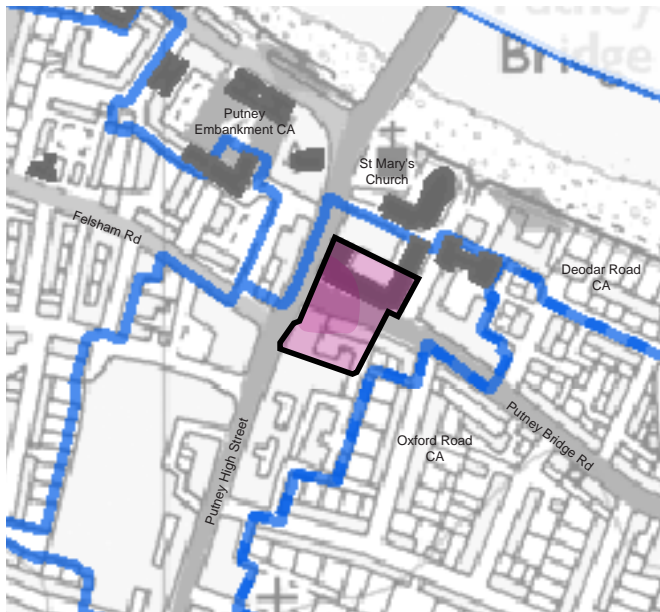


Fig. 224: Putney Tall Buildings Zone map

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Putney has a well established, small scale and historic character. While the residential areas include some small scale estates in places, these are mostly below the threshold for tall buildings or well integrated amongst mature tree planting. The majority of the area therefore does not have capacity for new tall building development, with the exception of parts of Upper Richmond Road within the town centre and a small zone further down Putney High Street towards the riverside.

### TB-D1-01



**Existing prevailing height: 3-9 storeys**

**Appropriate height: 7-8 storeys (21-24m)**

This small zone along Putney High Street is characterised by existing tall and mid-rise buildings. The scale of the plot provides opportunities for some height within parts of the zone, but development is heavily constrained by the scale of the surrounding buildings, the modest scale of development along Putney High Street and the extensive heritage assets including:

- the listed St Mary's Church on the riverside;
- Deodar Road Conservation Area;
- Oxford Road Conservation Area;
- Putney Embankment Conservation Area; and
- the listed Putney Bridge including views towards it.

The existing tall building at Putney Wharf adjacent to this area should not be seen as justification for the creation of further landmark tall buildings or attract the development of a tall building cluster.

Heights in excess of 8 storeys are considered likely to give rise to significant adverse effects on the sensitive character and modest scale of this historic town centre.

### TB-D1a-01, TB-D1a-02 and TB-D1a-03



**Existing prevailing height: 6-11 storeys**

**Appropriate height: 7-10 storeys (21-30m)**

An area of existing 20<sup>th</sup> century tall buildings (amongst more recent 21<sup>st</sup> century tall developments) along Upper Richmond Road within the eastern edge of Putney Town Centre. The zone abuts an area bounded by three railway lines which includes recently developed buildings up to 11 storeys. The scale of Upper Richmond Road means tall buildings are appropriate, but they need to be balanced with the adjacent Oxford Road Conservation Area which has a retained row of period terraces. Development also needs to step down towards the nearby East Putney and West Putney Conservation Areas.

# E Roehampton

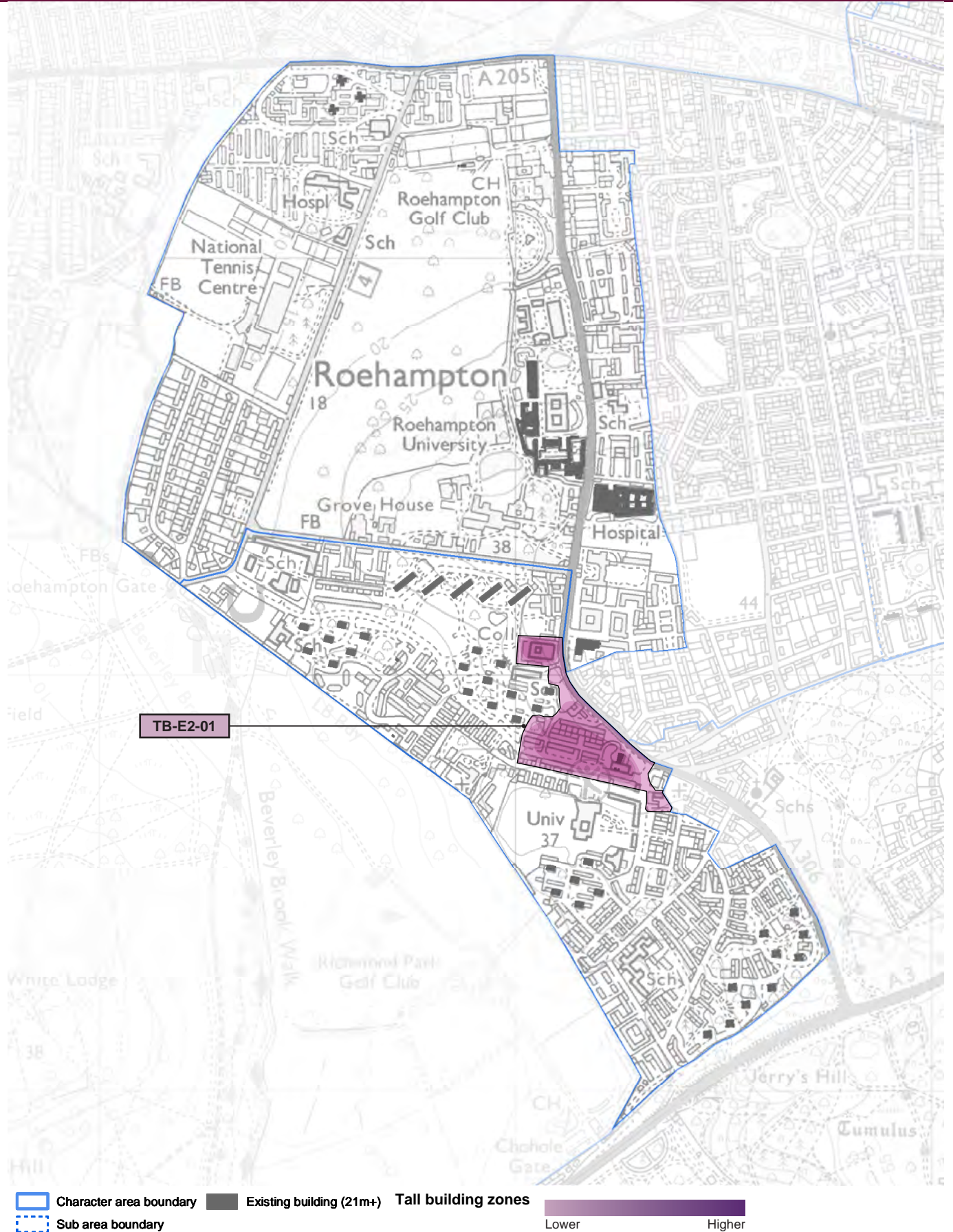


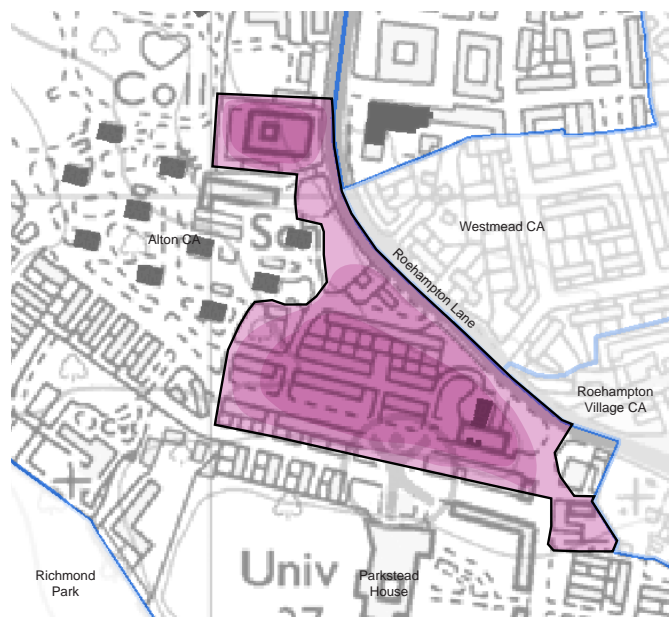
Fig. 225: Roehampton Tall Buildings Zone map

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Roehampton is a mixed area, with potential for some increases in height in the older parts of the area but generally not tall buildings. The listed 1950s towers in the south of the area are valued for their representation of a revolutionary new building style, but their height impacts upon the open character and generally wooded skyline of neighbouring Richmond Park. There is potential for some tall building development in the area adjacent to these post-war estates along Roehampton Lane. However these would need to consider the settings of Alton East and West Registered Parks and Gardens in addition to Richmond Park.

tall building development should be no higher than 9 storeys, with individual plots planned to respond appropriately to the surrounding conservation areas and the setting of the adjacent Richmond Park Registered Park & Garden. Existing mature planting across the wider area is valuable in softening their impact on the character of the area, and should be retained where possible. Development must also have regard to the setting of the grade I listed Parkstead House located to the south of the tall building zone.

### **TB-E2-01**



**Existing prevailing height: 2-10 storeys**

**Appropriate height: 7-9 storeys (21-27m)**

This is an area of relatively low quality existing estate blocks surrounded by Alton, Roehampton Village and Westmead Conservation Areas. Part of the zone extends into Alton Conservation Area adjacent to a collection of five 12 storey towers which forms part of the Alton West Estate built in the 1950s. These towers are visible above the tree line from a number of sensitive viewpoints within the adjacent Richmond Park and also look incongruous with the small scale and relatively suburban character of the surrounding area which has low public transport accessibility. Therefore, new

# F Tooting

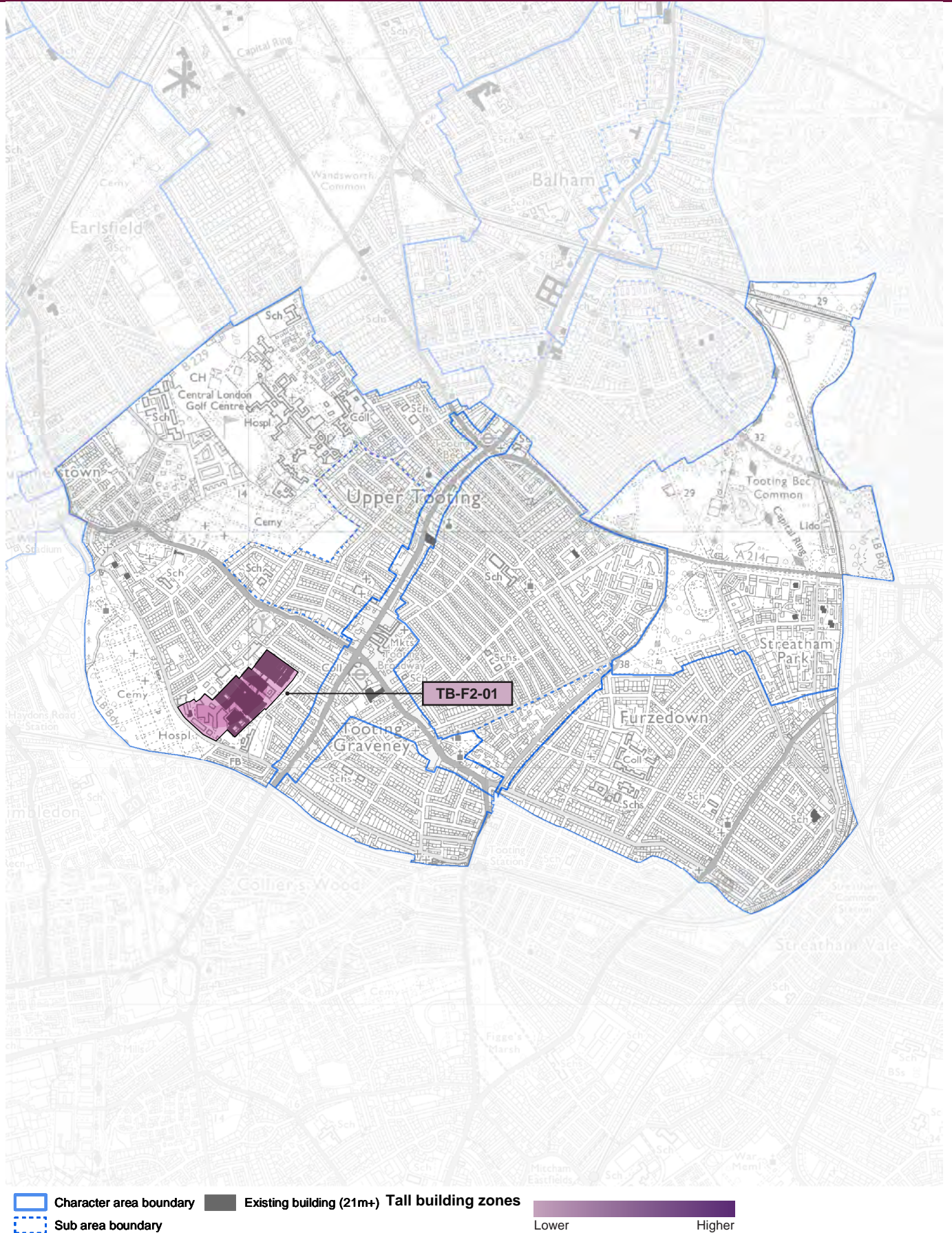
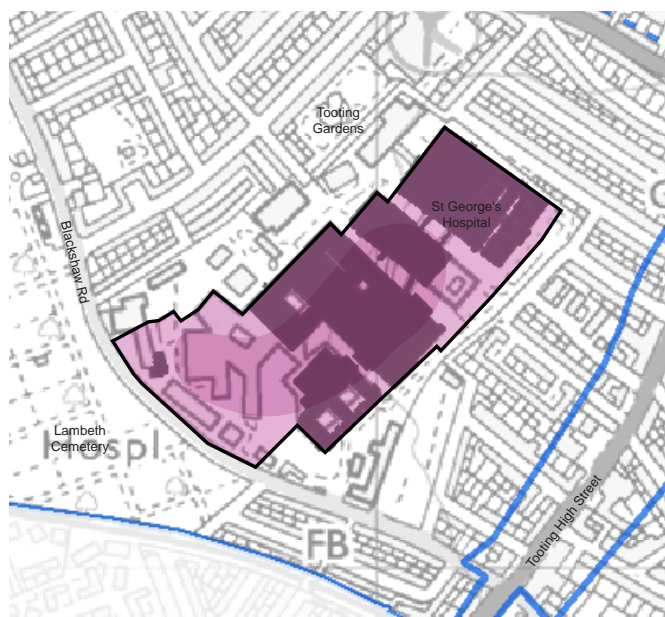


Fig. 226: Tooting Tall Buildings Zone map

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Tooting represents another area of small scale development within the borough, including both the residential areas and the town centre running along Tooting High Street, Upper Tooting Road and Mitcham Road. The pattern of the built environment therefore does not present many opportunities for tall building development, with the exception of around the St George's Hospital site.

### **TB-F2-01**



**Existing prevailing height: 2-13 storeys**

**Appropriate height: 7-8 storeys (21-24m)**

The zone contains the site of St George's Hospital and open land (including parking), commercial and residential buildings surrounding. The zone has capacity for tall buildings in parts of the area, although development will need to ensure appropriate offsets are provided from surrounding small scale residential properties and the small-scale Tooting Gardens. Development should protect the green character and sense of openness from within Lambeth Cemetery. Note that the existing building heights are derived from the latest available data, which are sometimes extended across an entire building footprint. These inconsistencies in data have not influenced the determination of tall building zones or the appropriate heights defined.

# G Wandsworth Town and Common

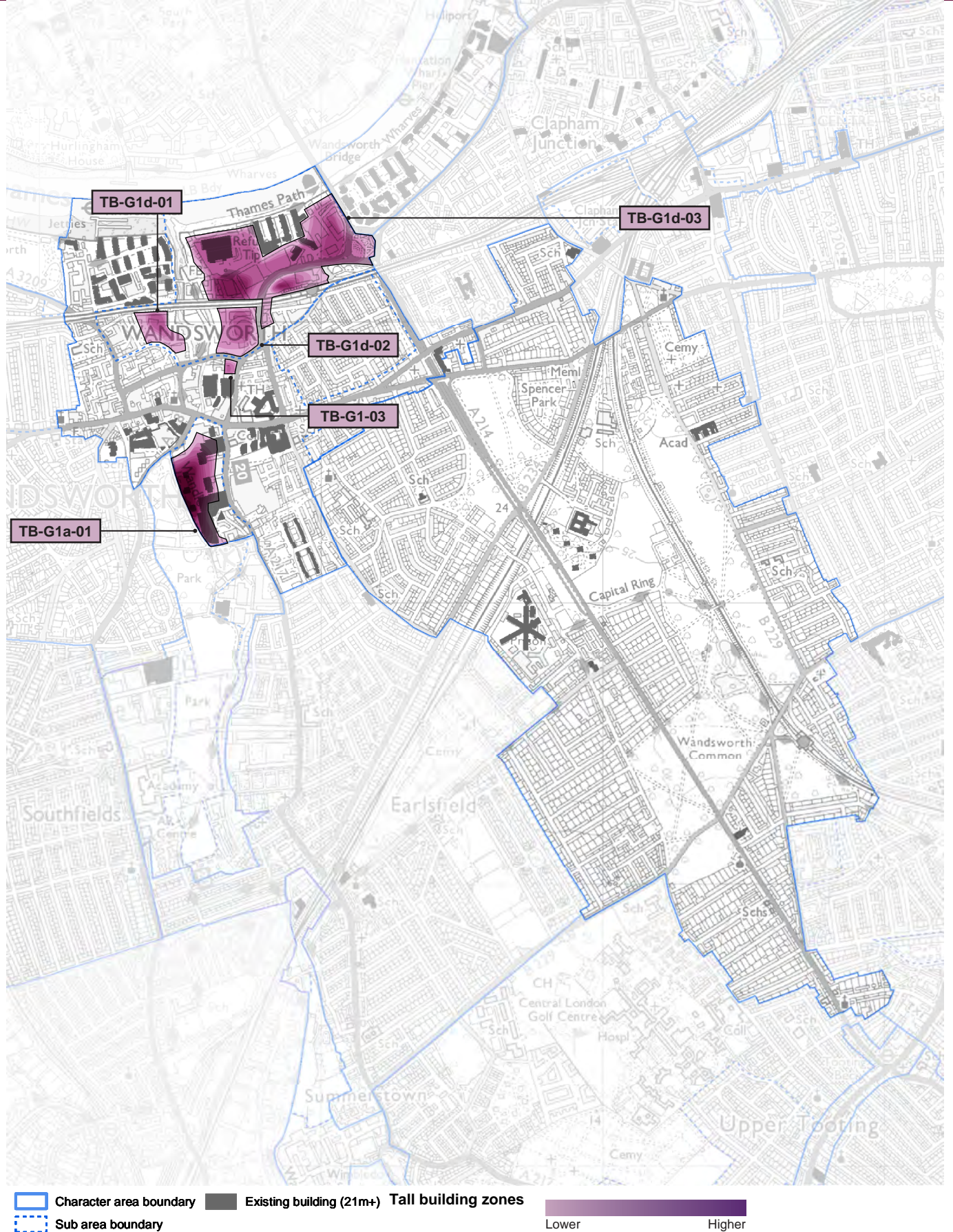


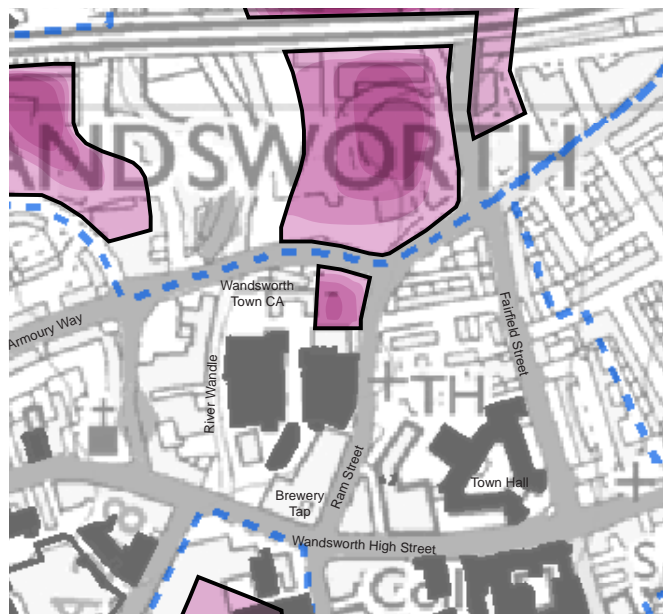
Fig. 227: Wandsworth Town and Common Tall Buildings Zone map

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Wandsworth town centre is a major redevelopment zone within the borough, with potential development areas along the High Street, along the Thames frontage and through the Wandle Valley beside King George's Park. The scale of the roads (including the A3 corridor) and the mainline railway, falling within an economic use intensification area, also allows the potential for further tall buildings. However, the area has a significant number of valued historical assets including listed buildings and conservation areas, which need careful consideration with any development proposals.

### TB-G1-03



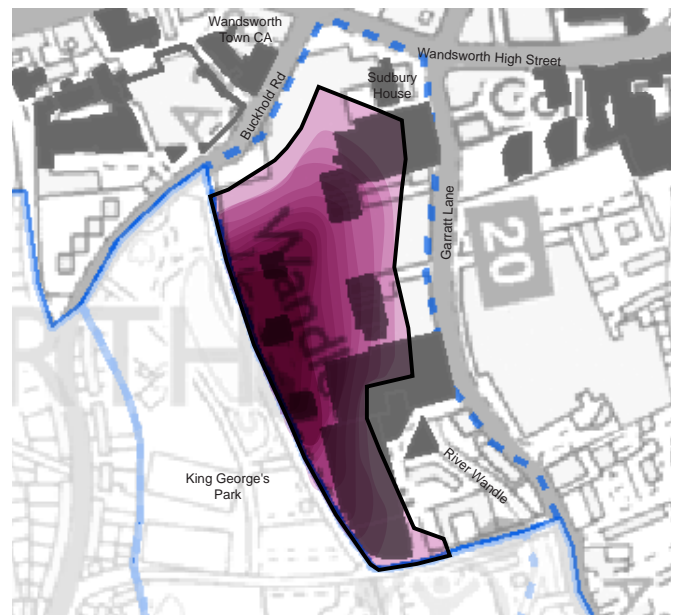
**Existing prevailing height: 1-2 storeys**

**Appropriate height: 7-10 storeys (21-30m)**

This zone is within Wandsworth town centre, set between Wandsworth High Street and Armoury Way which form part of the A3 loop passing through the area. Existing tall development in the wider area threatens the historic relatively small-scale character of the town centre and new buildings need to be appropriately set back from the road to protect local views and the setting of Wandsworth Town Conservation Area (which the zone is within) and listed buildings such as the Town Hall. The listed Brewery Tap sits to the south of this zone beyond other recently developed buildings. Development should respect this local historical landmark building. Development should

also be appropriately set back from the River Wandle to the west to ensure a quality river corridor is created with limited overshadowing. Much of the area around zone has already been developed as tall buildings, with limited scope for further densification other than the small area shown. The area to the west of this zone is likely to be developed but is not considered appropriate for tall buildings due to the presence of the grade II listed Ram Brewery stables.

### TB-G1a-01



**Existing prevailing height: 3-20+ storeys**

**Appropriate height: 7-20 storeys (21-60m)**

This zone extends south along Garratt Lane from close to Wandsworth High Street, to the edge of King George's Park. The zone is mostly occupied by the Southside Shopping Centre and includes three 20 storey towers along the park boundary. Existing tall development beyond the zone threatens the historic relatively small-scale character of the town centre - particularly in respect of the 23 storey Sudbury House. Therefore, new buildings need to be appropriately set back from the adjacent streets to protect local views and the setting of Wandsworth Town Conservation Area and listed buildings such as the Town Hall. The small scale character of Garratt Lane Burial Ground also requires careful consideration and for tall buildings to step down towards it. The large scale of King George's Park presents the main opportunity for taller buildings, although these should protect the green character and sense of openness within this open space.

### TB-G1d-01



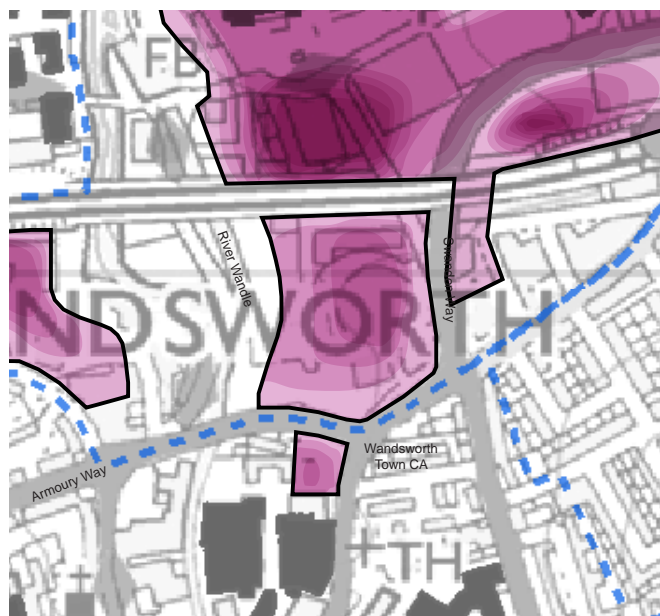
**Existing prevailing height: 2-3 storeys**

**Appropriate height: 7-10 storeys (21-30m)**

This zone is currently occupied by a mix of industrial and commercial premises, in addition to hardstanding and the construction area of the Thames Tideway Tunnel Dormay Street site. The zone is close to existing small scale residential buildings, particularly along Sudlow Road, and development must step down towards these appropriately. Development should also step appropriately towards the narrow River Wandle corridor to prevent overshadowing of this feature.

The zone is partially surrounded by Wandsworth Town Conservation Area which protects this historic core from inappropriate development. The whole of this area is subject to a consented masterplan which has been reviewed for its appropriateness in line with the methodology for this study.

### TB-G1d-02



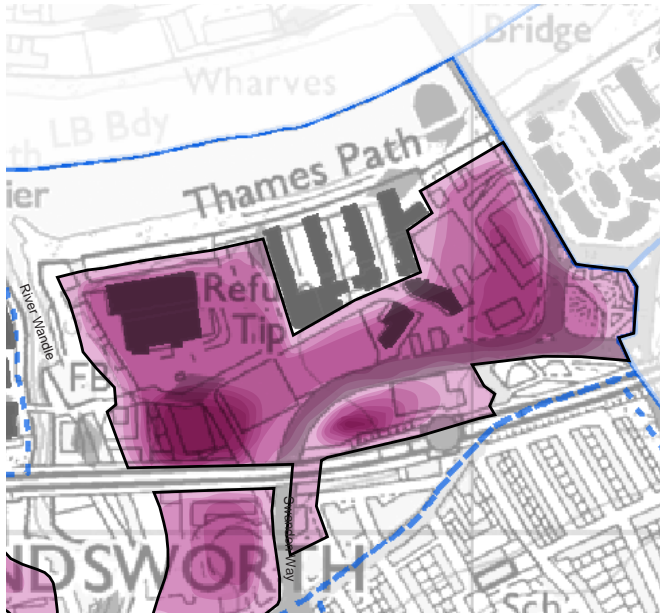
**Existing prevailing height: 2-3 storeys**

**Appropriate height: 7-10 storeys (21-30m)**

This zone is currently occupied by a disused gas holder. Buildings should range in height from west to east with taller elements in the east and north east of the zone while allowing for stepping down towards the River Wandle, Wandsworth Town Conservation Area and smaller scale buildings further to the east.

The whole of this area is subject to a consented masterplan (the Wandle Delta masterplan) which has been reviewed for its appropriateness in line with the methodology for this study.

### TB-G1d-03



Development closest to Swandon Way must respect the small scale character of Old York Road Conservation Area, which includes a small local centre around Wandsworth Town train station. Development to the south of Swandon Way must also step down towards Bramford Gardens.

**Existing prevailing height: 4-8 storeys**

**Appropriate height: 7-15 storeys (21-45m)**

This riverside zone has opportunities for tall buildings in keeping with the scale of the River Thames at this location. The zone sits beside a modern riverside development which rises to over 20 storeys in height. This zone mostly reflects an industrial character, including a large waste transfer station east of the River Wandle, although there is a modest scale recent development in the centre of the area. Part of the Wandle Delta masterplan (also on zones TB-G1d-01 and TB-G1d-02) are located within the eastern part of this zone and include buildings up to 15 storeys.

Care must be taken to protect the small scale nature of the River Wandle by setting development back from its edges. The zone is bounded by the busy and noisy Swandon Way and associated Wandsworth Roundabout gyratory. Developments should seek to improve the character and condition of this major infrastructure corridor which adversely dissects the area. Development in this zone needs to have very careful consideration of the relatively modest scale of the opposite river bank, including the green frontage of Hurlingham Park. In particular, the setting of nearby Fulham Palace Scheduled Monument to the north west needs to be protected by any development proposals in this zone. Therefore the tallest buildings within this plot should generally step away from the river and be more focused towards the elevated railway line and the gyratory, which could itself in the future become a focus for development.

## 4.6 Mid-rise buildings

### 4.6.1 Introduction

[Fig. 228](#) presents an overview map of zones with potential to accommodate mid-rise buildings. Each zone is supported by a description of the appropriate mid-rise building heights for that zone. The heights for these zones have been derived from the character area profiles which illustrate the overall character and development pattern within each area.

### 4.6.2 Borough-wide findings for mid-rise buildings

Overall, Wandsworth has capacity for mid-rise buildings across the borough, as shown on [Fig. 228](#).

Opportunities for mid-rise buildings are generally concentrated within five different types of area:

1. As **transition areas** to tall building zones: The majority of tall building zones have mid-rise building transition zones around them to provide appropriate stepping and integration to surrounding small scale built form.
2. Along **strategic road corridors**: The width of some roads provides the capacity for increased height, particularly at key junctions. This includes Upper Richmond Road in Putney, Balham High Road, Tooting High Street and West Hill.
3. Within **town centres**: There are opportunities for mid-rise buildings within the town centres of Putney, Clapham Junction and Balham (beyond the tall building zones), and within Tooting.
4. Within or adjacent to **existing estates**: This includes opportunities within Roehampton, Wimbledon Parkside, West Putney, East Putney, Tooting, Battersea and Nine Elms (south of the railway line).
5. Alongside **large scale open spaces**: Where mid-rise buildings would be seen in proportion to the scale of the open space, and visibility filtered by mature boundary trees. This includes locations like Roehampton Golf Club, Putney Heath, Wimbledon Park, King George's Park and Wandsworth Common.

In all cases, the acceptability of individual plots will depend on specific characteristics of the site. The tall building guidance provided in [Appendix A](#) is intended to help steer selection of appropriate sites and development of suitable building proposals.

### 4.6.3 General mid-rise building zone criteria

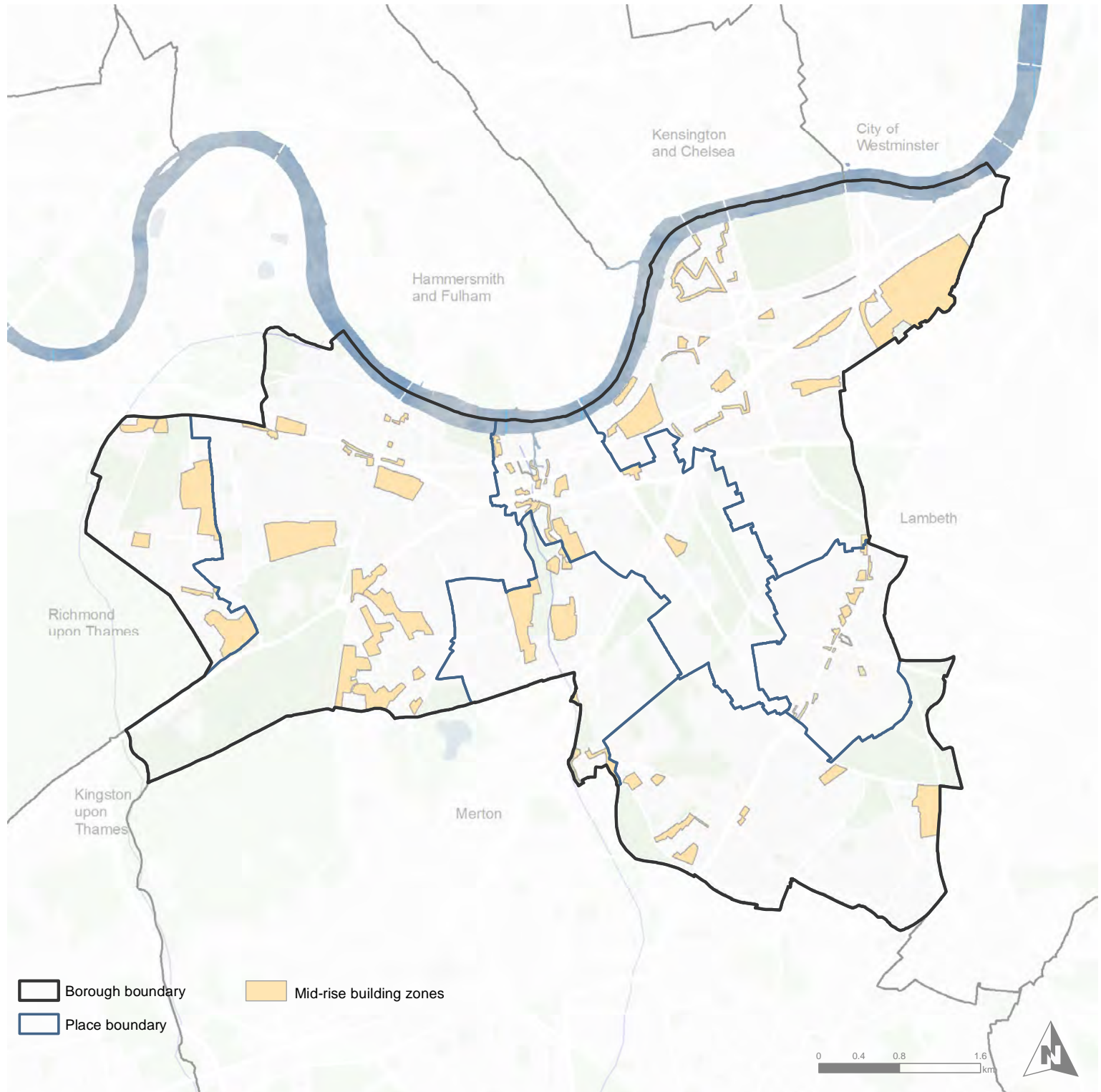
There are a number of criteria that apply generally to all of the mid-rise building zones. Mid-rise buildings shall:

- be carefully located and designed to step down to surrounding existing and proposed buildings.
- respond positively and protect the setting of existing buildings in the surrounding area, including heritage assets such as conservation areas, listed buildings and locally listed buildings.
- respect the scale, width and proportion of adjacent streets and watercourses, and local character, as outlined in the character area profiles in [Section 3](#), including potential effects on key characteristics, valued features and sensitivities.
- respond to the character area design guidance provided in [Section 3](#).
- deliver a varied and interesting roofline in response to surrounding architectural styles, avoiding long monotonous blocks of development.
- Development within town centres should include active uses and frontages at ground level.
- incorporate an appropriate range of building heights and open spaces.

The criteria is based on the site observations of the team undertaking the study and are not intended to be exhaustive or detailed. All development proposals should make reference to the full suite of planning policy documentation available on the Council's website in addition to London Plan and national policy, a summary of which is provided in [Appendix D](#). All development proposals will need to show further detailed analysis at a specific site scale and should have sensitive consideration of the building's massing, form, style and materials.

### 4.6.4 Character area summaries and criteria

The following pages provide a more detailed map for the mid-rise building zones in each Place. This section should be read alongside the character area profiles provided in [Section 3](#).



**Fig. 228:** Mid-rise buildings opportunity map  
 Contains OS data © Crown copyright and database right 2021

# A Balham

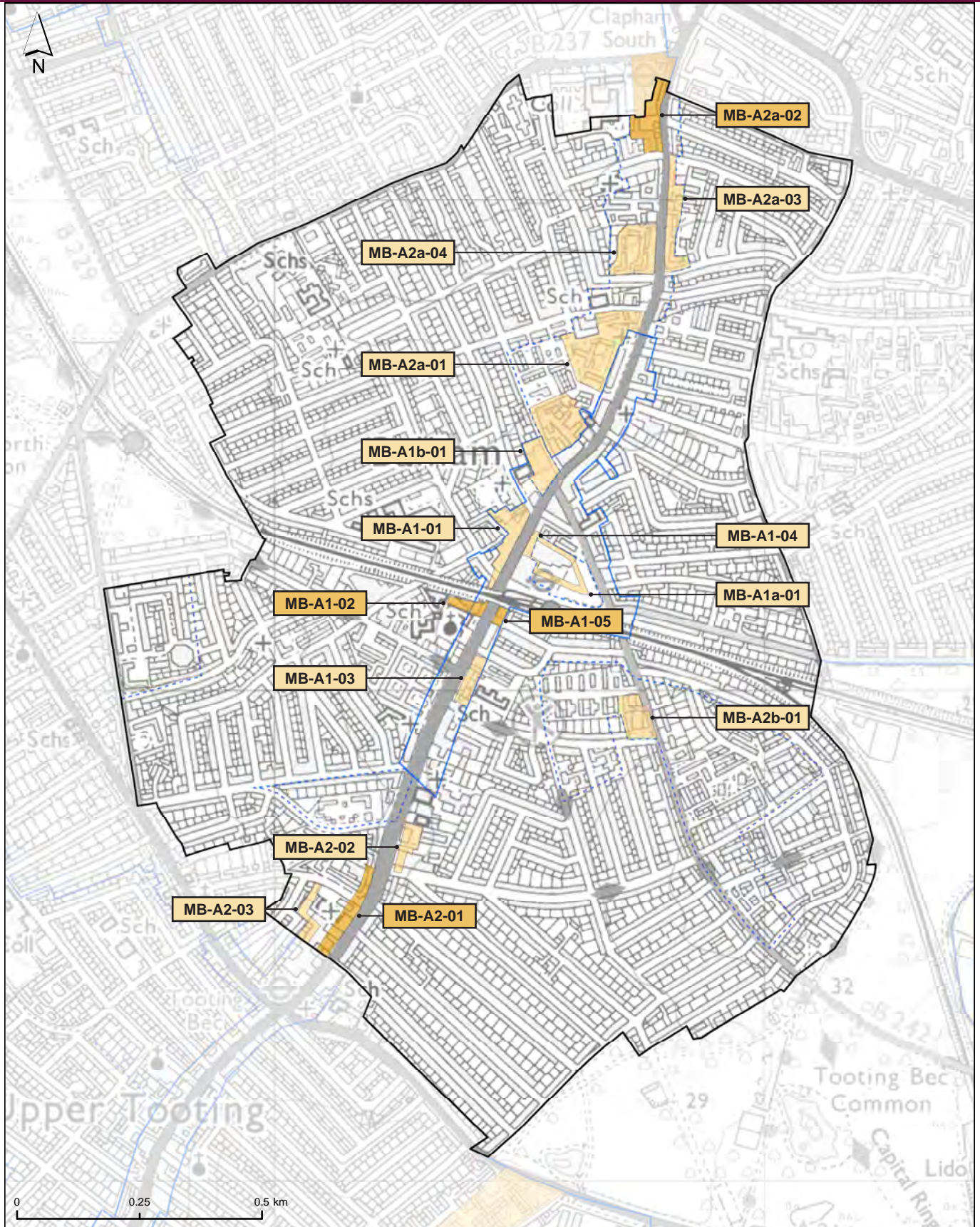


Fig. 229: Balham Mid-rise Buildings Zone map

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# B Battersea

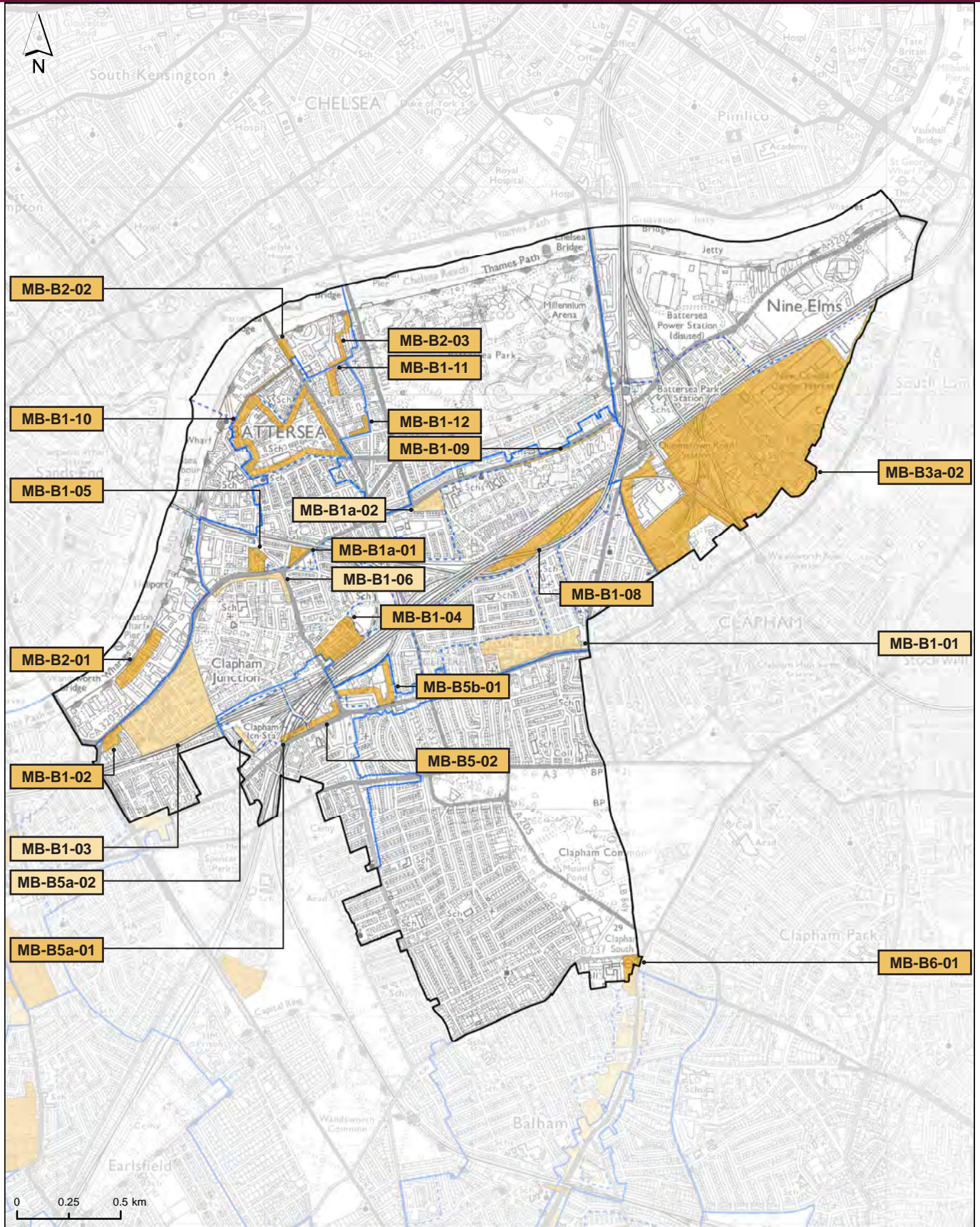


Fig. 230: Battersea Mid-rise Buildings Zone map

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# C Earlsfield and Southfields

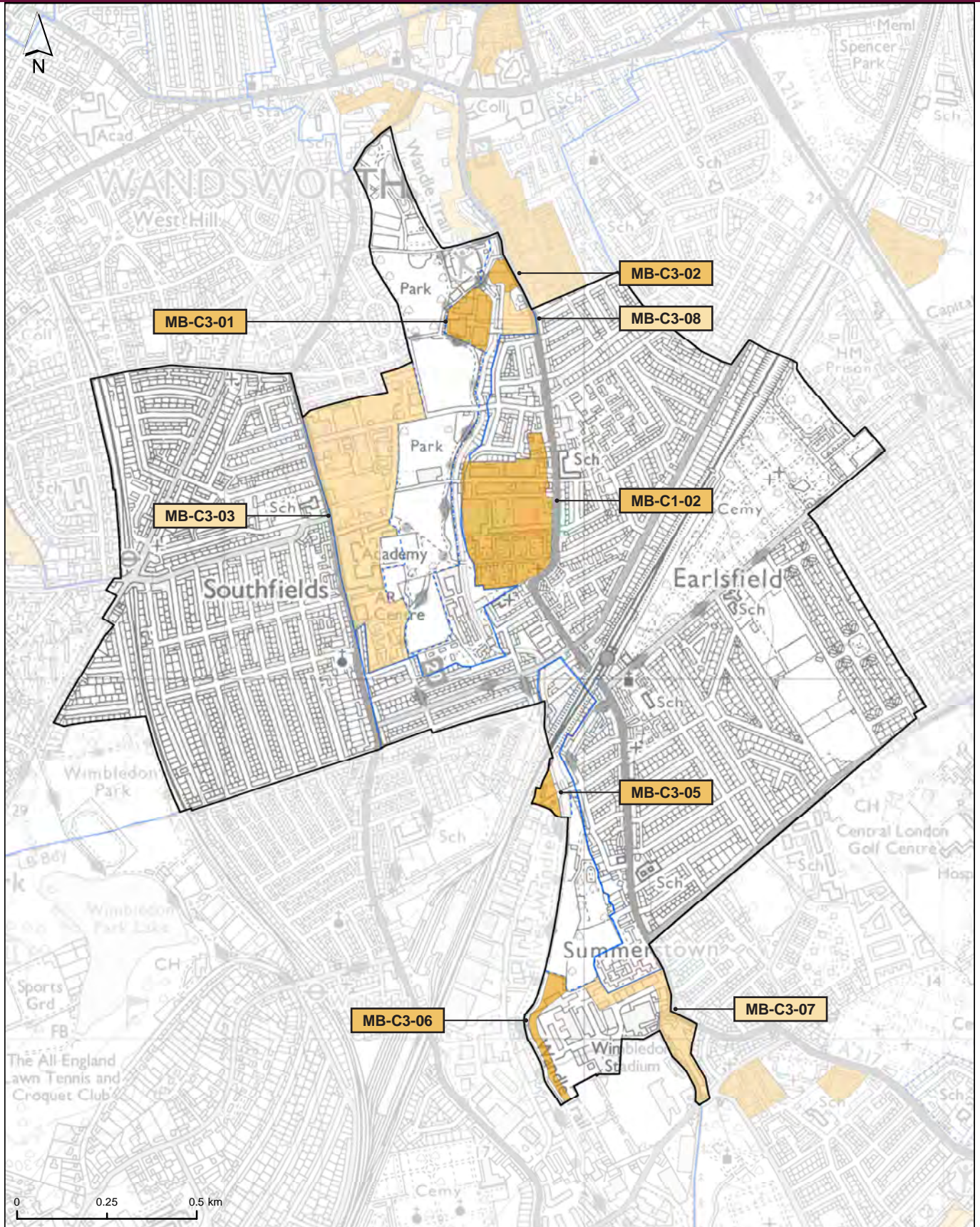


Fig. 231: Earlsfield and Southfields Mid-rise Buildings Zone map

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# D Putney

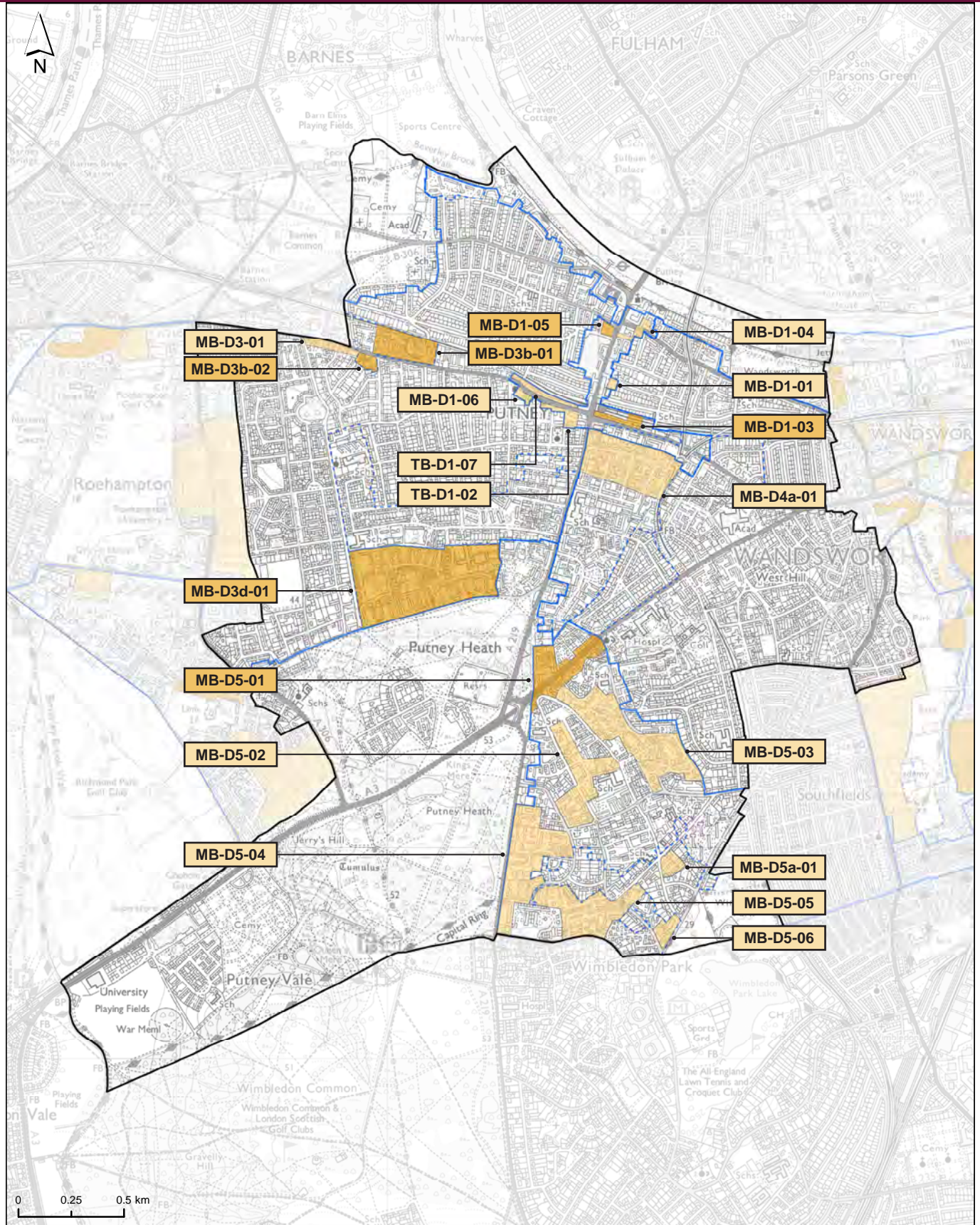
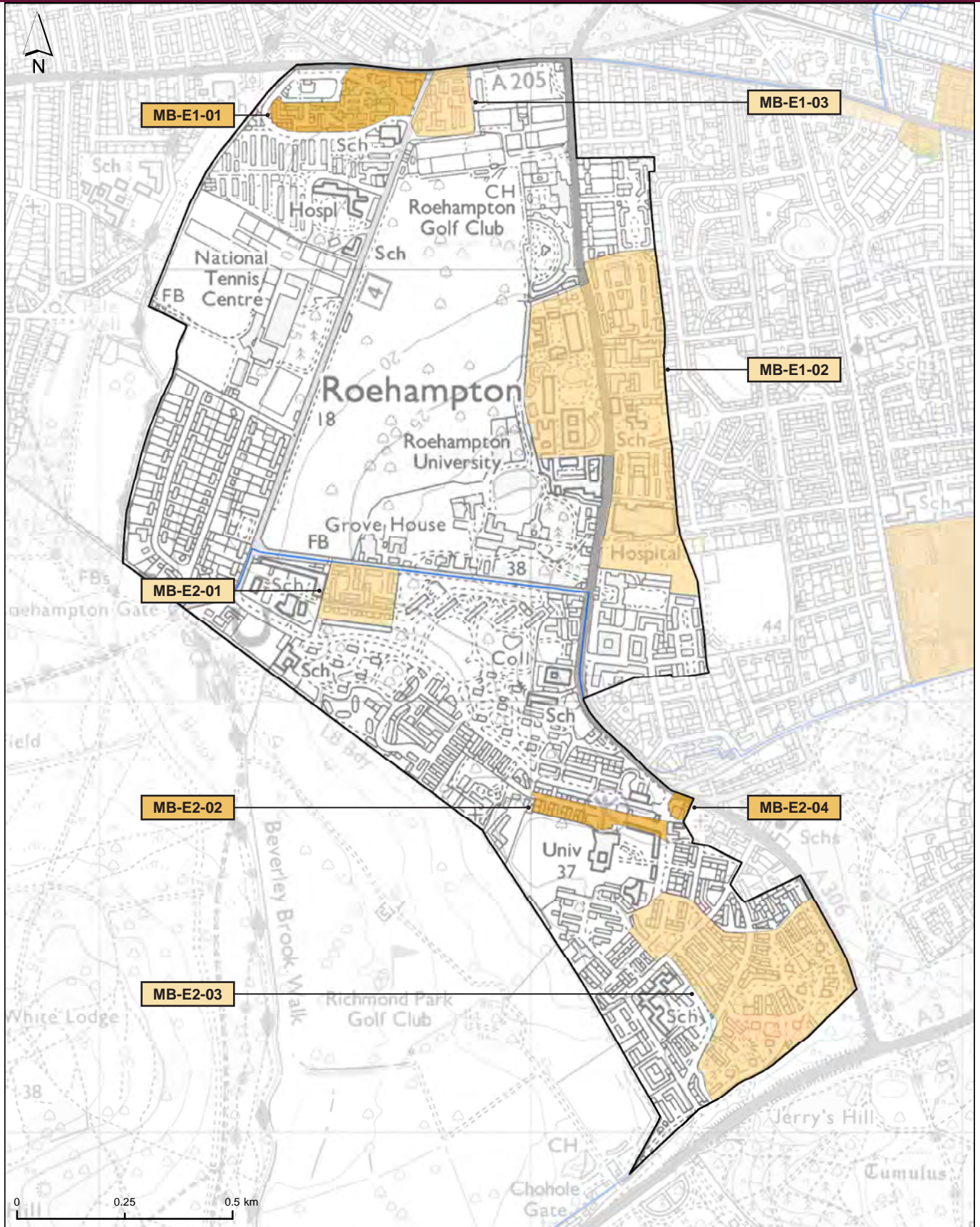


Fig. 232: Putney Mid-rise Buildings Zone map

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# E Roehampton



Character Area boundary  
 SubArea boundary

**Mid-rise building zones**  
 5 storeys (15m)  
  6 storeys (18m)

Fig. 233: Roehampton Mid-rise Buildings Zone map

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# F Tooting

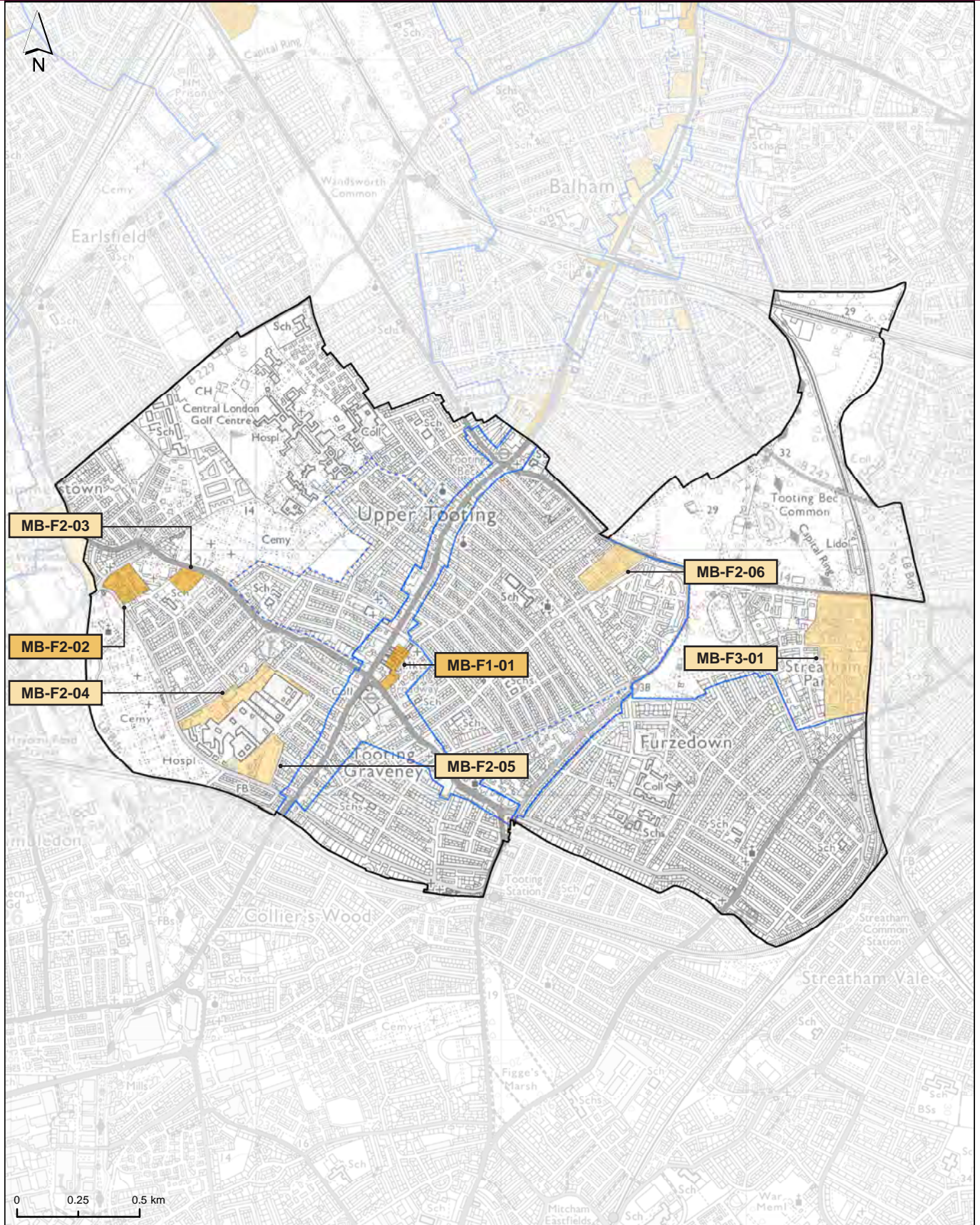


Fig. 234: Tooting Mid-rise Buildings Zone map

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# G Wandsworth Town and Common

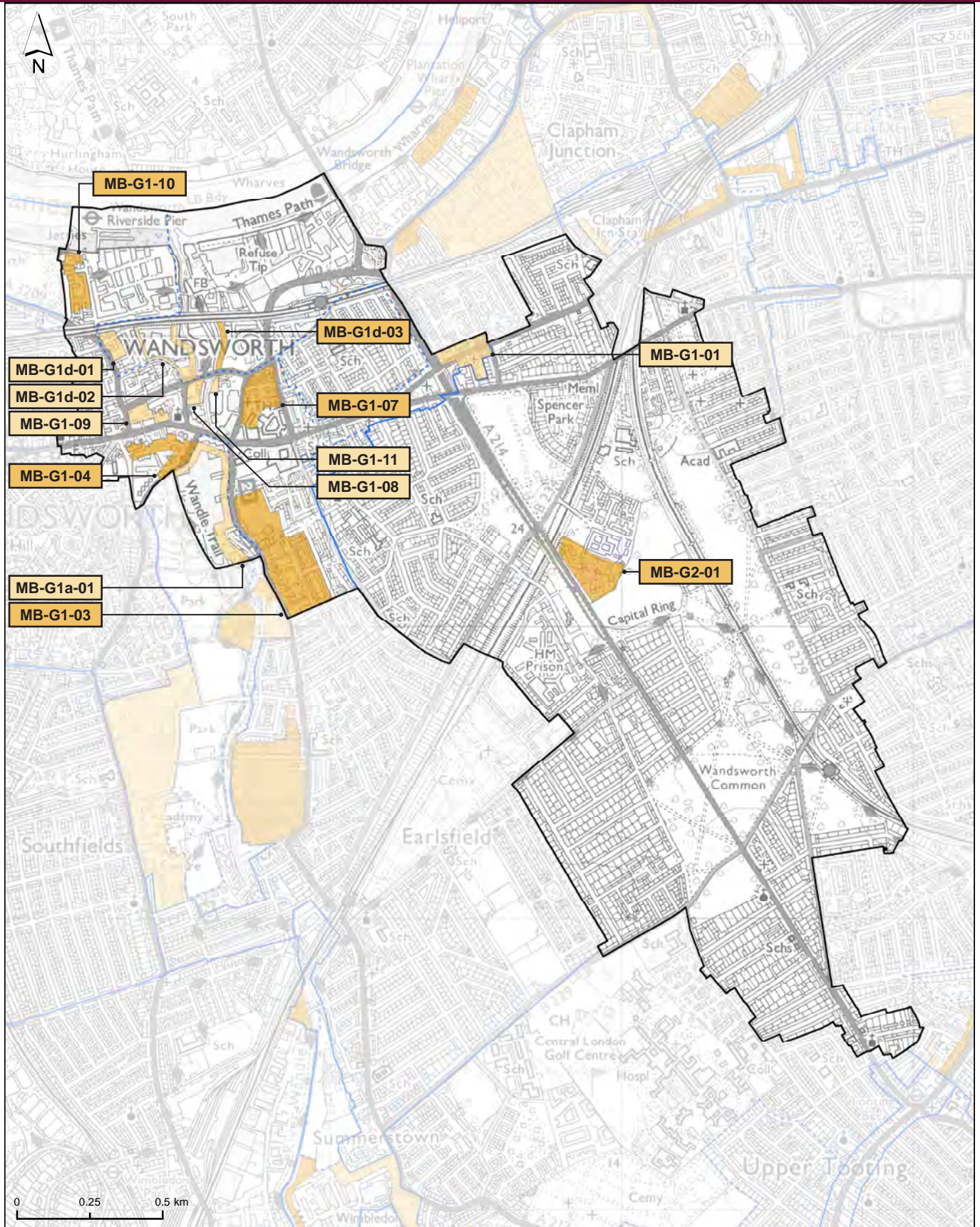


Fig. 235: Wandsworth Town and Common Mid-rise Buildings Zone map

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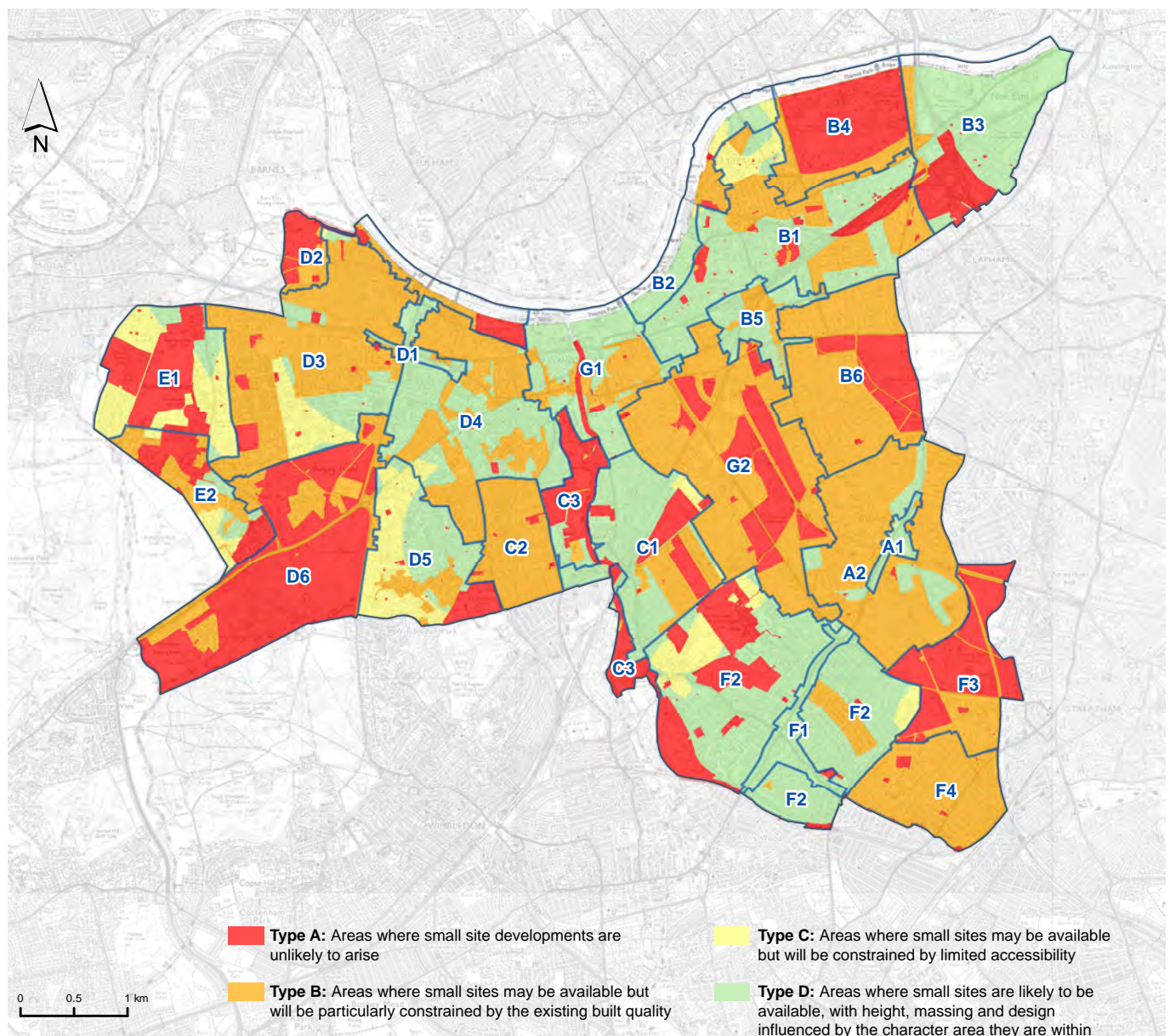
## 4.7 Small sites

Following on from the assessment of capacity for tall building development, the study has used the information gathered in this and characterisation sections, to provide a high level overview of opportunity for small sites across the borough. This is illustrated in the small sites opportunity map in [Fig. 236](#).

Module B of the draft Good Quality Homes for all Londoners SPG explores typical conditions which lend themselves to small site development. It provides guidance on preparing design codes to systematically

use intensification opportunities. Types of small housing developments defined in Module B are:

- **Street-facing conditions:** site with direct access to the street. The character of the existing street will inform the relationship between the proposed development and the surrounding buildings and guide considerations such as frontage line, front-to-front distances, building heights, rear projections and roof forms. Street-facing types include:
  - conversion of houses into flats
  - additions to the front or side of an existing property or within its curtilage
  - demolition and redevelopment



**Fig. 236:** Small sites opportunity map

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- redevelopment of flats and non-residential buildings
- **Backland conditions:** site behind development, commonly underused rear land and in some cases brownfield land. Such sites require a degree of innovation to respond to site constraints, to enable development. Consideration of access and servicing, and the inter-relationship between overlooking, privacy, aspect and daylight and sunlight, are paramount to the success and acceptability of new development in backland locations. There would be a presumption against development that would result in the loss of back gardens. Other considerations include building height, green cover, and roof form. Backland site types include:
  - backland infill development on vacant or underused sites
  - redevelopment of garage sites
  - infill development within the curtilage of a house

With the exception of public open spaces, there is potential for some form of small site development across different parts of the borough. In some areas, the realisation of a small site will be heavily constrained by the character of the area, and any development should refer to the profiles provided in Section 3. For example, within conservation areas, small site development would be likely to need to be in keeping with the surrounding architecture in terms of height, massing, materials and architectural quality. Therefore, in these areas, small sites would be unlikely to have the potential to increase density of housing in the borough, but may still fulfil some of the housing numbers required through development of empty or under-utilised plots. In these areas of higher sensitivity, including conservation areas, proposals which result in demolition and redevelopment of existing residential buildings will generally be resisted.

A similar principle applies in areas of the borough with a low PTAL score (e.g. Roehampton, Putney Heath, Furzedown Residential and areas to the south west of Wandsworth Common). In these areas, without further investment in public transport improvements, increased density of housing is unlikely to be suitable or achievable. Therefore, small sites development may not contribute to increased density in these areas, but again may still help fulfil housing numbers in vacant or very low density plots.

The remainder of the borough has potential for small sites development, with the density and housing numbers that may be achievable influenced by the

character of each area. The tall building opportunity map (Fig. 220) and mid-rise building opportunity map (Fig. 228) provide a good indication of where denser small site developments may be appropriate to bring forward, depending on their specific context.

The potential for small sites development is illustrated on Fig. 236. The map illustrates:

- **Type A:** areas where small site developments are unlikely to arise (existing open spaces);
- **Type B:** areas where small sites may be available but their development will be particularly constrained by the existing built quality (typically conservation areas);
- **Type C:** areas where small sites may be available but their density may be constrained by the low PTAL scores; and
- **Type D:** areas where small sites are likely to be available, and their height, massing and design will be influenced by the character area they sit within. Intensification, where in keeping with character, would be encouraged in areas with good public transport accessibility.

All development proposals should comply with Policy LP7 of the Draft Local Plan.

## 4.8 Policy recommendations

An overview of the policy recommendations in relation to character, tall buildings and small sites are set out below.

### 4.8.1 Character

Character and design is clearly an important part of what makes Wandsworth and its places special. In order to ensure that character is retained and reinforced, the local plan should:

- Include a general, criteria-based policy on the importance of design and character. This could be similar in scope to Policy IS 3 (Good quality design and townscape) in the current Core Strategy, if this is performing well. However, there would also be benefit in structuring a policy around the *Conserve / Restore / Improve / Transform* categorisations in the character area assessments to make distinction between considerations in different areas.
- Include a requirement for planning applications to consider and respond to the findings of this Urban Design Study. This could be within the general policy suggested above, or supporting text. This will be important to ensure the character area design guidance on each character area (in Section 3) are reflected in planning applications and permissions.
- Where there are area-based policies, these should include more detail of the character area design guidance. Refer to any relevant Conservation Area Appraisals and Management Strategies.
- Additional policies or supplementary planning guidance on particular elements of character and design should be considered, including: historic patterns, materials, movement & legibility and ground floor Shopfrontage design. These would be applicable to those areas where this has been flagged as important.

### 4.8.2 Tall buildings

A criteria-based tall buildings policy should be included to guide proposals for tall buildings across the borough, in accordance with London Plan Policy D9. This may make reference to the conclusions in Section 4.5 as well as criteria outlined in the tall buildings design guidance in [Appendix A](#).

The policy should set out that tall buildings are only acceptable in tall building zones and they will not be supported outside of the identified zones. Where a tall building is proposed in such areas, the policy should require outstanding design.

### 4.8.3 Mid-rise buildings

A criteria-based mid-rise buildings policy should be included to guide proposals for buildings across the borough which may appear tall within their local context. This may make reference to the conclusions in Section 4.6.

The policy should set out that mid-rise buildings are only acceptable in mid-rise building zones and they will not be supported outside of the identified zones. Where a mid-rise building is proposed in such areas, the policy should require outstanding design.

### 4.8.4 Small sites

Proposals for small sites should accord with the general, criteria-based policy on design and character suggested above. There would be value in a separate policy on small sites to cover the guidance provided in [Appendix B](#). This should also reference the need to optimise the small site density (balanced against character, design, policy and other material considerations), to make best use of available sites.







# Appendices

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# Appendix A

## Tall buildings

### A.1 Introduction

This section provides the supporting evidence and justification for the extents and appropriate heights of each of the tall building zones.

The broad areas identified in the tall buildings strategy (see [Fig. 218](#) in Section 4.4.3) have been analysed to understand whether there are individual zones within that have the potential to accommodate tall buildings. In some cases, the analysis of the characterisation notes reveals there is no potential for tall or mid-rise buildings. In other zones, while the potential to increase existing heights was identified, the existing constraints and valued features means they only have potential for buildings up to 5-6 storeys. These areas are shown in Section 4.6. For other zones, the analysis revealed potential for tall buildings over 7 storeys, and this analysis is summarised here.

The analysis includes a high level assessment of potential impacts on:

- townscape character, including relationship to existing landmarks and the River Thames;
- views and visual amenity, including long range views (particularly local or strategic views);
- heritage assets, including the setting of the Palace of Westminster and Westminster Abbey including Saint Margaret's Church World Heritage Site, Registered Parks and Gardens, Scheduled Monuments, conservation areas and listed buildings.

Tall building zones have been tested using analysis of:

- scenarios developed specifically for this Urban Design Study;
- consented tall buildings and/or masterplans; and
- existing tall buildings and how they contribute (positively, negatively or neutrally) to the existing character of an area.

A summary of the tall building zones is provided in [Table 2](#).

### *Tall building scenarios*

Ten sites were selected in consultation with Wandsworth Council and are intended to give a good representation and coverage of the different types and locations of likely development areas where analysis of existing buildings and consented schemes were not sufficient alone.

The scenarios developed are prepared solely for the purpose of testing additional height and density at a site and are not intended to be viable site specific masterplan proposals. In all cases, further analysis will be required to determine actual proposals for individual sites on the basis of detailed review and analysis of the specific local context which is not part of the scope of this borough-wide study. Where scenarios are noted as broadly appropriate in principle this represents the opinion of the writer and not of the Council, with further analysis by future developers required. The development of the scenarios follows the methodology set out in [Appendix C](#), but can be summarised as:

- identification of the relevant density for each site based on the SRQ matrix and PTAL rating;
- prepare the massing and test within the Vu.City software; and
- update the massing where necessary following the analysis.

The following scenarios have been prepared, illustrated on the following pages:

1. Southside Shopping Centre;
2. Asda, Lidl and Boots sites, Falcon Lane;
3. Clapham Junction Station Approach;
4. St George's Hospital car park;
5. Putney High Street cluster;
6. Markets area, Tooting High Street;
7. Riverside cluster;
8. Sainsbury's car park, Bedford Hill; and
9. Battersea Design and Tech Quarter.

Zone	Place	Character area	Appropriate height range (storeys)	Appropriate height range (m)	Justification
TB-A1a-01	Balham	Balham Town Centre	7 to 8	21 to 24	Analysis of scenario
TB-B1-01	Battersea	Battersea Residential	7 to 20	21 to 60	Analysis of existing buildings and consented masterplan
TB-B1-03	Battersea	Battersea Residential	7 to 20	21 to 60	Analysis of scenario
TB-B1-04	Battersea	Battersea Residential	7 to 12	21 to 36	Analysis of existing buildings
TB-B1-05	Battersea	Battersea Residential	7 to 8	21 to 24	Analysis of existing buildings
TB-B1-06	Battersea	Battersea Residential	7 to 8	21 to 24	Analysis of existing buildings
TB-B2-02	Battersea	Battersea Riverside	7 to 10	21 to 30	Analysis of scenario
TB-B2-03	Battersea	Battersea Riverside	7 to 12	21 to 36	Analysis of existing buildings
TB-B2-04	Battersea	Battersea Riverside	7 to 12	21 to 36	Analysis of existing buildings
TB-B2-05	Battersea	Battersea Riverside	7 to 10	21 to 30	Analysis of scenario
TB-B2-06	Battersea	Battersea Riverside	7 to 10	21 to 30	Analysis of scenario
TB-B3-01	Battersea	Nine Elms Mixed Use	10 to 25	30 to 75	Analysis of existing and consented buildings
TB-B3a-01	Battersea	Nine Elms Mixed Use	7 to 13	21 to 39	Analysis of scenario
TB-B3a-02	Battersea	Nine Elms Mixed Use	7 to 11	21 to 33	Analysis of scenario
TB-B3a-03	Battersea	Nine Elms Mixed Use	7 to 10	21 to 30	Analysis of consented buildings
TB-B5-01	Battersea	Clapham Junction Town Centre	7 to 15	21 to 45	Analysis of scenario
TB-B5-02	Battersea	Clapham Junction Town Centre	7 to 12	21 to 36	Analysis of consented buildings
TB-B5a-01	Battersea	Clapham Junction Town Centre	7 to 15	21 to 45	Analysis of scenario
TB-B5b-01	Battersea	Clapham Junction Town Centre	7 to 15	21 to 45	Analysis of scenario
TB-C3-01	Earlsfield and Southfields	Wandle Valley	7 to 10	21 to 30	Analysis of existing buildings under construction
TB-D1-01	Putney	Putney Town Centre	7 to 8	21 to 24	Analysis of scenario
TB-D1a-01	Putney	Putney Town Centre	7 to 11	21 to 33	Analysis of existing buildings
TB-E2-01	Roehampton	Roehampton Residential Estates	7 to 9	21 to 27	Analysis of consented masterplan
TB-F2-01	Tooting	Tooting Residential	7 to 8	21 to 24	Analysis of scenario
TB-G1-03	Wandsworth Town and Common	Wandsworth Town and Riverside	7 to 10	21 to 30	Analysis of existing buildings
TB-G1a-01	Wandsworth Town and Common	Wandsworth Town and Riverside	7 to 20	21 to 60	Analysis of scenario
TB-G1d-01	Wandsworth Town and Common	Wandsworth Town and Riverside	7 to 10	21 to 30	Analysis of consented masterplan
TB-G1d-02	Wandsworth Town and Common	Wandsworth Town and Riverside	7 to 10	21 to 30	Analysis of consented masterplan
TB-G1d-03	Wandsworth Town and Common	Wandsworth Town and Riverside	7 to 15	21 to 45	Analysis of existing buildings and consented masterplan

**Table 2 Overview of tall building zones**

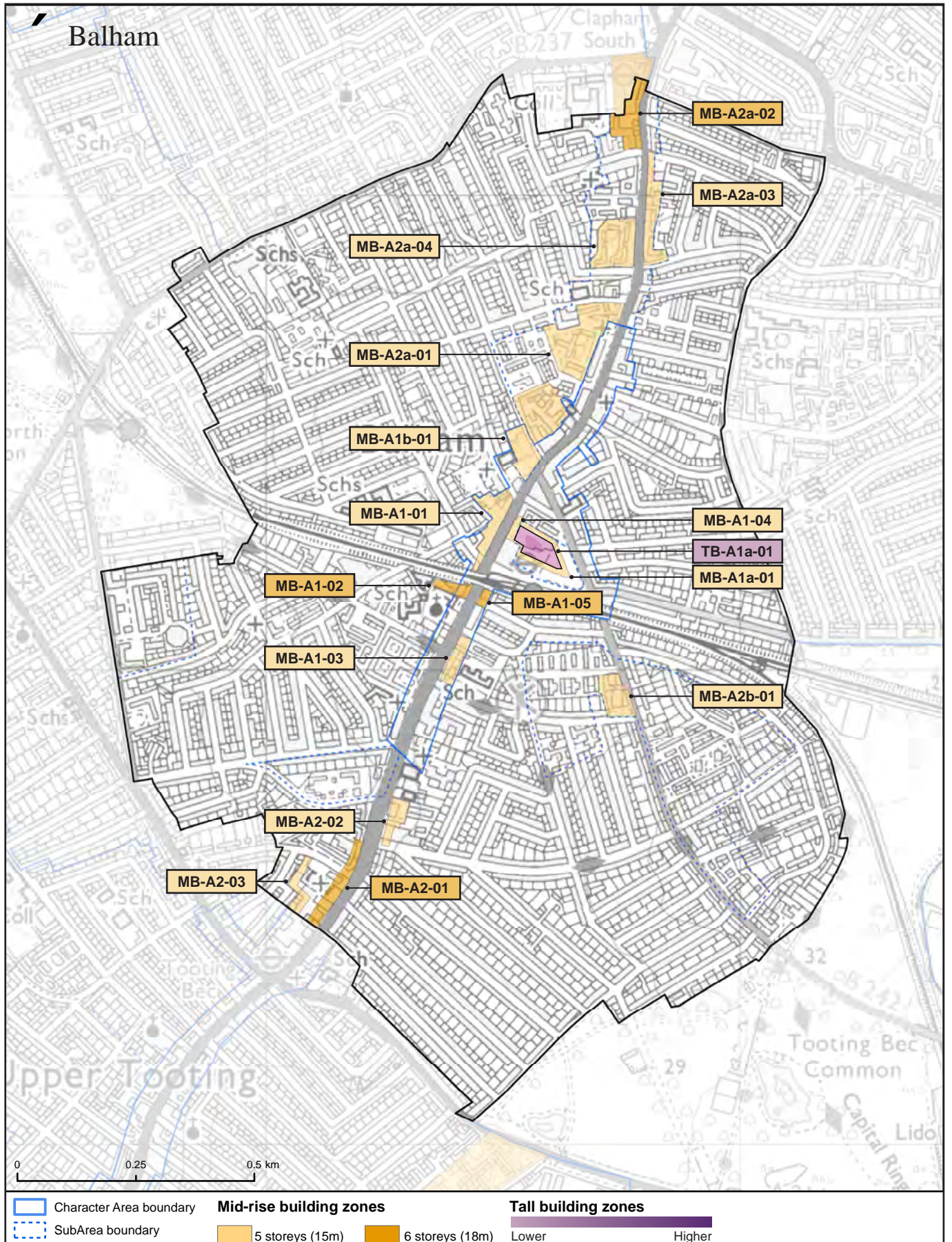


Fig. 237: Balham combined Tall and Mid-rise Buildings Zone map

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## A.2 Balham justification

### A.2.1 TB-A1a-01: Balham Town Centre sub-area A1a

Existing prevailing height: 3 storeys

Appropriate height: 7-8 storeys (21-24m)

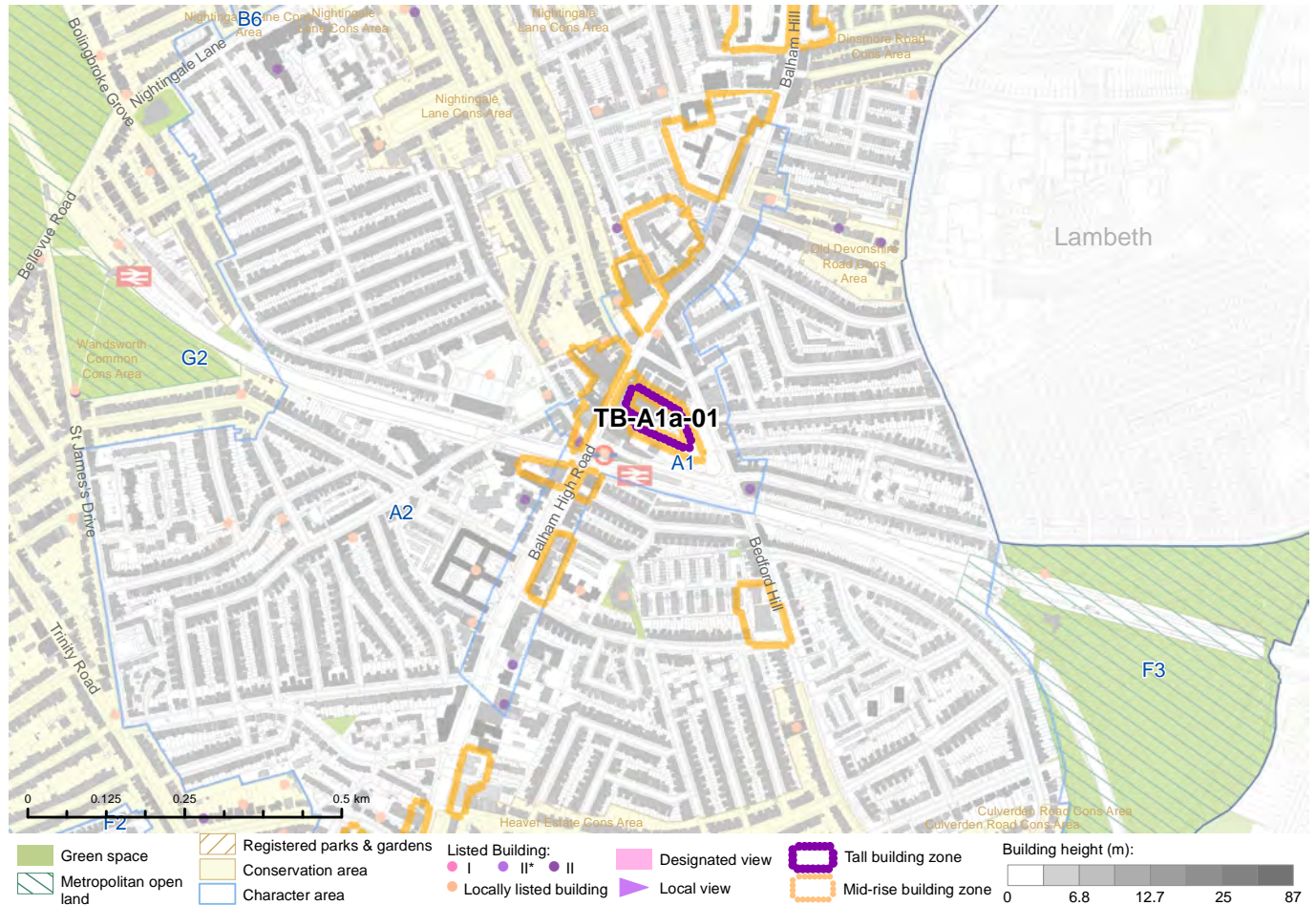


Fig. 240: TB-A1a-01 context map

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Fig. 238: Consistent urban grain on Bedford Hill, the Sainsbury's site to the left



Fig. 239: Existing car park of the Sainsbury's site

<b>TB-A1a-01</b>	
<b>Existing use</b>	Car Park
<b>Existing/consented tall buildings</b>	• None
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Town Centre: Balham</li> <li>• Area Strategy: Balham</li> <li>• Site Allocations: BA1 Sainsbury's Car Park</li> </ul>
<b>Designations (within 50m)</b>	None
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Balham Station (Grade II)</li> <li>• 4 locally listed buildings near Balham High Road</li> <li>• Nightingale Lane Conservation Area</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Nightingale Lane Conservation Area (A2).</li> <li>• Old Devonshire Road Conservation Area (A2).</li> <li>• Heaver Estate Conservation Area (A2).</li> </ul>
<b>PTAL rating</b>	Excellent (6a)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	A1 Balham Town Centre - (Conserve/Restore)
<b>Other character areas within 250m</b>	• A2 Balham Residential
<b>Sensitivity</b>	Low
<b>Probability</b>	High/Very high
<b>Capacity</b>	High/Higher
<b>Sainsbury's Car Park, Bedford Hill Scenario</b>	
<b>Site Area</b>	0.63 ha
<b>Total residential units</b>	130
<b>Non Residential GEA</b>	4,500 sqm
<b>Density (dph)</b>	206



Fig. 242: Sainsbury's Car Park, Bedford Hill scenario plan

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Fig. 243: Sainsbury's Car Park, Bedford Hill scenario massing model

### Justification: Scenario

A scenario for the Sainsbury's Car Park site has been developed to test the appropriate building height of tall building zone TB-A1a-01. The character profile highlights the car park as a negative feature in a key location with a relatively lower sensitivity to change. The character profile strategy for Balham Town Centre is to conserve and restore, noting that it has a good sense of place though its character is affected by negative elements in key locations - notably the supermarket car parks. Design guidance states that considered development on the site could help to restore and enhance the character of Balham Town Centre.

The scenario has been developed in line with the following principles: taller elements located close to the existing Sainsbury's building; reduction in building heights towards existing low-rise development along Bedford Hill; and existing surface parking to be re-provided in a ground floor podium.

### Extent of visibility

The ZTV indicates that visibility of the scenario is likely to be relatively contained to the residential streets surrounding the site, within A1 Balham Town Centre and A2 Balham Residential character areas. Visibility will extend along straight roads orientated towards the site, notably Station Road, Bedford Hill, Fernlea, Rossiter and Sistova Roads to the east of the site. Whilst the ZTV shows some visibility of the upper parts of the scenario from further away, in reality it is not likely to be readily perceptible in views from Tooting Bec Common and Wandsworth Common due to intervening trees and buildings.



Fig. 241: ZTV of Sainsbury's Car Park, Bedford Hill scenario

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## *High level townscape, visual and heritage assessment*

### **Baseline**

The following valued features may be affected:

#### **A1 Balham Town Centre:**

- Intact Victorian and Edwardian 3-4 storey terraces lining the majority of Balham High Road with well preserved and maintained architectural details. The buildings create a distinct recognisable proportion to the overall street scene.
- The setting of, and distinctive character provided by the Bedford Hotel (grade II listed) landmark building at the junction of Bedford Hill/Balham Station Road.
- The setting of the Bedford Hotel (Grade II).

#### **A2 Balham Residential:**

- Locally distinctive and coherent period houses forming coherent streets.
- Views along well-balanced and tree-lined streets.

The following townscape features/characteristics/views may be affected:

- The positive townscape character created by the consistent building height, roofline and grain along Bedford Hill including the relatively modest scale of the Bedford pub landmark and Bedford Hill itself.
- The pedestrian environment and streetscape character of Station Road opposite the railway wall.
- Views from the elevated railway line, which is how many people passing through the area experience and appreciate Balham.
- Views from the junction to the south east, incorporating the Bedford Hotel.
- Views along Station Road.
- Views from the junction of Balham High Road/ Balham Station Road from which the upper parts of the scenario would be visible behind existing Shopfront terraces.

### **Assessment**

Along Bedford Hill and the junction with Station Road, modelling of taller building scenarios on the site showed that built form taller than 4 storeys would be likely to result in unacceptable effects on townscape and views, in particular because of the relationship with the existing urban grain and scale of the street, and the modest landmark of the Bedford pub at the junction. Therefore, the scenario illustrates 4 storeys/12m in this area which is considered appropriate in principle. The area closest to the road has therefore been excluded

from both the tall building zone and mid-rise building zone, and the tall building zone set back behind a mid-rise zone of 6 storeys/18m. This allows taller buildings to step down to respond to the surrounding context.

Along Balham Station Road, modelling of taller scenarios on the site showed that built form taller than 4-6 storeys would be likely to result in unacceptable effects on townscape and views, in particular because of the tunnelling effect with the railway retaining wall on the opposite side, creating a perception of little space for people or perception of green. Therefore, the scenario illustrates a mixture of 4-6 storeys in this area which is considered appropriate in principle, set back from the road behind an area of public realm.

The scenario shows building heights stepping up to 8 storeys/24m in a small part of the site, which is considered appropriate in principle, and does not sit uncomfortably with the modest scale of the surrounding buildings. However, the relationship with the adjacent 3 storey buildings to the west would need further design consideration.

The scenario is considered not to adversely affect views from the railway, assuming good design principles, details, materials and quality ensure its frontages respond positively in all directions, enhancing overall identity and sense of place in these views. Views along well-balanced and tree-lined streets punctuated by landmarks.

None of the valued features, views or key features would be adversely affected by developments of up to 7-8 storeys if well designed and planned. Buildings of 7-8 storeys is considered to be the absolute maximum this site could accommodate and this would depend entirely on a high quality design that is carefully placed within the plot in response to all the surrounding constraints and considerations.

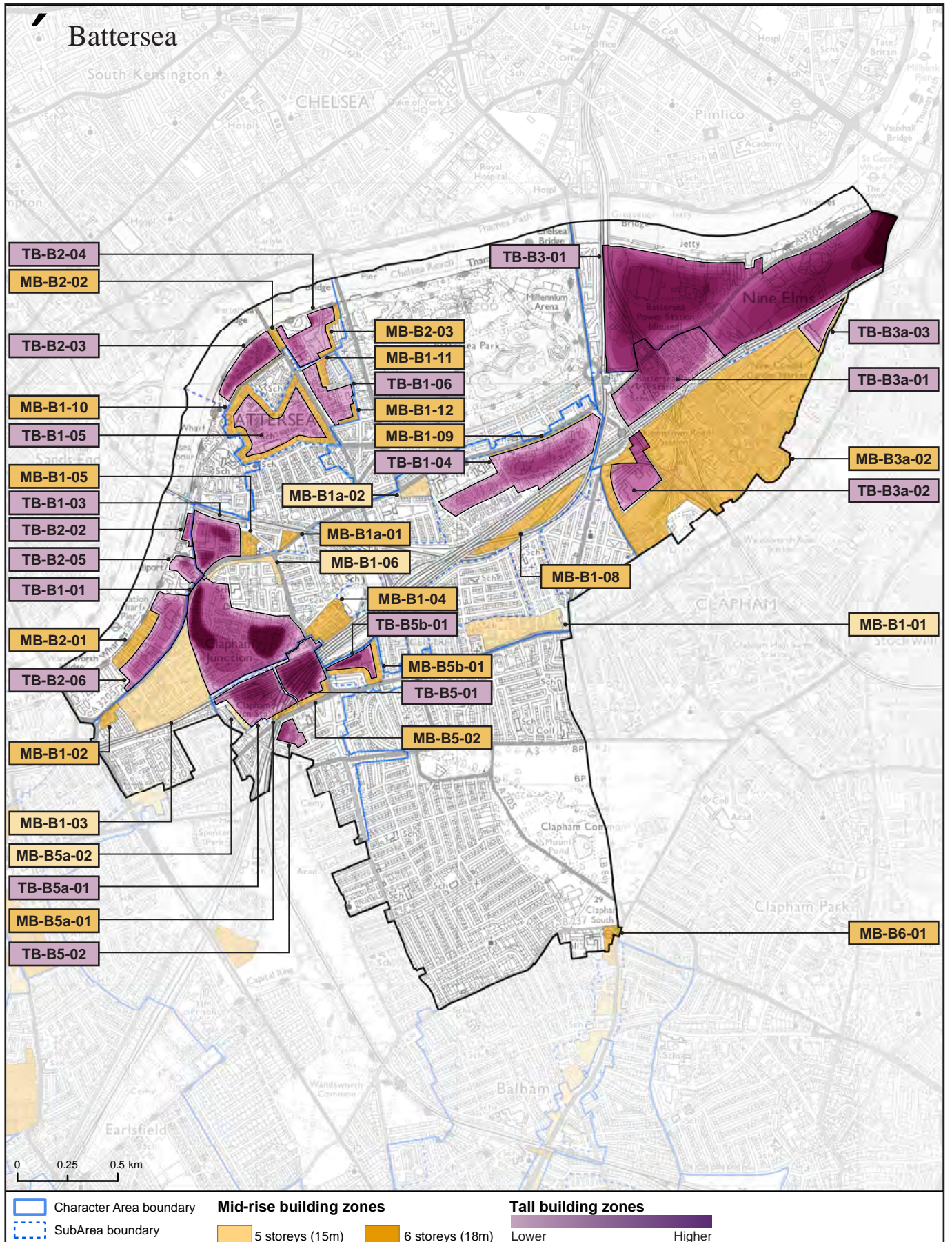


Fig. 244: Battersea combined Tall and Mid-rise Buildings Zone map

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### A.3 Battersea justification

#### A.3.1 TB-B1-01: Battersea Residential

Existing prevailing height: 3-23 storeys

Appropriate height: 7-20 storeys (21-60m)

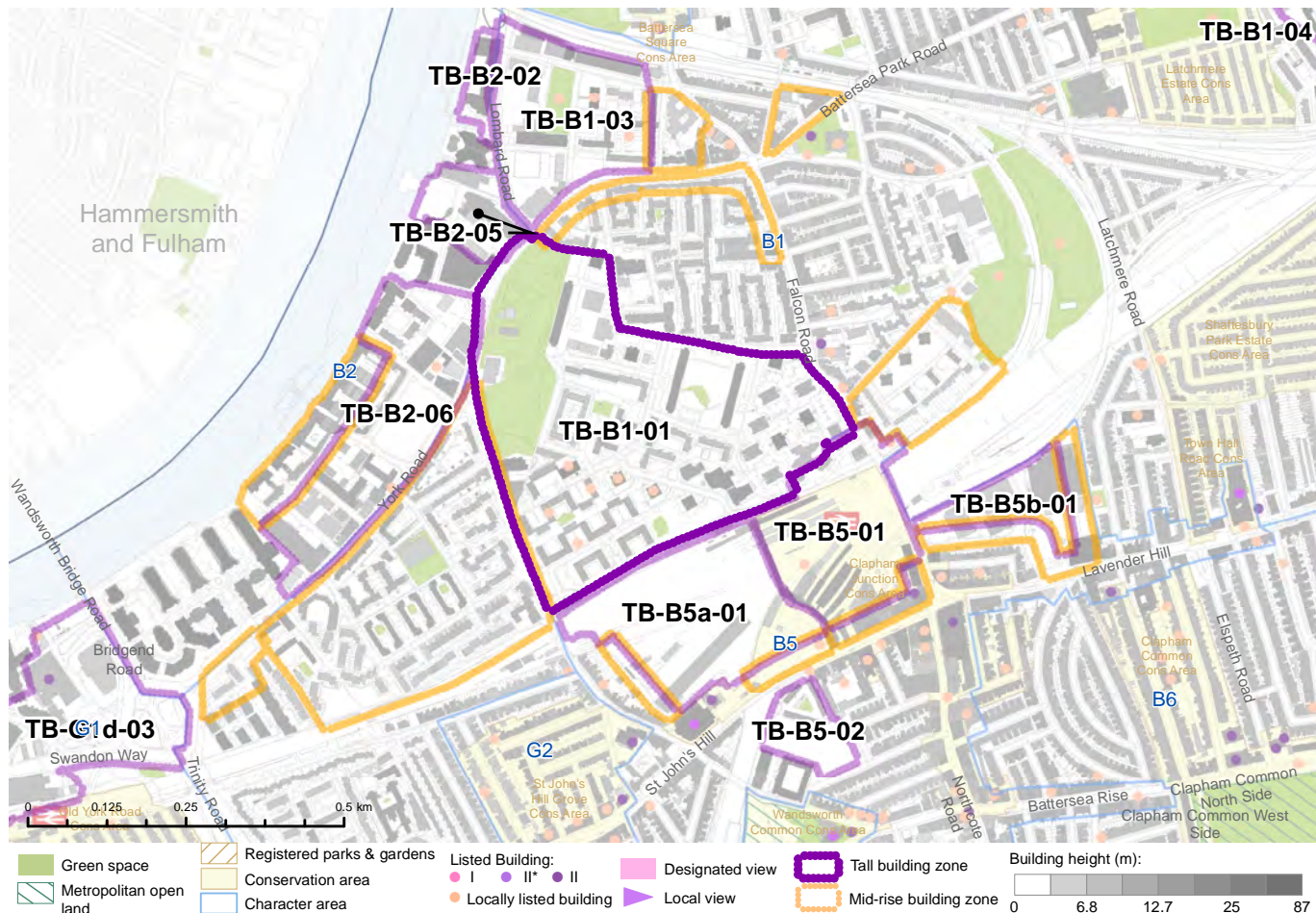


Fig. 245: TB-B1-01 context map

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Fig. 246: 3 and 9 storey buildings on Darien Road



Fig. 247: Buildings including St Peter's Church on Plough road along the western boundary

<b>TB-B1-01</b>	
<b>Existing use</b>	Residential area including estates and open spaces
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>• Post war blocks and towers within the York and Winstanley estate (existing). Approximately 31m (around 10 storeys), 45m (around 15 storeys), 64m (around 21 storeys), and a recently built tower of approximately 66m (22 storeys).</li> <li>• Multiple buildings within the Winstanley and York Road regeneration scheme. The consented development comprises blocks of up to 44m, 69m and a tower of up to approximately 108m (over 30 storeys) (under construction).</li> <li>• Existing St Peter with St Paul Church, 8 storeys with consent for additional 2 storeys (consented).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Area Strategy for Clapham Junction and York Road/Winstanley Regeneration Area.</li> <li>• Site allocations: CJ3 Land on the corner of Grant Road and Falcon Road; CJ5 Winstanley/York Road Regeneration Area.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• 7 locally listed buildings within the zone.</li> <li>• Clapham Junction Conservation Area.</li> <li>• Listed building on Falcon Road (Grade II).</li> <li>• Protected open space at York Gardens.</li> <li>• 4 locally listed buildings within 50m (at the riverside, Falcon Road and Clapham Junction Railway Station).</li> <li>• Locally listed garden at Falcon Road estate.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Locally listed building at Falcon Grove</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• St John's Hill Grove Conservation Area (G2).</li> <li>• Wandsworth Common Conservation Area (G2).</li> <li>• Battersea Square Conservation Area (B2).</li> <li>• Sands End Conservation Area (LB Hammersmith and Fulham).</li> </ul>
<b>PTAL rating</b>	Excellent (6b)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B1 Battersea Residential - (Improve)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• B2 Battersea Riverside</li> <li>• B5 Clapham Junction Town Centre</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Very high (and Low relating to the existing York Gardens open space)
<b>Capacity</b>	Higher (and Low relating to the existing York Gardens open space)

### *Justification: Existing buildings and consented masterplan*

This analysis tests the appropriate building height of tall building zone TB-B1-01, using the existing buildings within the York and Winstanley estates, and the consented masterplan development (see table above for associated heights).

The character profile notes some negative qualities within this area including the appearance of St Peter's Church, buildings with low density and lack of address to the street south of York Road, lack of sense of place within the public estates, few architectural features, inward-looking buildings and lack of wider connectivity. It also notes a perceived lack of safety within the estates. Existing towers contribute little to

legibility in the area. York Gardens is noted as high sensitivity as a public green space, relatively rare in the area. The strategy is to improve the character of the area which is currently fragmented. The design guidance states that new design should incorporate focal points, a movement strategy and address poor urban design elements of the existing estates. New distinctive landmarks should have design integrity, improve legibility and be focussed along main roads and at key junctions. Surrounding public realm should be proportionately sized so the height does not overwhelm the human scale.



**Fig.248:** ZTV of the consented York and Winstanley masterplan development  
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**Fig.249:** ZTV of existing tower in Winstanley estate  
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### *Analysis*

Evidence gathered through site visits and the character area profile indicate that the heights of the existing buildings in the zone do not negatively impact the character of the area (although their layout and other urban design characteristics do). They are set

within large plots, integrated with open spaces and the surroundings, to some extent, are softened by tree planting though green spaces could be further enhanced.

Sensitive/valued features which may be affected by tall buildings include: York Gardens (which is being reconfigured as part of the consented masterplan), and the overall sense of low rise housing and quiet tree-lined streets which characterise the wider area (which is not represented in the existing character of the zone).

Whilst the consented York and Winstanley development rises to over 30 storeys, it is considered that the zone does not have further capacity for buildings up to this height owing to the smaller scale surroundings and sensitivities of the nearby Clapham Junction Conservation Area.

The zone has capacity for further tall buildings up to 20 storeys, within the south east of the zone, and the north west of the zone, as long as they are set in large plots and generally located towards the railway corridor. The tallest buildings should generally be located away from the smaller-scale character to the north east of the zone, with 2-4 storey terraced properties.

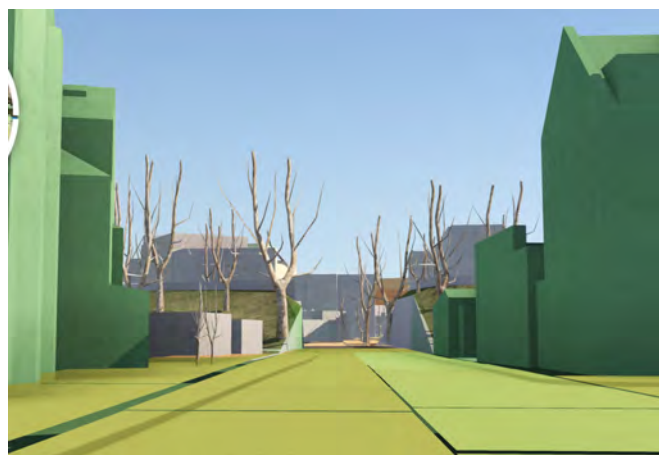


**Fig. 251:** Existing and consented tall buildings within the zone (lower building heights masked out in light blue)

The following views illustrate that there is limited potential for further development of the tallest buildings within the consented masterplan due to the potential impact on surrounding conservation areas, including those on the opposite side of the River Thames. However, development up to 20 storeys would generally be appropriate.



**Fig. 250:** From Clapham Junction Railway Station within Clapham Junction Conservation Area.



**Fig. 253:** Limited visibility from the Battersea Square Conservation Area



**Fig. 254:** View from Sands End Conservation Area in LB Hammersmith and Fulham. Existing and consented buildings form a cluster that steps down to the right (south west of the zone).



**Fig. 252:** View north across the open space at Kambala Road/Mantua Street. This area will be densely occupied by tall buildings, with residual capacity for buildings of 7-8 storeys only in this area.

### A.3.2 TB-B1-03: Battersea Residential

Existing prevailing height: 2-20+ storeys

Appropriate height: 7-20 storeys (21-60m)

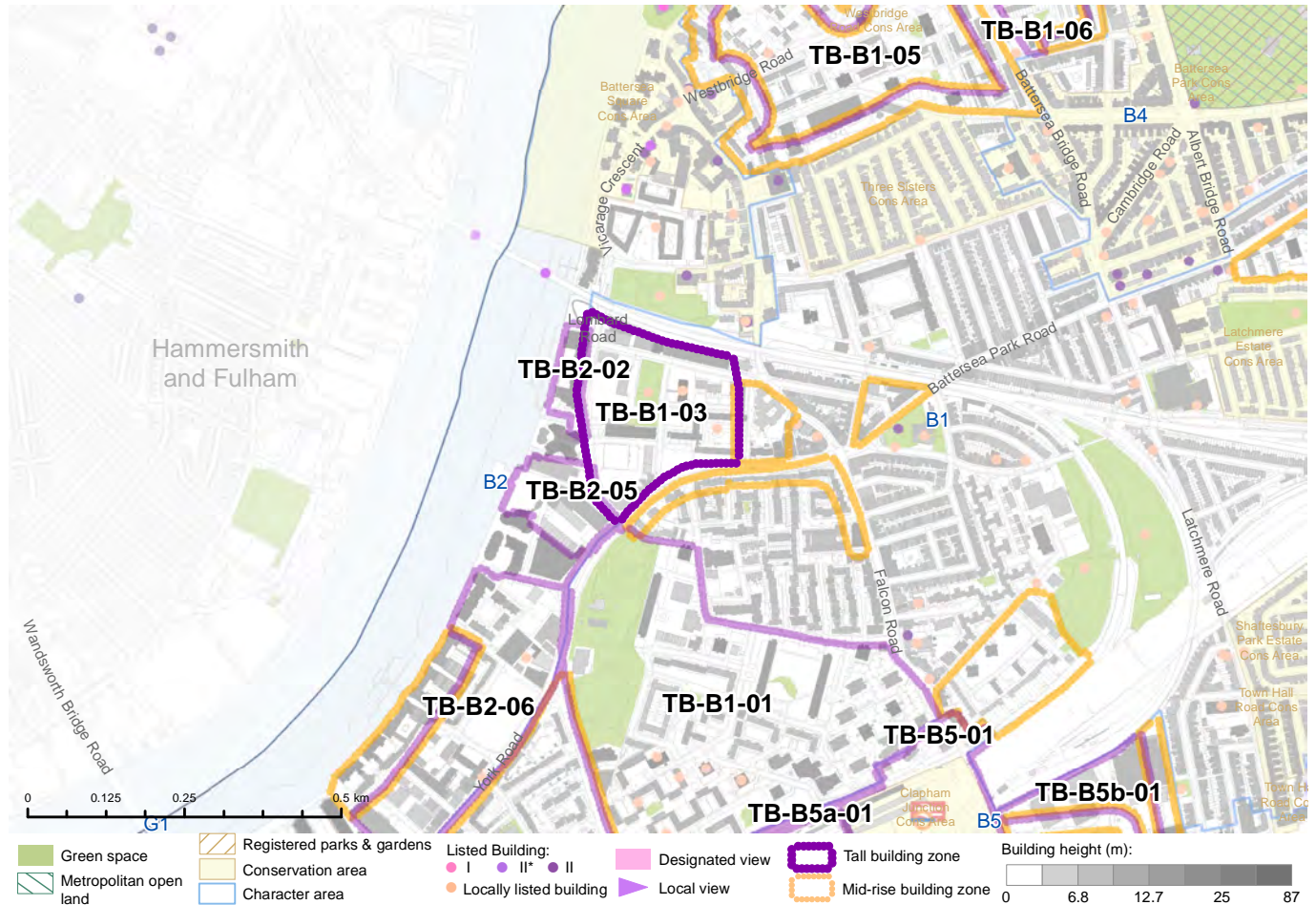


Fig. 255: TB-B1-03 context map

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Fig. 256: 24 storey Totteridge House and blocks on Yelverton Road



Fig. 257: View along Harroway Road, with Harroway Gardens to the left

TB-B1-03	
<b>Site Area</b>	0.3 ha 19 Lombard Road, 80 Gwynne Road 0.6 ha 37 Lombard Road (Travis Perkins) 0.3 ha 36 Lombard Road 0.7 ha Dovercourt Site
<b>Existing use</b>	Industrial and residential estate
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>• 56-66 Gwynne Road (existing).</li> <li>• Totteridge House (24 storey tower) and 7 storey block (existing).</li> <li>• York Road Business Centre, 55 Lombard Road (under construction).</li> <li>• 2-18 Yelverton Road (consented).</li> <li>• 58-70 York Road (under construction).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Site allocations: RIV5 York Road Business Centre; RIV7 Travis Perkins, 37 Lombard Road; RIV8 19 Lombard Road, 80 Gwynne Road; RIV9 The Chopper PH, 58-70 York Road.</li> <li>• Area Strategy: Wandsworth Riverside.</li> <li>• Focal Point : Lombard Road/York Road Riverside.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Harroway Road open space (locally listed), within the zone.</li> <li>• Fred Wells Gardens (locally listed).</li> <li>• Locally listed buildings within the zone including Totteridge House and Badric Court.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Battersea Square Conservation Area.</li> <li>• Katherine Low Settlement (grade II listed building) and locally listed.</li> <li>• Candlemaker public house (locally listed).</li> <li>• York Gardens protected open space.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Three Sisters Conservation Area (B2).</li> <li>• Church of St Mary (grade I) (B2).</li> </ul>
<b>PTAL rating</b>	Low to high (2-6a)
<b>Urban design study</b>	
<b>Character areas - (and strategy)</b>	B1 Battersea Residential - (Improve)
<b>Other character areas within 250m</b>	B2 Battersea Riverside
<b>Sensitivity</b>	Low
<b>Probability</b>	Medium/High (low for the open space)
<b>Capacity</b>	High
<b>Riverside Cluster Scenario</b>	
<b>Total residential units</b>	73 (19 Lombard Road, 80 Gwynne Road) 218 (37 Lombard Road (Travis Perkins)) 95 (36 Lombard Road) 188 (Dovercourt Site)
<b>Non Residential GEA</b>	500 sqm (19 Lombard Road, 80 Gwynne Road) 5,500 sqm (37 Lombard Road (Travis Perkins)) 1,800 sqm (36 Lombard Road) 8,200 sqm (Dovercourt Site)
<b>Density (dph)</b>	243 (19 Lombard Road, 80 Gwynne Road) 363 (37 Lombard Road (Travis Perkins)) 316 (36 Lombard Road) 268 (Dovercourt Site)

### Justification: Scenario

A scenario for a cluster of development plots, including two within this zone along Lombard Road, has been developed to test the appropriate building height of tall building zone TB-B1-03. The character profile notes some negative qualities within this area including

the scale of the 24 storey Totteridge House and the quality of surrounding blocks along Yelverton Road. The strategy is to improve the character of the area which is currently fragmented. The design guidance states that new design should incorporate focal points, a movement strategy and address poor urban design elements of the existing estates. New distinctive landmarks should have design integrity, improve legibility and be focussed along main roads and at key junctions. Surrounding public realm should be proportionately sized so the height does not overwhelm the human scale.

The scenario has been developed to place taller building elements along main roads and existing open spaces and to re-provide existing employment uses within podiums. There may also be constraints relating to the nearby heliport which will require consultation to establish, although these are likely to primarily relate to scenario plots within the adjacent zone TB-B2-02.

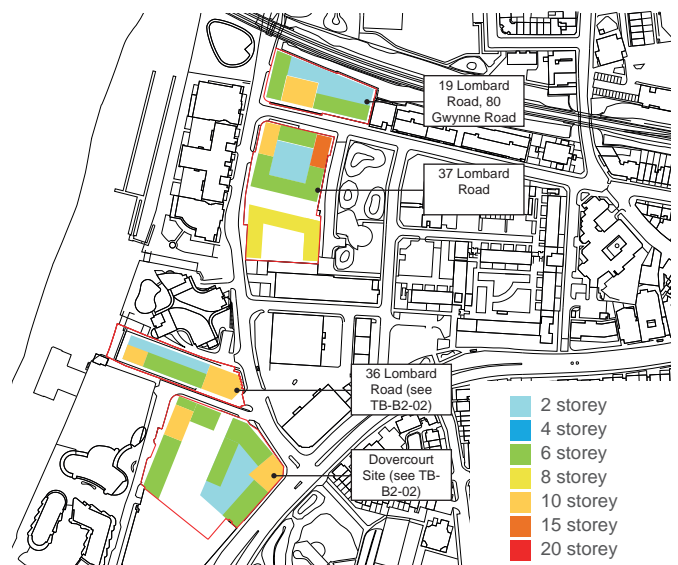


Fig. 259: Riverside Cluster plan

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Fig. 258: Riverside Cluster massing model

### **Extent of visibility**

The ZTV indicates widespread visibility along the River Thames, extending almost to Battersea Bridge to the north with views from Wandsworth Bridge to the south west. Views from frontages along the north bank of the Thames in neighbouring boroughs. Views also available along local streets and more elevated areas of the local green spaces at Fred Wells Gardens, Harroway Gardens and York Gardens. Whilst the ZTV shows some visibility of the upper storeys from more distant locations such as Battersea Park Registered Park and Garden, in reality it is not likely to be readily perceptible in these views due to intervening trees and buildings.



**Fig. 260:** Riverside Cluster scenario ZTV  
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### **High level townscape, visual and heritage assessment**

#### **Baseline**

The following valued features may be affected:

#### **B1 Battersea Residential**

- The open and green character of York Gardens and the locally listed Harroway Road open space. It should be noted however that York Gardens is being re-provided in an alternative arrangement as part of a consented masterplan for the adjacent tall building zone, whereby it will be located within a ring of mid-rise and tall building development.

#### **B2 Battersea Riverside:**

- This historic character of Battersea Square Conservation Area;
- The openness and character of the River Thames corridor.
- Views up and down the River Thames, including

the stretch around Vicarage Gardens and the nearby houseboats. Generally the scenario would sit amongst other development, much of which is taller, and would not adversely affect the river frontage views if designed well.

The following townscape features/characteristics/views may be affected:

- The small-scale character of period terraces along Battersea High Street and Simpson Street to the east of the area (beyond a mid-rise building zone).
- The mature trees within the tall building zone which serve to soften the appearance of existing poor quality blocks and integrate them better into the local context.



**Fig. 261:** Riverside Cluster massing model

### **Assessment**

Any potential impacts on this area on the adjacent York Gardens will be mitigated by its transformation as part of the consented masterplan for the adjacent area, which re-provides the open space within the plot set behind other tall building development. Similarly, any new development in this plot should protect the locally listed Harroway Road open space, including its mature trees which will enhance the integration of future tall building proposals.

The heights illustrated in the scenario are considered to be broadly appropriate in principle, if well-designed and in accordance with design principles in [Appendix A](#). Around Harroway open space, taller buildings should respect the relatively small scale of the streets and the setting of the valued open green space. Site specific designs may therefore look to consider setting taller buildings back from the open space and Gwynne Road, so as not to dominate these spaces with tall elements.

Individual buildings will need to carefully consider the appropriate height for individual plots within the zone, and generally the greatest height should be located internally to the plot (stepping down to surrounding streets) and along York Road where it is closest to the River Thames frontage. Development in the north of the area may have additional height due to the presence of the elevated railway line, although it should not exceed 10 storeys to ensure the small-scale historic character of Battersea Square Conservation Area is protected.

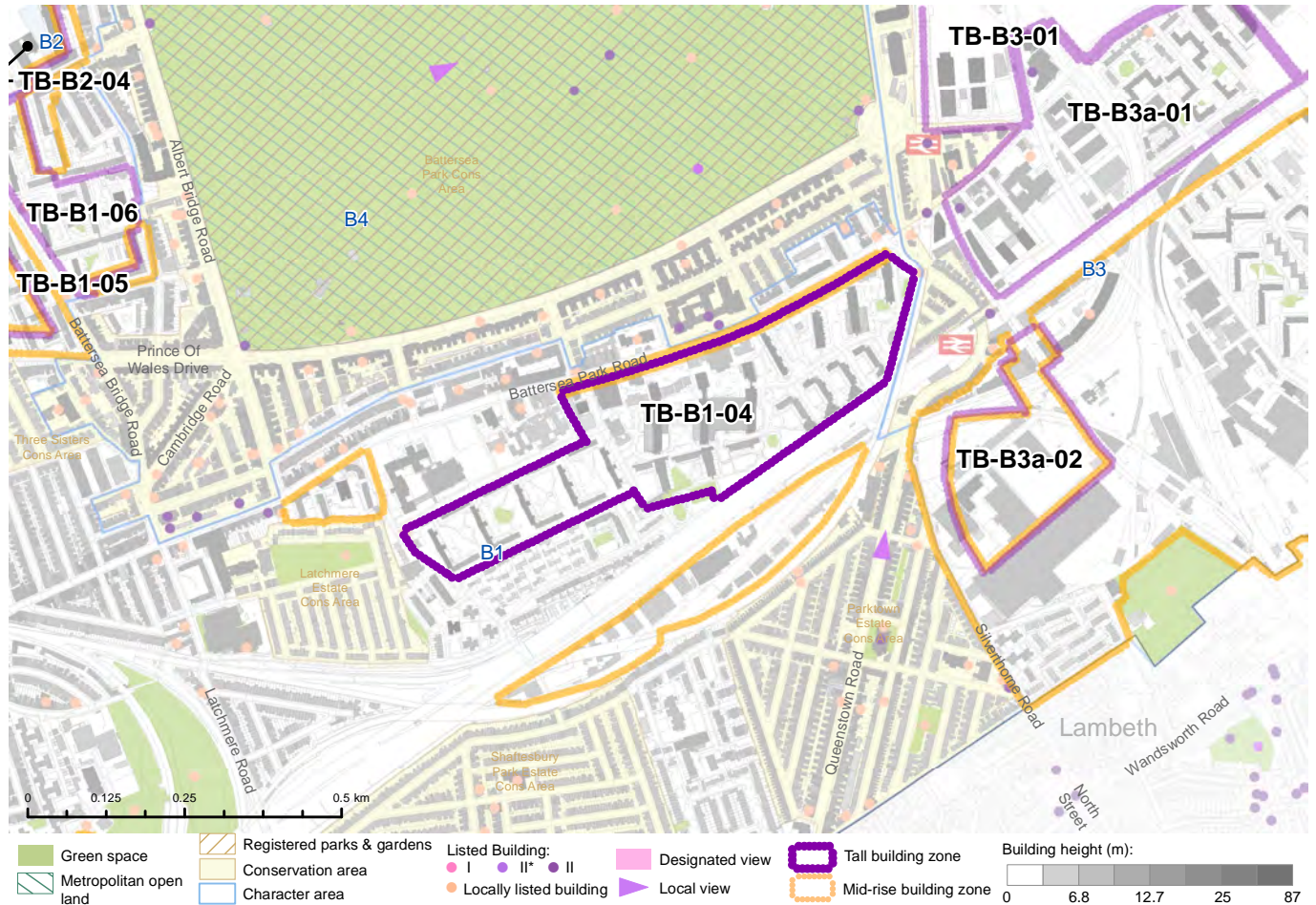
Development should also step down towards the mid-rise zone in the east to protect the character of and views from within the small-scale residential period terraces along Battersea High Street and Simpson Street.

The existing 24 storey post-war tower is overly dominant in the area, including on surrounding sensitive receptors such as Battersea Square Conservation Area. Therefore, this height should not be replicated in the zone, and re-development proposals should seek an overall improvement to the character and a reduction in impacts.

The consented 20 storey building in the south of the zone (55-59 Lombard Road) is considered appropriate only for this area, responding to the width of the York Road and the adjacent taller development to the south and east.

None of the valued features, views or key features would be adversely affected by developments of up to 7-20 storeys in areas noted above if well designed and planned.

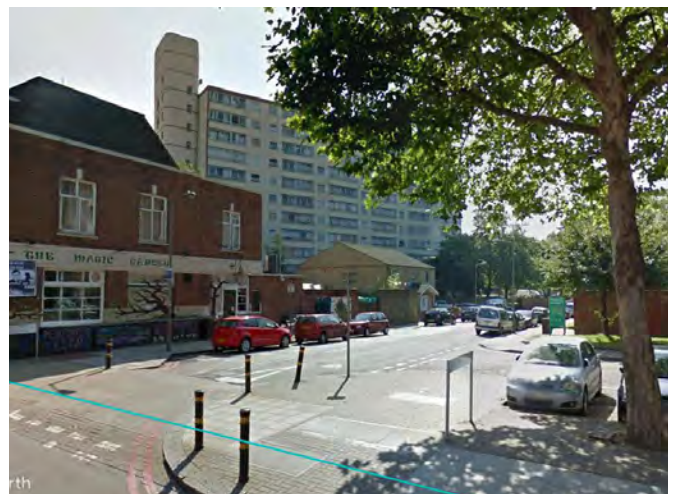
**A.3.3 TB-B1-04: Battersea Residential**  
**Existing prevailing height: 2-20+ storeys**  
**Appropriate height: 7-12 storeys (21-36m)**



**Fig. 262: TB-B1-04 context map**  
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**Fig. 263: Existing 4 and 11 storey blocks on Blondel Street**



**Fig. 264: View into the zone from Battersea Park Road**



<b>TB-B1-04</b>	
<b>Existing use</b>	Residential area including estates and open spaces
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Multiple existing towers and blocks within the residential estate (existing).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Site allocation: OUT2 259-311 Battersea Park Road.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Parktown Estate Conservation Area (B1).</li> <li>Battersea Park Conservation Area (B4).</li> <li>Latchmere Estate Conservation Area (B1).</li> <li>3 listed buildings (grade II) within Battersea Park CA.</li> <li>Listed open space (SW corner of Battersea Park Road and Queenstown Road).</li> <li>Railway bridge (locally listed).</li> <li>Chesterton Centre (locally listed).</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>Railway bridge (grade II).</li> <li>5 locally listed buildings (to the north of the zone).</li> <li>Latchmere Recreation Ground (locally listed).</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Shaftesbury Park Estate Conservation Area (B1).</li> <li>Battersea Park Registered Park and Garden (B4).</li> </ul>
<b>PTAL rating</b>	Low to high (2-6a)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B1 Battersea Residential
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>B2 Battersea Riverside</li> <li>B3 Nine Elms Mixed Use</li> <li>B4 Battersea Park</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Low-high
<b>Capacity</b>	Lower-higher

### *Justification: Existing buildings*

This analysis tests the appropriate building height of tall building zone TB-B1-04, using the existing buildings across the area.

The area includes a mix of 2-3 storey post-war homes close to the railway line, a series of 12 linear blocks rising to 12 storeys in height and two individual towers over 20 storeys (one of which is immediately outside the western edge of the tall building zone). The character profile notes the two towers (Castlemaine House to the west and Park South opposite Alexandra Avenue leading to Battersea Park) as visual detractors due to their height and contrast with the prevailing scale.

The Doddington and Rollo Estate (which forms part of the linear blocks) is noted as addressing the street poorly and visually detracting from the character of Battersea Park Conservation Area.

The strategy is to improve the character of the area which is currently fragmented. The design guidance states that new design should incorporate focal points, a movement strategy and address poor urban design elements of the existing estates. New distinctive

landmarks should have design integrity, improve legibility and be focussed along main roads and at key junctions. Surrounding public realm should be proportionately sized so the height does not overwhelm the human scale.

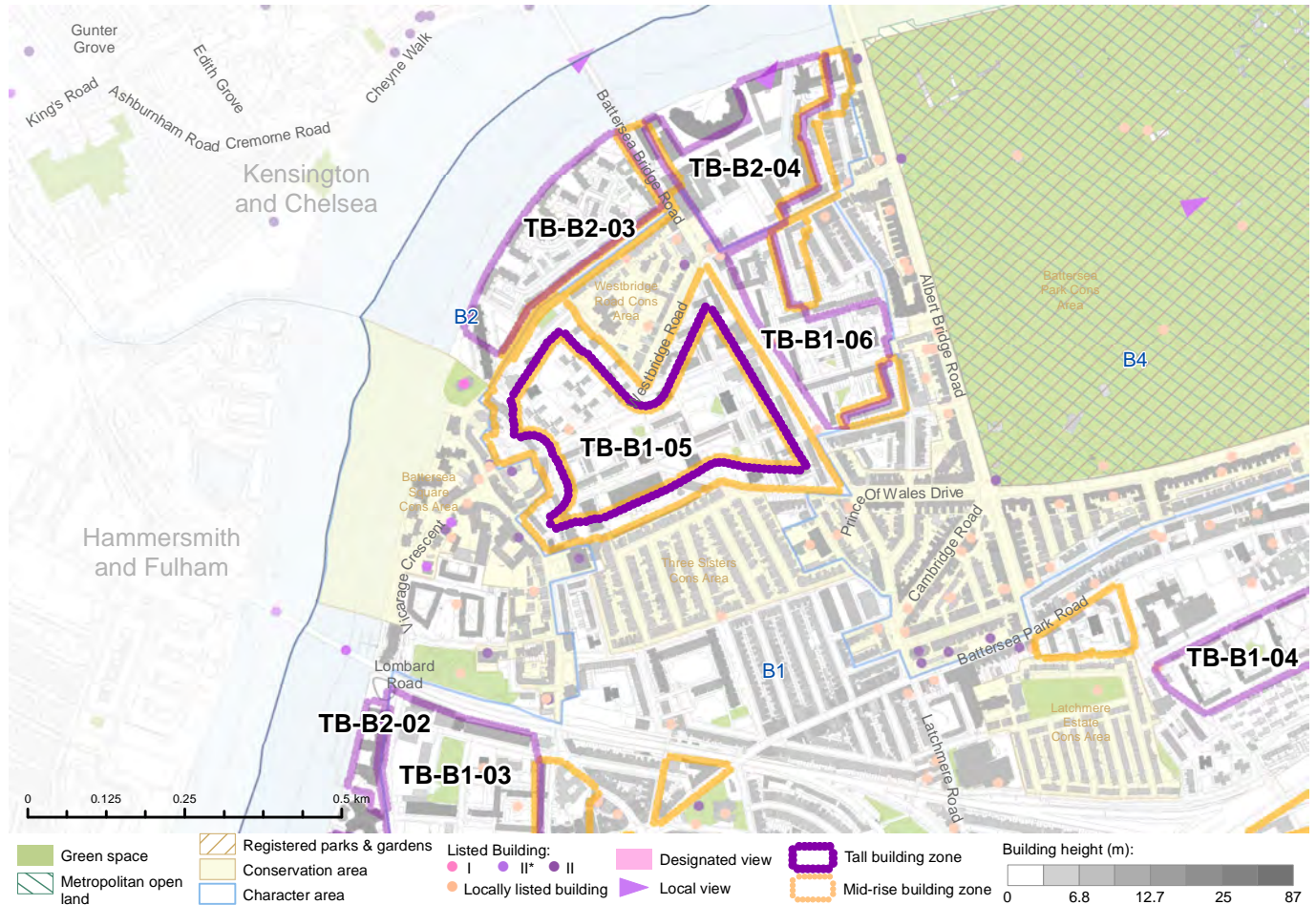
### *Analysis*

Evidence gathered through site visits and the character area profile indicate that the heights of the existing main blocks which characterise this zone (the 12 blocks rising to 12 storeys at their highest) do not negatively impact the character of the area (although their layout and other urban design characteristics do, including how they address the street). They are set within large plots, integrated with open spaces and the surroundings, to some extent, are softened by tree planting though green spaces could be further enhanced. These blocks are generally not visible from within Battersea Park, although the two 20+ storey towers are, which further indicates that development over 12 storeys would not be appropriate for this zone.

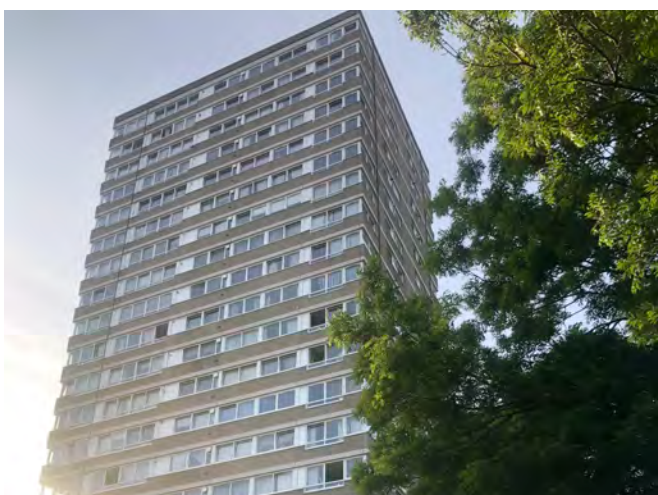
Sensitive/valued features which may be affected by tall buildings include Shaftesbury Estate Conservation Area and Parktown Estate Conservation Area to the south beyond the wide railway line. While the height range indicated should protect the character and openness of Battersea Park Registered Park and Garden, development must step down towards and properly address Battersea Park Conservation Area, which includes 2-4 storey period houses along Battersea Park Road. The small-scale character of Latchmere Estate Conservation Area on the western edge of the zone should be respected, with building heights stepping down appropriately. Development should also respect the small scale nature of residential properties along Blondel Street and Rowditch Lane.nhi9

The tallest blocks should generally step towards the centre of the zone, with capacity for greater height along the edge with the railway rather than along Battersea Park Road which should generally be no higher than 4-6 storeys unless well set back. Existing mature trees are an important feature which soften the appearance of existing tall blocks, and these should be retained.

**A.3.4 TB-B1-05: Battersea Residential**  
**Existing prevailing height: 4-20+ storeys**  
**Appropriate height: 7-8 storeys (21-24m)**



**Fig. 265: TB-B1-05 context map**  
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**Fig. 266: 21 storey tower on Parkham Street**



**Fig. 267: Existing towers behind car parking area on Westbridge Road**

<b>TB-B1-05</b>	
<b>Existing use</b>	Residential area including estates and open spaces
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Multiple existing towers and blocks within the residential estate (existing).</li> </ul>
<b>Planning policy</b>	-
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>The Stage PH (locally listed), within the zone.</li> <li>Former Police Station, Battersea Bridge Road (locally listed), partially within the zone.</li> <li>Three Sisters Conservation Area (B1).</li> <li>Battersea Square Conservation Area (B2).</li> <li>Westbridge Road Conservation Area (B1).</li> <li>Battersea Park Conservation Area (B4).</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>5 listed buildings (grade II), to the west and north of the zone.</li> <li>Church of St Mary (grade I) (B2).</li> <li>9 locally listed buildings.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Battersea Park Registered Park and Garden (B4).</li> </ul>
<b>PTAL rating</b>	Low (2-3)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B1 Battersea Residential - (Improve)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>B2 Battersea Riverside</li> <li>B4 Battersea Park</li> <li>B5 Clapham Junction Town Centre</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Low-high
<b>Capacity</b>	Low-high

### *Justification: Existing buildings*

This analysis tests the appropriate building height of tall building zone TB-B1-05, using the existing buildings across the area, also assessed in the context of the built form surrounding the zone.

The area is characterised by a series of 3-4 storey post war development, within which are located six towers over 20 storeys high and a further 14 storey block. The 21 storey towers of the Surrey Lane Estate within this zone is noted as a detractor in the character profile for this area.

The strategy is to improve the character of the area which is currently fragmented. The design guidance states that new design should incorporate focal points, a movement strategy and address poor urban design elements of the existing estates. New distinctive landmarks should have design integrity, improve legibility and be focussed along main roads and at key junctions. Surrounding public realm should be proportionately sized so the height does not overwhelm the human scale.

### *Analysis*

Evidence gathered through site visits and the character

area profile indicate that the heights of the existing towers detract on the character of the immediate area. This includes the conservation areas which largely encircle this zone - Battersea Park, Westbridge Road, Three Sisters and Battersea Square. These historic areas are all characterised by small scale development with an interesting development pattern, locally rare building types and valued features.

The zone itself has few valued features within, and therefore its scale and width means it could accommodate buildings up to 8 storeys in height is appropriately stepped up from the zone edges and responding to the surrounding sensitive historic urban grain, including through the surrounding mid-rise building zone. Development on the eastern side of Battersea Bridge Road rises to 8 storeys and illustrates how these blocks can respond well to the sensitive surrounding character while providing additional height through appropriate set backs from the road and inclusion of mature tree planting.

The tallest blocks should generally step towards the centre of the zone, with capacity for greater height along the edge with Battersea Park Road provided the setting of and views towards the listed Battersea Bridge are protected. Existing mature trees are an important feature which soften the appearance of existing tall blocks, and these should be retained.

### A.3.5 TB-B1-06: Battersea Residential

Existing prevailing height: 4-17 storeys

Appropriate height: 7-8 storeys (21-24m)

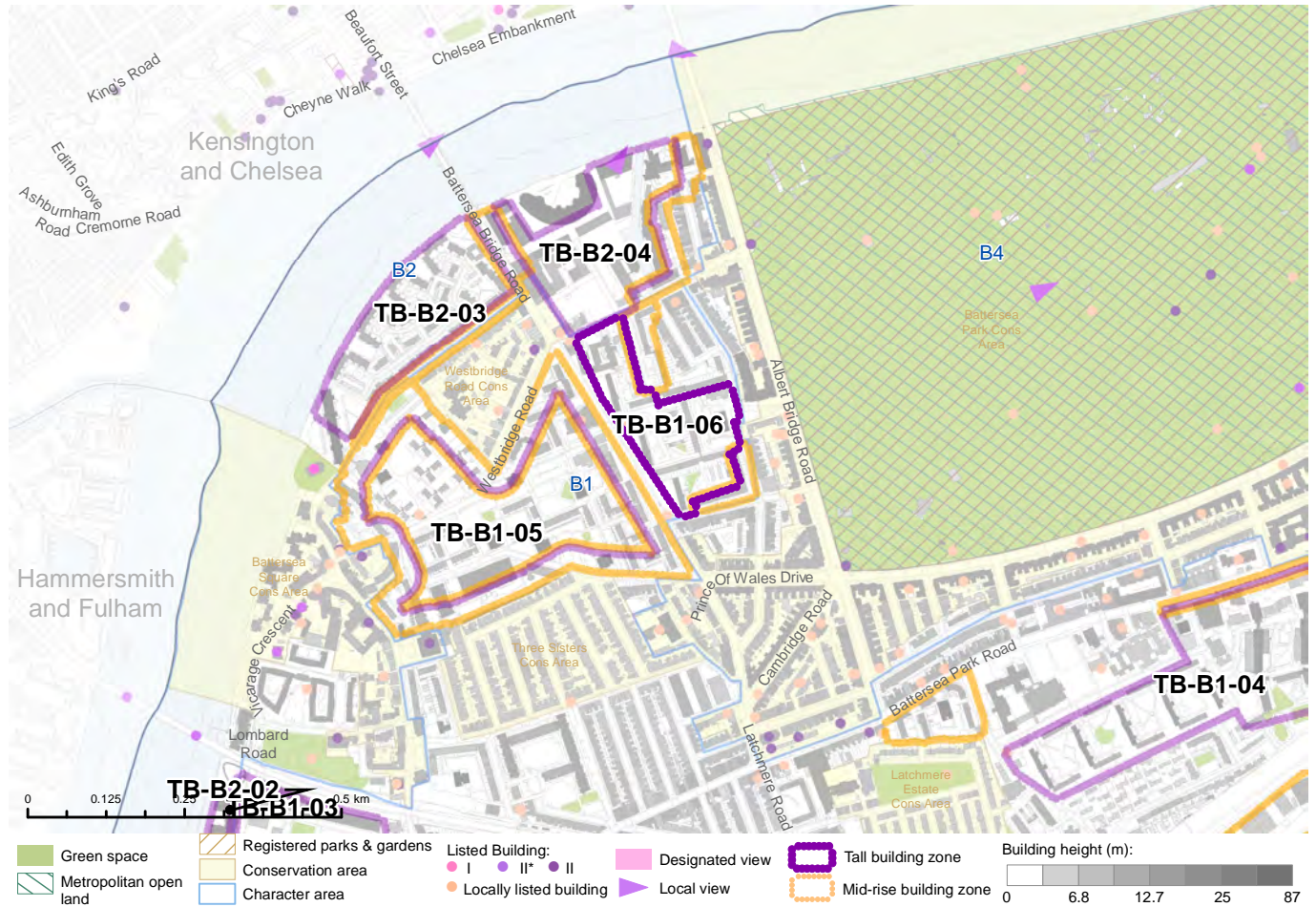


Fig. 268: TB-B1-06 context map

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Fig. 269: 7 storey blocks from Battersea Bridge Road



Fig. 270: Buildings, green spaces and access road on Petford St/Watford Close

TB-B1-06	
<b>Existing use</b>	Residential area including estates and open spaces
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Multiple existing towers and blocks within the residential estate (existing).</li> </ul>
<b>Planning policy</b>	-
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Union Arms Public House (locally listed), within the zone.</li> <li>Battersea Park Conservation Area (B4).</li> <li>Westbridge Road Conservation Area (B1).</li> <li>4 locally listed buildings (east, north and west of the zone).</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>Listed building (grade II), Westbridge Road.</li> <li>5 locally listed buildings (east, north and west of the zone).</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Battersea Square Conservation Area (B2).</li> <li>Three Sisters Conservation Area (B1).</li> <li>Battersea Park Registered Park and Garden (B4).</li> <li>Church of St Mary (grade I) (B2).</li> </ul>
<b>PTAL rating</b>	Low-medium (2-3)
Urban design study	
<b>Character area - (and strategy)</b>	B1 Battersea Residential - (Improve)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>B2 Battersea Riverside</li> <li>B4 Battersea Park</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Low-,medium
<b>Capacity</b>	Low-medium

### *Justification: Existing buildings*

This analysis tests the appropriate building height of tall building zone TB-B1-06, using the existing buildings across the area, also assessed in the context of the built form surrounding the zone.

The area is characterised by a series of 2-8 storey post war development, with the tallest blocks located along (but set back from) Battersea Park Road. None of the buildings within this area are noted as detractors in the character profile, although the 17 storey tower within the east of this zone adversely affects views, particularly from within Battersea Park Conservation Area and the Registered Park & Garden.

The strategy is to improve the character of the area which is currently fragmented. The design guidance states that new design should incorporate focal points, a movement strategy and address poor urban design elements of the existing estates. New distinctive landmarks should have design integrity, improve legibility and be focussed along main roads and at key junctions. Surrounding public realm should be proportionately sized so the height does not overwhelm the human scale.

### *Analysis*

Evidence gathered through site visits and the character area profile indicate that the heights of the existing

tallest blocks within this area (8 storeys, along Battersea Park Road) do not adversely affect the character of the area or surrounding townscape, and respond positively to the width of Battersea Park Road while preserving a positive setting and approach to the listed Battersea Bridge. Further from Battersea Park Road, development has the potential for some additional height to the existing blocks, provided it steps down appropriately towards the surrounding smaller scale built form, particularly within Battersea Park Conservation Area and the retained 2 storey period terraces west of Searles Close and south of Petworth Street.

The additional height present in the 17 storey tower along Maskelyne Close adversely affects the setting of Battersea Park and views from within. It also sits out of context with the surrounding smaller scale built form within nearby period streets, including Westbridge Road Conservation Area.

The zone itself has few valued features within, and therefore its scale and width means it could accommodate buildings up to 8 storeys in height is appropriately stepped up from the zone edges and responding to the surrounding sensitive historic urban grain, including through the surrounding mid-rise building zone. The existing development within the zone along Battersea Bridge Road rises to 8 storeys and illustrates how these blocks can respond well to the sensitive surrounding character while providing additional height through appropriate set backs from the road and inclusion of mature tree planting.

The tallest blocks should generally step towards the centre of the zone, with capacity for greater height along the edge with Battersea Park Road provided the setting of and views towards the listed Battersea Bridge are protected. Existing mature trees are an important feature which soften the appearance of existing tall blocks, and these should be retained.

### A.3.6 TB-B2-02, TB-B2-05 and TB-B2-06: Battersea Riverside

Existing prevailing height: 2-20+ storeys

Appropriate height: 7-10 storeys (21-30m)

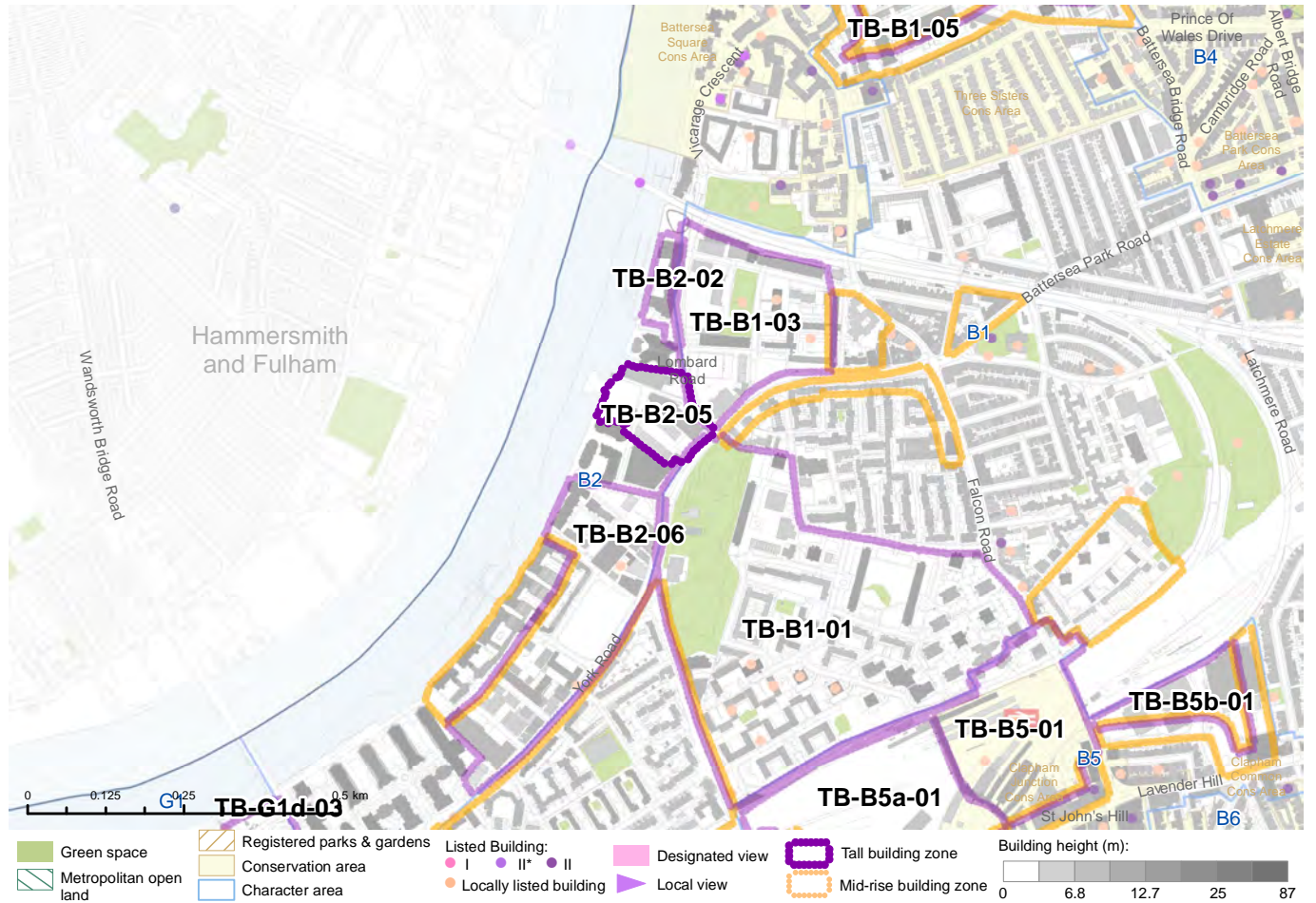


Fig. 271: TB-B2-02 context map

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Fig. 272: Locally listed Candlemakers Apartments, York Road



Fig. 273: View north east across the zone from Bridges Court Road

<b>TB-B2-02</b>	
<b>Existing use</b>	Industrial/residential
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>• Multiple residential/mixed use towers and blocks (existing).</li> <li>• Heliport Heights (under construction).</li> <li>• The Candle Factory (consented).</li> <li>• Plantation Wharf (under construction).</li> <li>• Chatfield Road (consented).</li> <li>• Shell Savoy Filling Station (consented).</li> <li>• Homebase, York Road (consented).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Site allocations: RIV1 Former Prices Candles Factory, 110 York Road; RIV2 Dovercourt site, York Road; RIV3 41-47 Chatfield Road; RIV4 Gartons Industrial Estate, Gartons Way; RIV6 36 Lombard Road; RIV10 200 York Road, Travelodge Hotel;</li> <li>• Thames Policy Area.</li> <li>• Area Strategy: Wandsworth Riverside.</li> <li>• Focal Point : Lombard Road/York Road Riverside.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Candlemakers Apartments (locally listed), within the zone.</li> <li>• Protected Open Space at York Gardens.</li> <li>• Thames Path.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Cremorne Bridge (grade II*).</li> <li>• Harroway Gardens (locally listed).</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• St John's Hill Grove Conservation Area (G2).</li> <li>• Old York Road Conservation Area (G1).</li> <li>• Battersea Square Conservation Area (B2).</li> <li>• Three Sisters Conservation Area (B1)</li> <li>• Church of St Mary (grade I) (B2).</li> </ul>
<b>PTAL rating</b>	Medium to excellent (3-6b)
<b>Urban design study</b>	
<b>Character areas - (and strategy)</b>	B2 Battersea Riverside - (Restore/Improve)
<b>Other character areas within 250m</b>	B1 Battersea Residential
<b>Sensitivity</b>	Low
<b>Probability</b>	Medium-high
<b>Capacity</b>	Higher
<b>Riverside Cluster scenario</b>	
<b>Total residential units</b>	73 (19 Lombard Road, 80 Gwynne Road) 218 (37 Lombard Road (Travis Perkins)) 95 (36 Lombard Road) 188 (Dovercourt Site)
<b>Non Residential GEA</b>	500 sqm (19 Lombard Road, 80 Gwynne Road) 5,500 sqm (37 Lombard Road (Travis Perkins)) 1,800 sqm (36 Lombard Road) 8,200 sqm (Dovercourt Site)
<b>Density (dph)</b>	243 (19 Lombard Road, 80 Gwynne Road) 363 (37 Lombard Road (Travis Perkins)) 316 (36 Lombard Road) 268 (Dovercourt Site)

### Justification: Scenario

A scenario for a cluster of development plots, including two within this zone along Lombard Road including the Dovercourt Site, has been developed to test the appropriate building height of tall building zone TB-B2-02. The character profile notes some negative qualities within this area including the presence of some imposing landmark buildings which lack local distinctiveness, a lack of activity and green space along the river frontage and a highly developed, monotonous

frontage to the northern bank of the Thames.

The strategy is to restore and improve the character of the area which is characterised by a mix of development, not all of which sits comfortable with the historic setting. This zone sits within an area that has the potential for improvement through carefully planned development, particularly of commercial/ industrial premises set back from the river along York Road. The design guidance states that new design should retain, respect and restore the historic elements while promoting new development of a distinctive character which creates remarkable but modest scale landmarks.

The scenario has been developed to place taller building elements along main roads and existing open spaces and to re-provide existing employment uses within podiums. There may also be constraints relating to the nearby heliport which will require consultation to establish.



Fig. 275: Riverside Cluster plan

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Fig. 274: Riverside Cluster massing model

### *Extent of visibility*

The ZTV indicates widespread visibility along the River Thames, extending almost to Battersea Bridge to the north with views from Wandsworth Bridge to the south west. Views from frontages along the north bank of the Thames in neighbouring boroughs. Views also available along local streets and more elevated areas of the local green spaces at Fred Wells Gardens, Harroway Gardens and York Gardens. Whilst the ZTV shows some visibility of the upper storeys from more distant locations such as Battersea Park Registered Park and Garden, in reality it is not likely to be readily perceptible in these views due to intervening trees and buildings.



**Fig. 277:** Riverside Cluster scenario ZTV

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### *High level townscape, visual and heritage assessment*

#### **Baseline**

The following valued features may be affected:

#### **B1 Battersea Residential**

- The open and green character of York Gardens and the locally listed Harroway Road open space within the adjacent zone TB-B1-03. It should be noted however that York Gardens is being re-provided in an alternative arrangement as part of a consented masterplan for the adjacent tall building zone, whereby it will be located within a ring of mid-rise and tall building development.

#### **B2 Battersea Riverside:**

- This historic character of Battersea Square Conservation Area;
- The openness and character of the River Thames corridor.



**Fig. 276:** Riverside Cluster massing model

- Views up and down the River Thames, including the stretch around Vicarage Gardens and the nearby houseboats. Generally the scenario would sit amongst other development, much of which is taller, and would not adversely affect the river frontage views if designed well.

The following townscape features/characteristics/views may be affected:

- The small-scale dock feature towards the east of the zone, which is reminiscent of the areas historic industrial uses.
- Occasional mature trees surrounding buildings and plots within the tall building zone which serve to soften the otherwise largely urban character of the area.
- Small-scale residential terraces to the south of the area, particularly along Holgate Avenue.
- Existing communities of Plantation Wharf. While this area would benefit from enhancements to create a better sense of place, aid legibility and improve the overall quality of experience at this part of the river, the urban form is relatively modest in scale. This development also creates an important break in the taller buildings either side along the River Thames frontage.

#### **Assessment**

Any potential impacts on this area on the adjacent York Gardens will be mitigated by its transformation as part of the consented masterplan for the adjacent area, which re-provides the open space within the plot set behind other tall building development. Similarly, any new development in this plot should protect the setting of the locally listed Harroway Road open space in the adjacent tall building zone (TB-B1-03).



The heights illustrated in the scenario are considered to be broadly appropriate in principle, if well-designed and in accordance with design principles in [Appendix A](#).

Individual buildings will need to carefully consider the appropriate height for individual plots within the zone, and generally the greatest height should be located internally to the plot (stepping down to surrounding streets) and along York Road. Development in the north of the area should not exceed the existing tallest buildings excluding the 28 storey Lombard Wharf which should not set a precedent for very tall development in this location, to protect the small-scale historic character of Battersea Square Conservation Area.

Development should preserve the mid-rise character of the river frontage at Plantation Wharf to avoid creating a canyon with tall buildings all along the Thames. The tallest buildings should therefore be located within the plot, stepping down towards the river, York Road and adjacent uses where present.

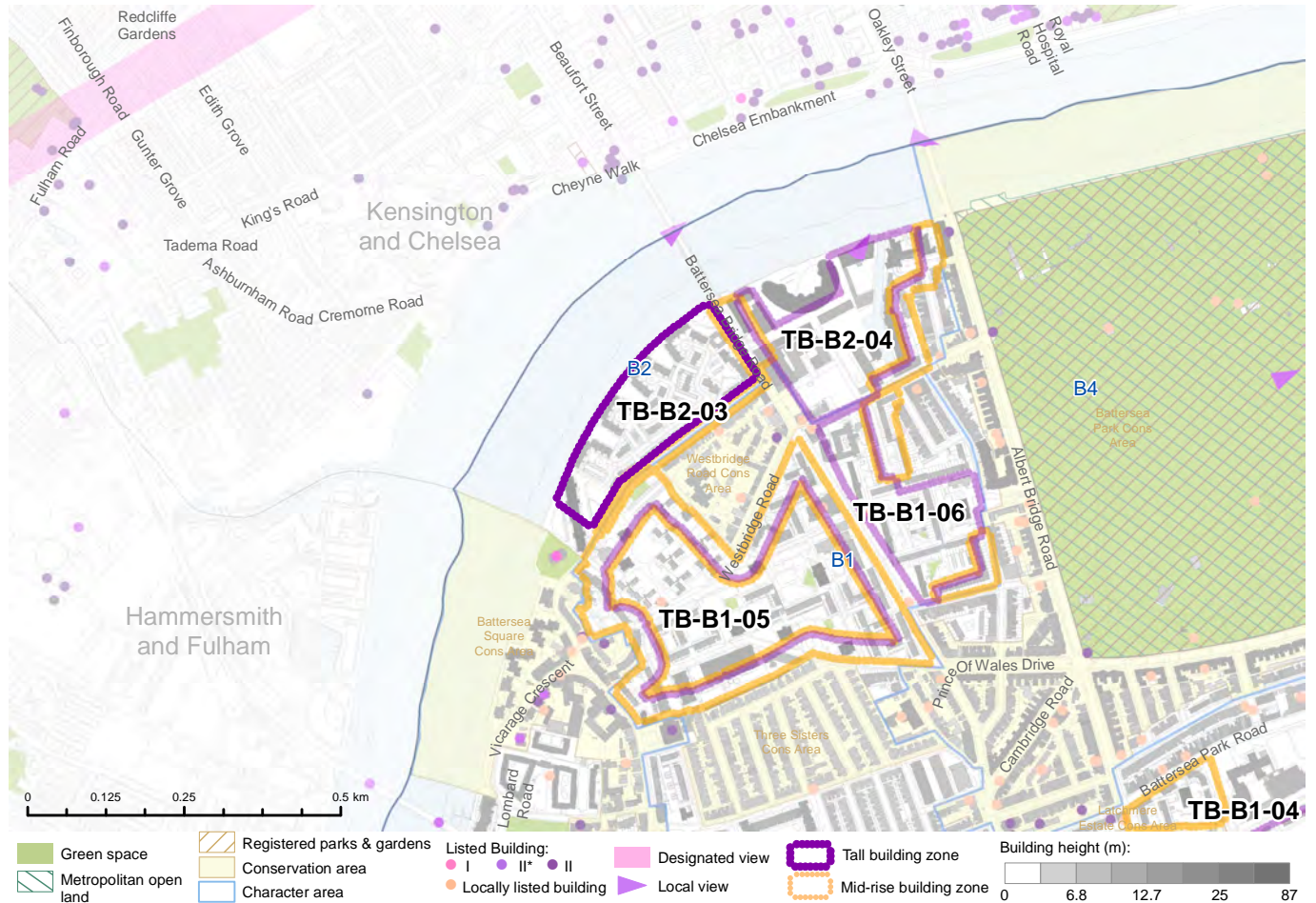
There is consent for two developments over 20 storeys high along York Road within this zone. There is not considered capacity for any further very tall buildings over 10 storeys in the zone, principally due to the potential impacts on the character of the River Thames which is already substantially developed with tall buildings both up and downstream.

None of the valued features, views or key features would be adversely affected by developments of up to 7-10 storeys in areas noted above if well designed and planned.

### A.3.7 TB-B2-03: Battersea Riverside

Existing prevailing height: 3-18 storeys

Appropriate height: 7-12 storeys (21-36m)

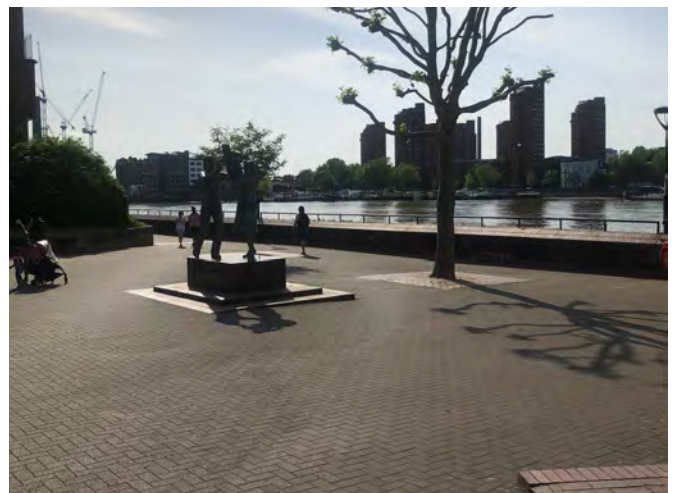


**Fig. 278:** TB-B2-03 context map

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**Fig. 279:** View along the Thames Path with the tall building zone to the right characterised by 3-4 storey 20<sup>th</sup> century development at present



**Fig. 280:** The public open space in the north east of the zone breaks up the built form along the riverside

TB-B2-03	
<b>Existing use</b>	Residential area including estates and open spaces
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>• Montevetro building (existing).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Thames Policy Area.</li> <li>• Area Strategy: Wandsworth Riverside.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Westbridge Road Conservation Area (B1).</li> <li>• 2 locally listed buildings within Westbridge Road CA.</li> <li>• Thames Path.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Church of St Mary (grade I) (B2).</li> <li>• Battersea Square Conservation Area (B2).</li> <li>• Locally listed building and 2 locally listed buildings within Westbridge Road CA.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Battersea Park Conservation Area (B4).</li> <li>• Battersea Park Registered Park and Garden (B4).</li> <li>• Three Sisters Conservation Area (B1).</li> <li>• Chelsea Old Church (All Saints) (grade I) (LB Hammersmith and Fulham).</li> <li>• 100 Cheyne Walk Registered Park and Garden (LB Hammersmith and Fulham).</li> </ul>
<b>PTAL rating</b>	Medium (2-3)
Urban design study	
<b>Character area - (and strategy)</b>	B2 Battersea Riverside - (Restore/Improve)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• B1 Battersea Residential</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Low-medium
<b>Capacity</b>	Medium

### *Justification: Existing buildings*

This analysis tests the appropriate building height of tall building zone TB-B2-03, using the existing buildings within and immediately adjacent to the area.

The area is characterised by a series of 3-4 storey 20<sup>th</sup> century apartment blocks with an extensive amount of space given over to roads and car parking, including some single storey garage blocks. At the western end of the area, a distinctive modern block (the Montevetro building) rises from 5 storeys next to the riverside St Mary's grade I listed church, to 18 storeys beside the River Thames. To the east of the zone, beyond Battersea Bridge, existing buildings are taller and include the modern landmark Albion Riverside, a Fosters designed building which rises to 12 storeys.

While the Montevetro building slopes down towards the historic core of Battersea Square Conservation Area, its height, length and proximity to the grade I listed St Mary's church are considered to detract from this sensitive riverside setting.

The strategy is to restore and improve the character of the area which is characterised by a mix of development, not all of which sits comfortable with the historic setting. This zone sits within an area that

has the potential for improvement through carefully planned development. The design guidance states that new design should retain, respect and restore the historic elements while promoting new development of a distinctive character which creates remarkable landmarks.

### *Analysis*

Evidence gathered through site visits and the character area profile indicate that the height of the Montevetro tower should not create a precedent for further development of this scale along the riverside. In particular, development in the west of this zone should not dominate and diminish the small-scale character of Battersea Square Conservation Area, while buildings in the east should respond positively to and preserve views of the listed Battersea Bridge.

Albion Riverside to the east of this area is considered to sit positively along the riverside, with the scale of the building responding to the adjacent uses and the width of the River Thames. It is also largely set back to create a more generous feeling of space along the river frontage.

Should this zone be redeveloped in the future, it is considered to have capacity for a positive landmark building up to 12 storeys, provided it is located to allow appropriate stepping down towards Battersea Bridge (and Battersea Bridge Road), Battersea Square Conservation Area and Westbridge Road Conservation Area immediately to the south. Development around a building at this height should generally be lower than 10 storeys and should be carefully planned to avoid a canyoning effect along the River Thames which is already heavily developed in this part of London.

Development of this scale is considered to fit well with the existing context of the opposite river bank, which is characterised by individual tall landmark buildings (such as the Worlds End Estate and the Lots Road Power Station development) set amongst an interesting mix of small-scale, mid-rise and more modest tall buildings.

Any future development should protect and enhance the quality of the riverside public realm through wide set backs, mature planting within public open spaces and provision of active uses at ground floor.

### A.3.8 TB-B2-04: Battersea Riverside

Existing prevailing height: 3-12 storeys

Appropriate height: 7-12 storeys (21-36m)

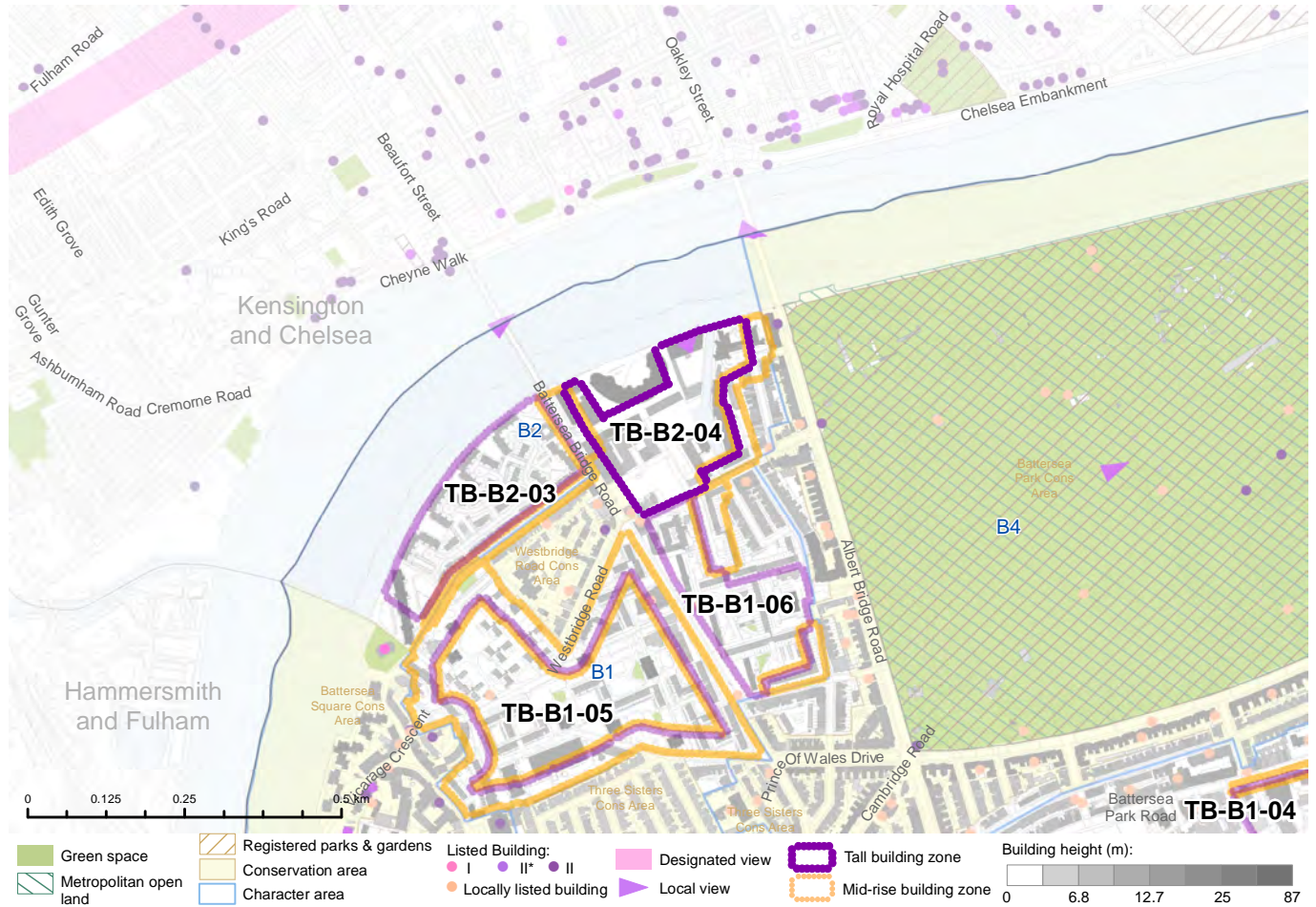


Fig. 281: TB-B2-04 context map

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Fig. 282: Albion Riverside building



Fig. 283: The small scale Ransome's Dock

<b>TB-B2-04</b>	
<b>Existing use</b>	Mixed use/offices.
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Riverside buildings (existing).</li> <li>Royal College of Art- Battersea South Campus (existing).</li> <li>Elcho Street (consented).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Thames Policy Area.</li> <li>Area Strategy: Wandsworth Riverside.</li> <li>Focal Point : Lombard Road/York Road Riverside.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Westbridge Road Conservation Area (B1).</li> <li>3 locally listed buildings to the south west of the zone.</li> <li>Thames Path.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>Battersea Park Registered Park and Garden (B4).</li> <li>Battersea Park Conservation Area (B4).</li> <li>Listed building (grade II) within Battersea Park CA.</li> <li>Listed building (grade II) and 3 locally listed buildings within Westbridge Road CA. One locally listed building to the south east of the zone.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Three Sisters Conservation Area (B1).</li> <li>Battersea Square Conservation Area (B2).</li> <li>Chelsea Old Church (All Saints) (grade I) (LB Hammersmith and Fulham).</li> <li>100 Cheyne Walk Registered Park and Garden (LB Hammersmith and Fulham).</li> <li>Chelsea Physic Garden (LB Hammersmith and Fulham).</li> </ul>
<b>PTAL rating</b>	Medium (2-3)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B2 Battersea Riverside - (Restore/Improve)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>B1 Battersea Residential</li> <li>B4 Battersea Park</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Low-medium
<b>Capacity</b>	Medium/higher

### *Justification: Existing buildings*

This analysis tests the appropriate building height of tall building zone TB-B2-04, using the existing buildings within and in close proximity to the area.

The area is characterised by riverside buildings between 5 and 12 storeys including Albion Riverside, a Fosters designed building which is the tallest in the area but excluded from the actual zone. Behind the riverside blocks, buildings are typically commercial/industrial in character and generally only around 3-4 storeys in height. There are consented tall building developments in this area at between 8 and 10 storeys.

The zone includes the small scale Ransomes Dock feature which has been retained among the tall building developments and is used for residential moorings.

The strategy is to restore and improve the character of the area which is characterised by a mix of development, not all of which sits comfortable with the historic setting. This zone sits within an area that

has the potential for further improvement through carefully planned development of the existing buildings and commercial premises. The design guidance states that new design should retain, respect and restore the historic elements while promoting new development of a distinctive character which creates remarkable landmarks.

### *Analysis*

Evidence gathered through site visits and the character area profile indicate that the height of Albion Riverside sits positively along the riverside, with the scale of the building responding to the adjacent uses and the width of the River Thames. It is also largely set back to create a more generous feeling of space along the river frontage. The heights of buildings generally along the riverfront here are considered to be at capacity. Increases in height would risk adversely affecting the character of the River Thames including the north bank which is designated as a conservation area by the Royal Borough of Kensington and Chelsea. Taller development would also sit uncomfortably between the two listed bridges (Battersea, grade II and Albert, grade II\*) and would affect views from within Battersea Park Registered Park and Garden.

The zone is set back from Albert Bridge Road to ensure appropriate stepping down of building heights towards this approach to the listed bridge and also to protect the immediate setting of Battersea Park. Apart from along the River Thames frontage where there are existing tall buildings, the zone is also set back from the small scale Ransomes Dock to ensure this historic feature continues to positively contribute to the character of the area. The tall building zone is also set back from the majority of Parkgate Road, which is characterised by 3-4 storey small-scale development along its southern edge.

Any future development should protect and enhance the quality of the riverside public realm through wide set backs, mature planting within public open spaces and provision of active uses at ground floor.

### A.3.9 TB-B3-01: Nine Elms Mixed Use

Existing prevailing height: 6-40+ storeys

Appropriate height: 8-25 storeys (24-75m)

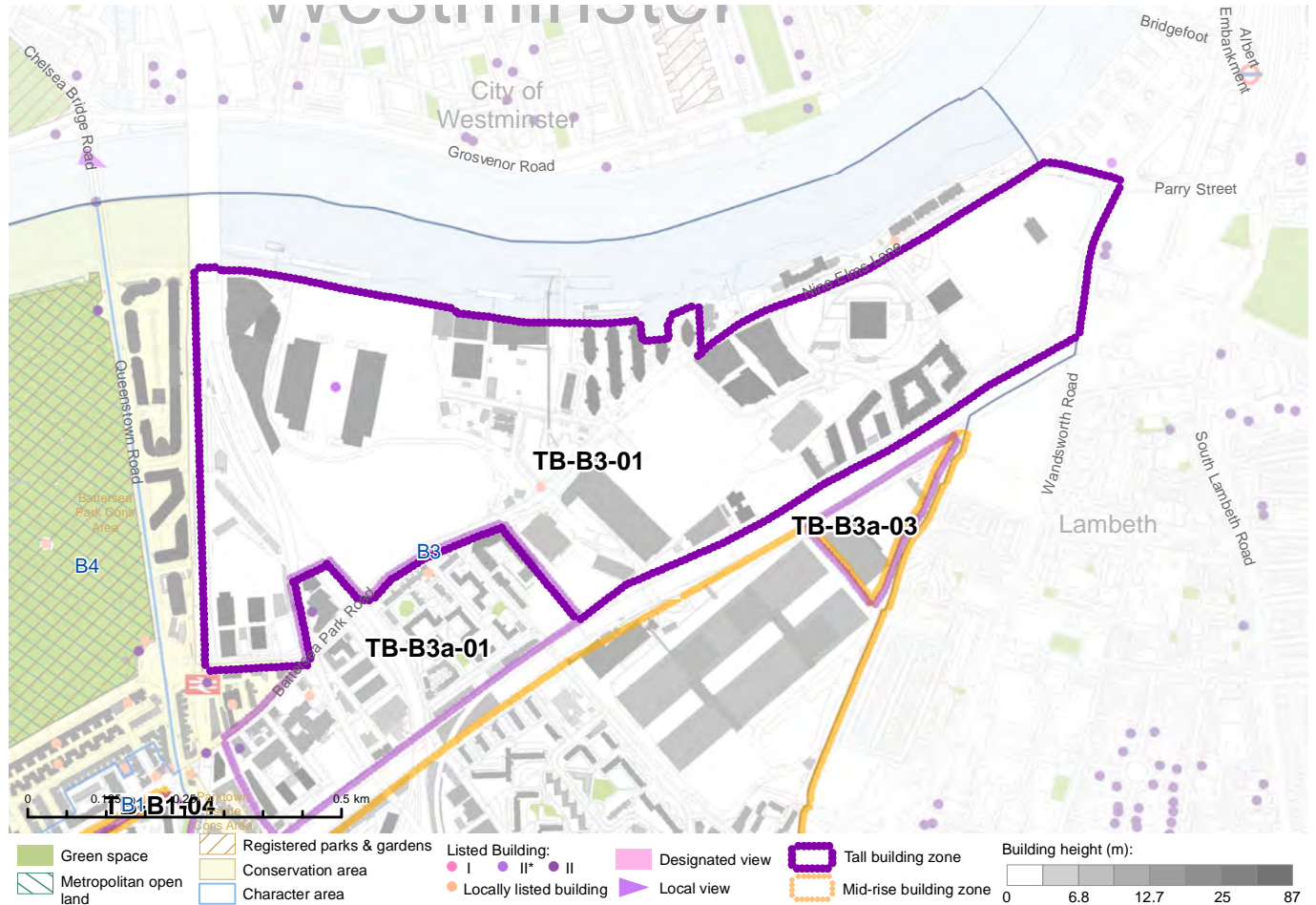


Fig. 284: TB-B3-01 context map

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Fig. 285: Embassy Gardens and the US Embassy



Fig. 286: Battersea Power Station undergoing redevelopment

<b>TB-B3-01</b>	
<b>Existing use</b>	Mixed use/residential/offices.
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Multiple existing and consented buildings within the Opportunity Area.</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Site allocations: NE1 Kirtling Street Cluster; NE2 41-49 Nine Elms Lane and 49-59 Battersea Park Road; NE4 Metropolitan Police Warehouse Garage, Ponton Road; NE10 Middle Wharf; NE12a New Covent Garden Market - Entrance Site;</li> <li>Central Activities Zone</li> <li>Nine Elms Vauxhall Opportunity Area</li> <li>Area Strategy: Nine Elms.</li> <li>Thames Policy Area.</li> <li>Area Strategy: Wandsworth Riverside.</li> <li>Focal Points: Battersea Power Station; Nine Elms near Vauxhall.</li> <li>Safeguarded Wharves: Cringle Dock; Kirtling Wharf.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Battersea Power Station (grade II*), within the zone.</li> <li>Battersea Park Conservation Area (B4).</li> <li>Brunswick House (grade II*) to the east of the zone.</li> <li>4 grade II listed buildings (to the east, south and west of the zone).</li> <li>Parktown Estate Conservation Area</li> <li>Thames Path.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>Battersea Park Registered Park and Garden (B4).</li> <li>4 locally listed buildings, to the south and south east of the zone.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Royal Hospital, Chelsea and Ranelagh Gardens Registered Park and Garden (LB Hammersmith and Fulham).</li> <li>Dolphin Square Gardens Registered Park and Garden (LB Hammersmith and Fulham).</li> <li>Note also the proximity to the Palace of Westminster World Heritage Site in the neighbouring City of Westminster authority, and the London View Management Framework</li> </ul>
<b>PTAL rating</b>	Medium-excellent (2-6b)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B3 Nine Elms Mixed Use - (Transform)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>B4 Battersea Park</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Very high
<b>Capacity</b>	Higher

### *Justification: Existing and consented buildings*

This analysis tests the appropriate building height of tall building zone TB-B3-01 within the Nine Elms Opportunity Area, using analysis of existing and consented buildings within the area.

Nine Elms Opportunity Area is one of the largest major redevelopment sites in London and sits within the Central Activities Zone. It stretches from the Grosvenor Railway Bridge and Battersea Power Station in the west to the tall buildings at Vauxhall riverside in the east. The zone is located entirely to the north of the mainline railway between Clapham Junction and

London Waterloo, beyond which the scale of the built environment reduces. While some development plots remain, particularly around the waste transfer station and adjacent Thames Tideway Tunnel Kirtling Street tunnelling site, the majority of the area now contains 21<sup>st</sup> century tall buildings and other developments under construction.

The majority of development in the area sits between 10 and 25 storeys including the Battersea Power Station development itself, the Riverlight scheme by the river, the US Embassy building and the Embassy Gardens development. There is a cluster of taller towers over 30 storeys in the east of the area close to Vauxhall station, including St George Wharf tower (50 storeys) and another 58 storey tower under construction on the south side of Wandsworth Road. The strategy is to continue to transform the character of the area, improving its fragmented and incoherent character by completing the remaining development plots. The design guidance states that new design should respect remnant historic features and protect the setting of and views towards Battersea Power Station.

### *Analysis*

The area is considered to have no further capacity for towers in excess of 25 storeys as this would risk harm on the quality of the new built environment, the setting of preserved historic assets such as Battersea Power Station and the river crossings, and the setting of Westminster World Heritage Site. However, the remaining development plots have capacity for development of the same scale as the majority of other schemes in the area (10-25 storeys), particularly along the railway line and in the remaining undeveloped stretch of riverside so long as the overall quality of the river environment is considered and protected.

It should be noted that there are a number of existing and consented buildings within this range in the area, which could not be any higher due to specific constraints in the wider landscape, particularly in relation to the Palace of Westminster World Heritage Site. The appropriate height range for this zone will therefore vary for every individual plot and will require specific assessments for any development or re-development proposals.

All new development should appropriately step up from neighbouring developments both within and outside the tall building zone. Riverside development should create a continuous, high quality and generous public realm along the Thames, with a coherent pattern of access to the frontage. Development should also step

down towards Nine Elms Lane which cuts through the centre of the area to avoid creating a shaded and hostile feeling character along this route. The zone is set back to the south of Nine Elms Lane where some retained older small-scale properties are located to avoid fully developing the whole river frontage with tall buildings.

The existing and consented developments include a number of towers over 25 storeys, rising to over 50 (150m) in height. These are primarily clustered in the core of the Opportunity Area close to Vauxhall in neighbouring LB Lambeth. When these consented developments are built out there is considered limited capacity for further extra high towers, and further development should be consistent with the more modest scale 10-25 storey blocks already located along the riverside and the mainline railway. The heat map therefore shows the tallest form of development within this zone as to the far east within the area indicated in the [Site Specific Allocations Document](#) (March 2016), which notes a zone which is not affected by World Heritage Site views. Outside of this zone development should not exceed 20 storeys. The general form of development that will be supported in the Opportunity Area will be 8-10 storey high density development with tall buildings within the 10-25 storey height range on key sites such as along the riverside and in prominent locations along strategic routes as set out in the public realm strategy. Tall buildings must respond to the specific restrictions set out in the London View Management Framework. Generally any development proposals which come forward in this area will need to respond to the detailed considerations set out in the [Site Specific Allocations Document](#) (March 2016) which includes more detailed local context, protected views and settings of heritage assets.

While the zone does not fall within the protected vistas towards Westminster Palace and the Houses of Parliament, all new developments should assess and ensure no impacts from their specific siting and design within the overall zone. Buildings up to 25 storeys would be considered to not affect the outstanding universal value of the site provided they are of good quality and located in the area noted close to Vauxhall. Elsewhere buildings should be between 7 and 10 storeys with taller elements up to 20 storeys in specific locations as noted.



### A.3.10 TB-B3a-01: Nine Elms Mixed Use sub area B3a

Existing prevailing height: 2-12 storeys

Appropriate height: 7-13 storeys (21-39m)

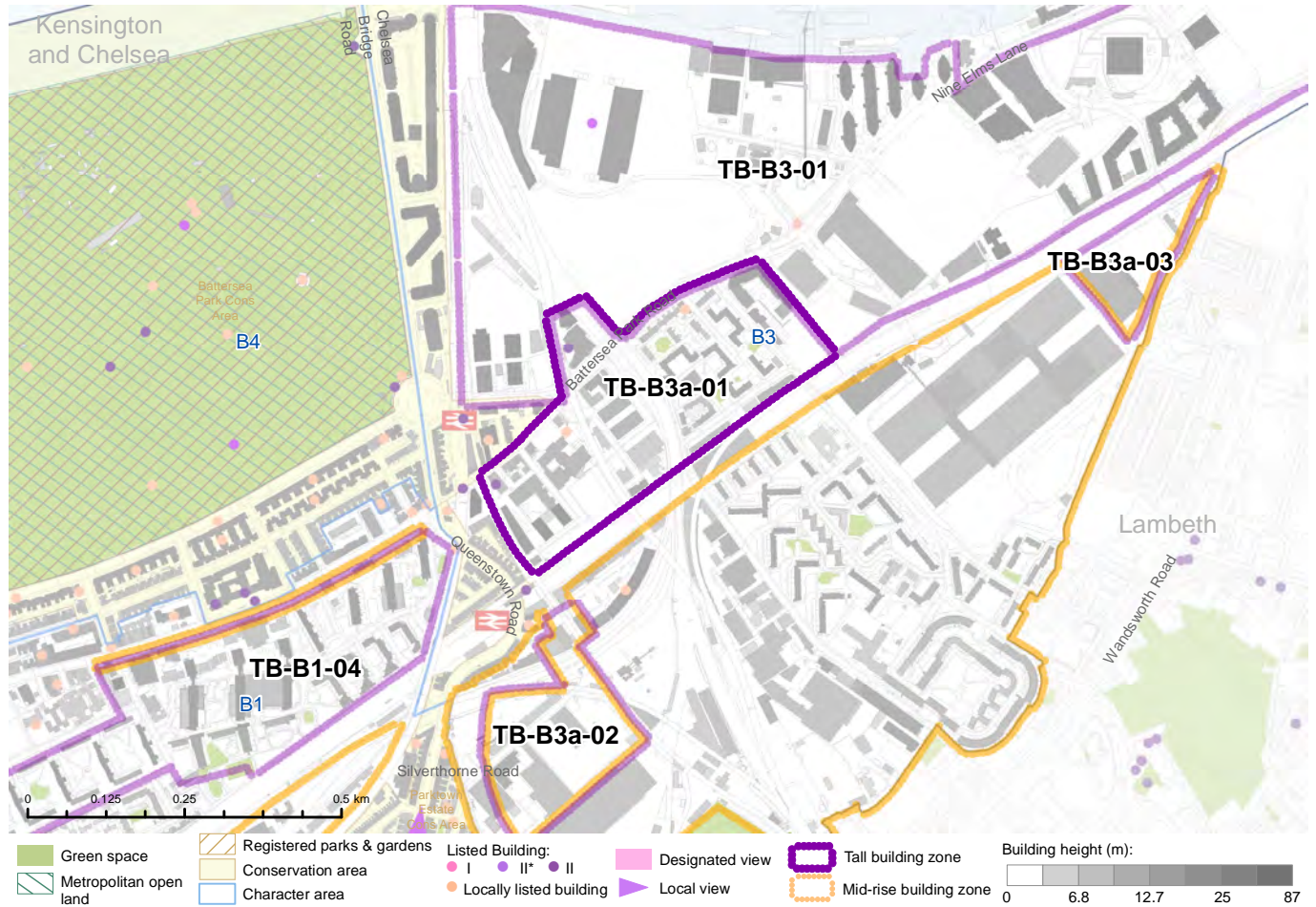


Fig. 287: TB-B3a-01 context map

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Fig. 288: View across the zone from Battersea Park Road



Fig. 289: 5 storey blocks on Savona Street

<b>TB-B3a-01</b>	
<b>Site Area</b>	9.9 ha
<b>Existing use</b>	Residential estates, employment uses, education and open spaces.
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>• Battersea Exchange (existing).</li> <li>• Viridian block, Battersea Park Road (existing).</li> <li>• Palmerston Court (consented).</li> <li>• 38 Havelock Terrace (consented).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Battersea Design and Technology Quarter</li> <li>• Site allocations: NE13 (Draft) Battersea Park Road (between Stewarts Lane and Thessally Road).</li> <li>• Industrial Business Park.</li> <li>• Nine Elms Vauxhall Opportunity Area</li> <li>• Area Strategy: Nine Elms.</li> <li>• Thames Policy Area.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Within the zone: Whittington Lodge, Battersea Dogs and Cats Home (grade II); Masons Arms Public House (grade II); 2 locally listed buildings within the zone.</li> <li>• Parktown Estate Conservation Area (B1).</li> <li>• Battersea Park Conservation Area (B4).</li> <li>• Queenstown Road Station and Railway Bridge (both grade II), to the west of the zone.</li> <li>• One locally listed building (church to the west of the zone).</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Battersea Park Rail Station (grade II).</li> <li>• 5 locally listed buildings, to the south, south west and west of the zone.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Battersea Park Registered Park and Garden (B4).</li> </ul>
<b>PTAL rating</b>	Medium-high (2-6a)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B3 Nine Elms Mixed Use - (Transform)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• B4 Battersea Park</li> <li>• B1 Battersea Residential</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Very High
<b>Capacity</b>	Higher

### Justification: Scenario

This scenario for a cluster of development plots is based on the current Battersea Design & Tech Quarter Economic Appraisal & Design Framework (We Made That, February 2020). A detailed height plan is not currently available, and the heights shown in the model have been assumed for the purpose of the townscape and characterisation analysis.

The scenario tests the appropriate building height of tall building zone TB-B3a-01. Some of the scenario blocks are located outside this zone (within mid-rise building zone MB-B3a-02). The scenario also supports the analysis for the nearby TB-B3a-02 presented on the following pages. These plots area also close to the small-scale Parktown Estate Conservation Area which is characterised by 2-3 storey period terraces.

The tall building zone is located within the Nine Elms Opportunity Area.

The strategy is to continue to transform the character of the area, improving its fragmented and incoherent character by completing the remaining development plots. The design guidance states that new design should respect remnant historic features and protect the setting of and views towards Battersea Power Station.

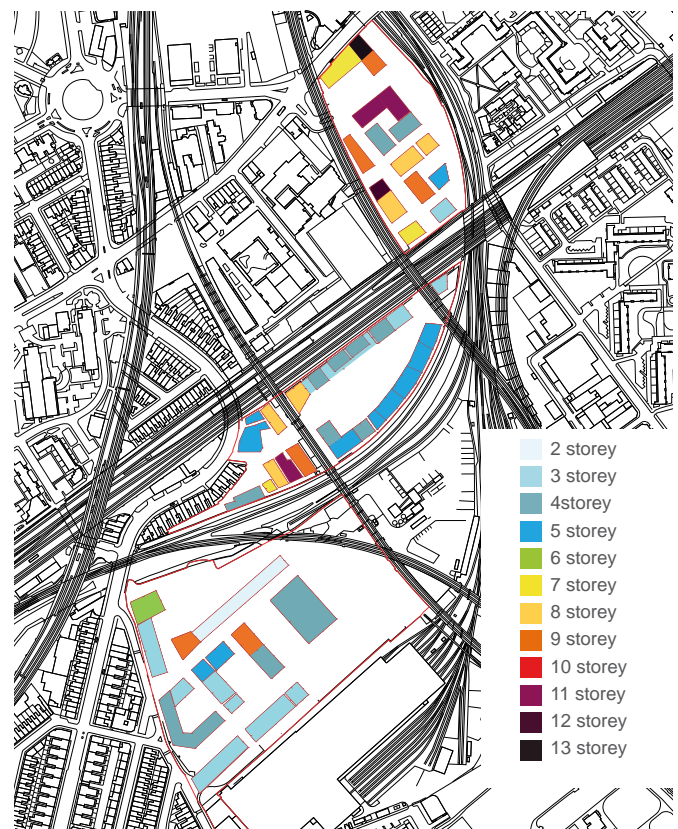


Fig. 290: Battersea Design and Tech Quarter plan

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Fig. 291: Battersea Design and Tech Quarter massing model

### ***Extent of visibility***

Owing to the spread and scale of the buildings in this scenario the visibility is widespread, including:

- along the railway lines;
- from parts of Battersea Park (although mature tree cover will limit visibility in reality);
- from parts of the northern bank of the River Thames including the Royal Hospital Gardens in neighbouring RB Kensington & Chelsea;
- from across parts of the Nine Elms Opportunity Area, although other new development will restrict visibility in the future;
- from some locations within Parkstown Estate Conservation Area to the south west;
- extensive visibility along Battersea Park Road.



**Fig. 293:** Battersea Design and Tech Quarter scenario ZTV  
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### ***High level townscape, visual and heritage assessment***

#### ***Baseline***

The following valued features may be affected:

#### ***B3 Nine Elms Mixed Use:***

- The setting of the iconic Battersea Power Station (grade II\* listed);
- The setting of and distinctive character provided by the following further landmark buildings:
  - new build developments within Nine Elms including the US Embassy
  - New Covent Garden Market
- Mature trees where present due to their rarity in the area;
- The cultural and historic value of the area's past industrial heritage;

- The historic character of the locally listed Hamptons Depository within the site boundary of the scenario but outside the tall building zone.
- Visibility within the backdrop of views from Chelsea Bridge to Battersea Power Station. The scenarios would be seen as part of the wider extensive development of the Nine Elms area.
- View to Battersea Power Station from Queenstown Road north of the Parktown Estate Conservation Area. New development in the scenario may affect the middle ground of this view but unlikely to be overly noticeable in the overall context of wider development in the area.

#### ***B1 Battersea Residential:***

- The distinctive character of the Parktown Estate Conservation Area.

#### ***B4 Battersea Park:***

- The openness, cultural and historic character of Battersea Park Registered Park & Garden and the wider Conservation Area;
- Nationally and locally listed buildings including five storey mansion blocks along the frontage of the park.

The following townscape features/characteristics/views may be affected:

- The 2-3 storey period terraces along Queenstown Road within Parktown Estate Conservation Area adjacent to the western edge of the zone.
- The small-scale character of historic buildings around the Church of Our Lady of Mount Carmel and St Joseph and the grade II listed Battersea Park Station to the north west of the zone.
- Retained 3 storey period buildings either side of Lockington Road along Battersea Park Road.



**Fig. 292:** Battersea Design and Tech Quarter massing model

## *Assessment*

### *Conclusion*

Potential impacts on views will depend on the eventual form of buildings in these areas, and their approach to materials and detailing. However, the massing and spread may have widespread visibility including on the opposite bank of the River Thames.

Considering the setting of, and views from the nearby conservation area to the south west, the building heights shown are considered to be broadly appropriate in principle, if well-designed and in accordance with design principles in [Appendix A](#). Heights should respect the setting of existing landmark buildings including the former Hamptons Depository to the south of the zone and Queenstown Road Station to the west. The area in the west of the zone, around Lockington Road, has already been developed and is likely to be at capacity, with no further potential for additional height to protect the setting of Battersea Park Conservation Area and Parktown Estate Conservation Area

The heights of the scenario within the tall building zone are considered to be appropriate for the area. The tallest elements should be located within the land trapped by the surrounding railways, in the centre of the zone, where the development would not immediately abut other buildings. There is a consented scheme at the northern tip of this part of the zone which rises to 61m (approximately 18 storeys) which sits alongside taller development to the west within zone TB-B3-01. However, due to the proximity of Battersea Park Registered Park & Garden and Conservation Area, the heights shown in the scenario are considered appropriate and further new development should not exceed these.

None of the valued features, views or key features would be adversely affected by developments of up to 7-13 storeys in areas noted above if well designed and planned.

### A.3.11 TB-B3a-02: Nine Elms Mixed Use sub area B3a

Existing prevailing height: 2-5 storeys

Appropriate height: 7-11 storeys (21-33m)

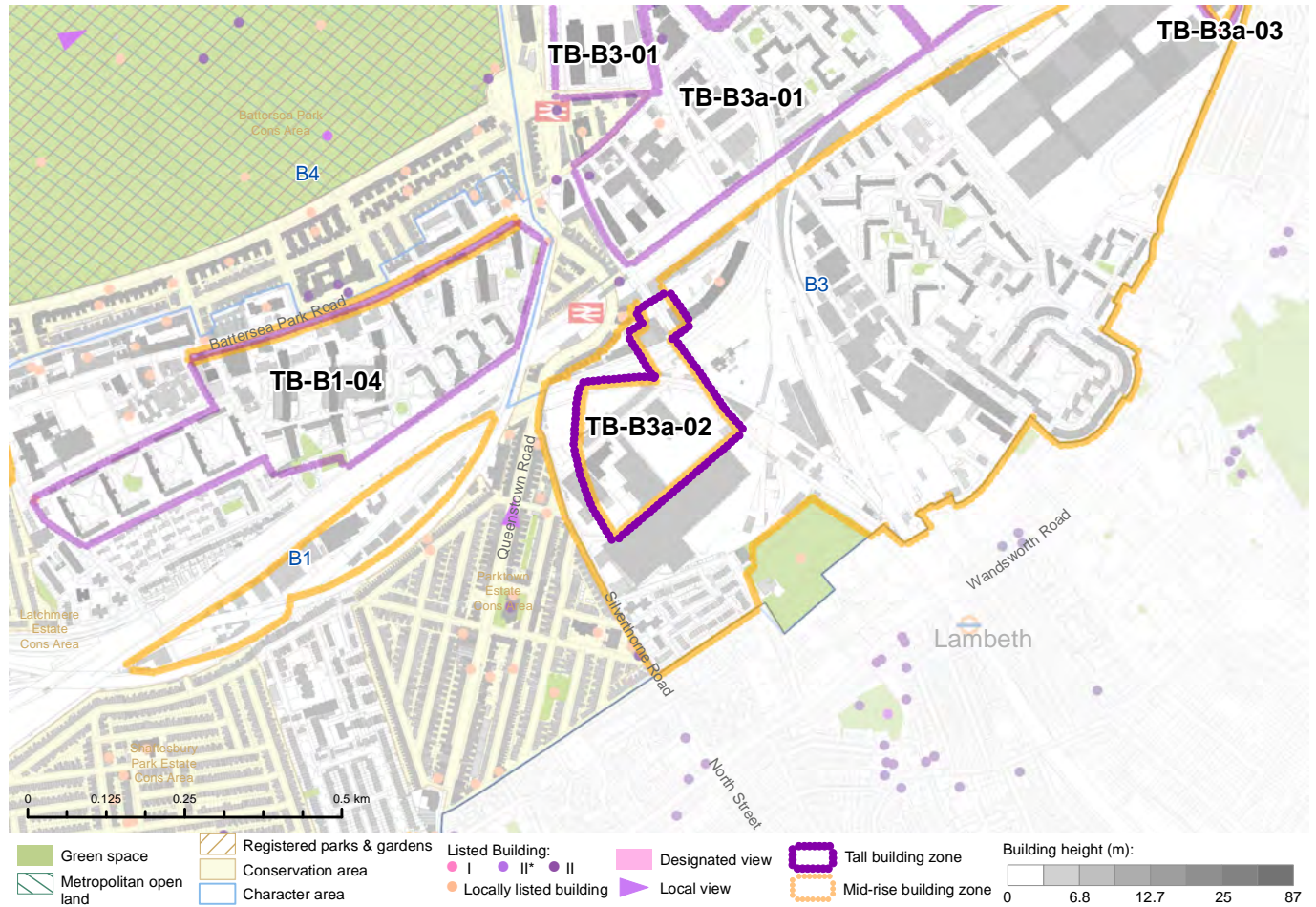


Fig. 294: TB-B3a-02 context map

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Fig. 295: View towards the zone from Silverthorne Road at the junction with the access road marking the southern boundary of the zone



Fig. 296: View east along Ingate Place in the north of the zone

<b>TB-B3a-02</b>	
<b>Existing use</b>	Industrial estate/bus depot.
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>• 6-10 Ingate Place (under construction).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Battersea Design and Technology Quarter</li> <li>• Site allocations: NE7 (Draft) Ingate Place; NE8 Silverthorne Road.</li> <li>• Industrial Business Park.</li> <li>• Nine Elms Vauxhall Opportunity Area.</li> <li>• Area Strategy: Nine Elms.</li> <li>• Queenstown Strategic Industrial Location</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Parktown Estate Conservation Area (B1).</li> <li>• Queenstown Road Station (grade II), to the north of the zone.</li> <li>• Two locally listed buildings on Queenstown Road adjacent to the zone to the north.</li> <li>• Locally listed former Hamptons Depository, to the north east of the zone.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Two locally listed buildings within Parktown Estate Conservation Area.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Battersea Park Registered Park and Garden (B4).</li> <li>• Battersea Park Conservation Area (B4).</li> </ul>
<b>PTAL rating</b>	Medium-high (2-6a)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B3 Nine Elms Mixed Use - (Transform)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• B4 Battersea Park</li> <li>• B1 Battersea Residential</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Very High
<b>Capacity</b>	Higher

### Justification: Scenario

This scenario for a cluster of development plots is based on the current Battersea Design & Tech Quarter Economic Appraisal & Design Framework (We Made That, February 2020). A detailed height plan is not currently available, and the heights shown in the model have been assumed for the purpose of the townscape and characterisation analysis.

The scenario tests the appropriate building height of tall building zone TB-B3a-02. Some of the scenario blocks are located outside this zone (within mid-rise building zone MB-B3a-02). These plots are also close to the small-scale Parktown Estate Conservation Area which is characterised by 2-3 storey period terraces. The tall building zone is located within the Nine Elms Opportunity Area.

The strategy is to continue to transform the character of the area, improving its fragmented and incoherent character by completing the remaining development plots. The design guidance states that new design should respect remnant historic features and protect the setting of and views towards Battersea Power Station.

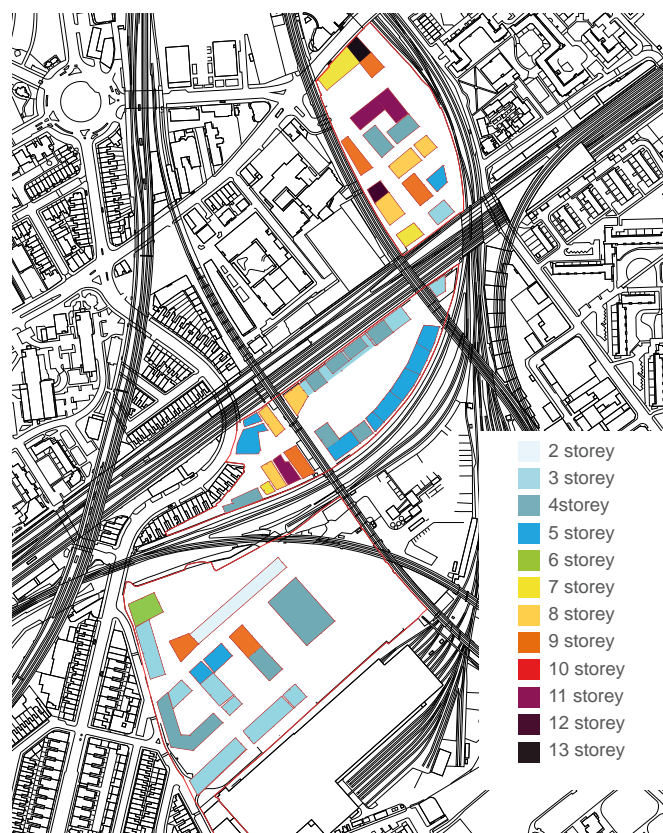


Fig. 297: Battersea Design and Tech Quarter plan

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Fig. 298: Battersea Design and Tech Quarter massing model

### **Extent of visibility**

Owing to the spread and scale of the buildings in this scenario the visibility is widespread, including:

- along the railway lines;
- from parts of Battersea Park (although mature tree cover will limit visibility in reality);
- from parts of the northern bank of the River Thames including the Royal Hospital Gardens in neighbouring RB Kensington & Chelsea;
- from across parts of the Nine Elms Opportunity Area, although other new development will restrict visibility in the future;
- from some locations within Parkstown Estate Conservation Area to the south west;
- extensive visibility along Battersea Park Road.



**Fig. 299:** Battersea Design and Tech Quarter scenario ZTV  
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### **High level townscape, visual and heritage assessment**

#### **Baseline**

The following valued features may be affected:

#### **B3 Nine Elms Mixed Use:**

- The setting of the iconic Battersea Power Station (grade II\* listed);
- The setting of and distinctive character provided by the following further landmark buildings:
  - new build developments within Nine Elms including the US Embassy
  - New Covent Garden Market
- Mature trees where present due to their rarity in the area;
- The cultural and historic value of the area's past industrial heritage;

- The historic character of the locally listed Hamptons Depository within the site boundary of the scenario but outside the tall building zone.
- Visibility within the backdrop of views from Chelsea Bridge to Battersea Power Station. The scenarios would be seen as part of the wider extensive development of the Nine Elms area.
- View to Battersea Power Station from Queenstown Road north of the Parktown Estate Conservation Area. New development in the scenario may affect the middle ground of this view but unlikely to be overly noticeable in the overall context of wider development in the area.

#### **B1 Battersea Residential:**

- The distinctive character of the Parktown Estate Conservation Area.

#### **B4 Battersea Park:**

- The openness, cultural and historic character of Battersea Park Registered Park & Garden and the wider Conservation Area;
- Nationally and locally listed buildings including five storey mansion blocks along the frontage of the park.

The following townscape features/characteristics/views may be affected:

- The 2-3 storey period terraces along Queenstown Road within Parktown Estate Conservation Area adjacent to the western edge of the zone.
- The small-scale character of historic buildings around the Church of Our Lady of Mount Carmel and St Joseph and the grade II listed Battersea Park Station to the north west of the zone.
- Retained 3 storey period buildings either side of Lockington Road along Battersea Park Road.



**Fig. 300:** Battersea Design and Tech Quarter massing model

## *Assessment*

### *Conclusion*

Potential impacts on views will depend on the eventual form of buildings in these areas, and their approach to materials and detailing. However, the massing and spread may have widespread visibility particularly along the railway tracks.

Considering the setting of, and views from the nearby conservation area to the south west, the building heights shown are considered to be broadly appropriate in principle, if well-designed and in accordance with design principles in [Appendix A](#). Heights should respect the setting of existing landmark buildings including the former Hamptons Depository to the south of the zone and Queenstown Road Station to the west.

The heights of the scenario within the tall building zone are considered to be appropriate for the area. The tallest elements should be located along the railway lines and particularly to mark the national rail station at Queenstown Road. However, development requires careful placement within the zone to preserve views towards Battersea Power Station while also respecting the smaller scale character of the area, particularly to the south and west. In particular, development should step down towards the small scale and consistent Parktown Estate Conservation Area to the west and the Hamptons Depository to the north and east.

It is noted that a development (6-10 Ingate Place) is under construction and will rise to 15 storeys (45m). However, due to the proximity of Battersea Park Registered Park & Garden and Conservation Area, the heights shown in the scenario are considered appropriate and further new development should not exceed these.

None of the valued features, views or key features would be adversely affected by developments of up to 7-11 storeys in areas noted above if well designed and planned.



### A.3.12 TB-B3a-03: Nine Elms Mixed Use sub area B3a

Existing prevailing height: 0-4 storeys

Appropriate height: 7-10 storeys (21-30m)

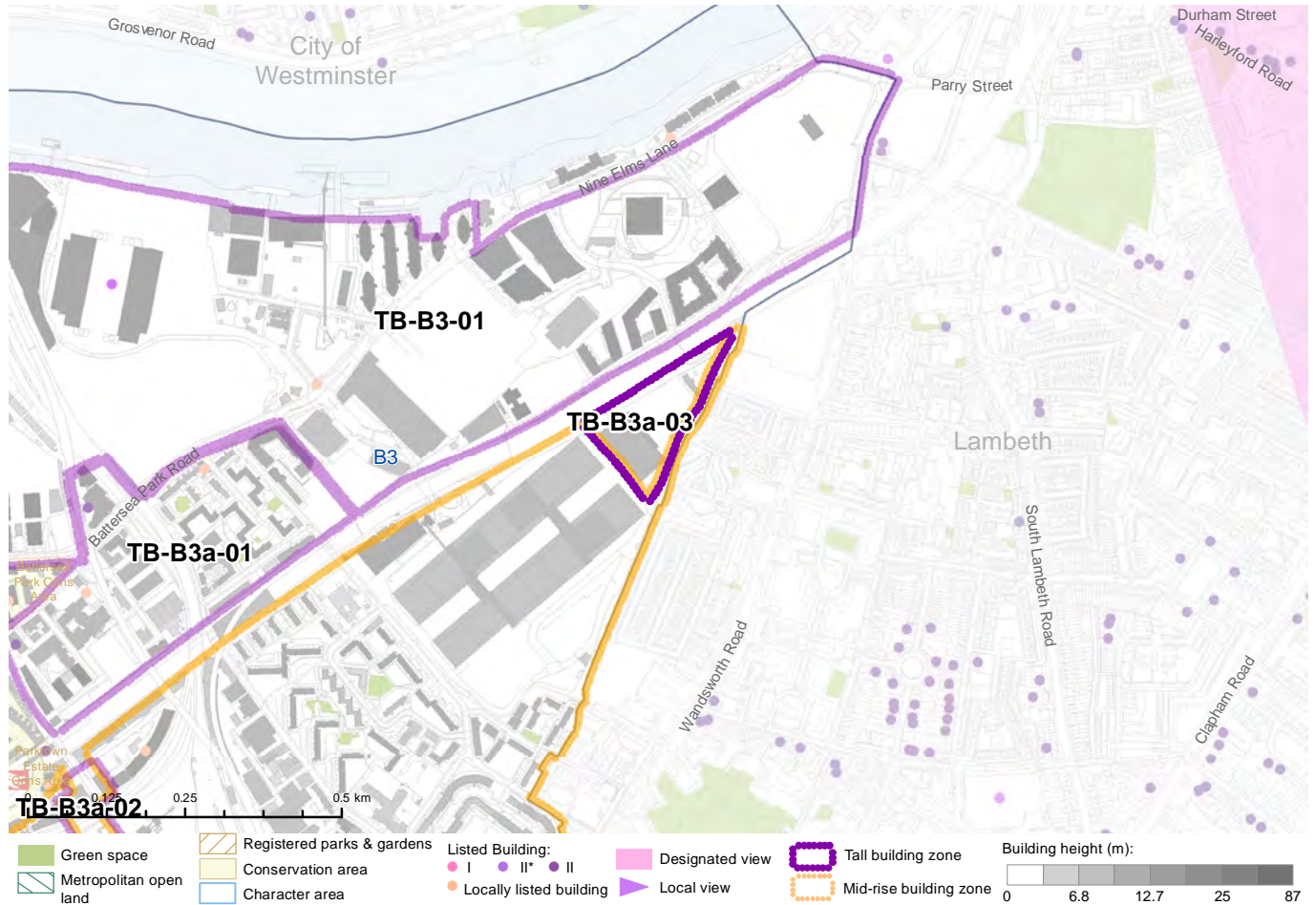


Fig. 301: TB-B3a-03 context map

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Fig. 302: Looking along the northern boundary of the zone beyond the hoarding to the right of the view, and towards towers in Vauxhall



Fig. 303: View north along the southern boundary of the zone, with 5 storey blocks in LB Lambeth to the right of the view and towers in Vauxhall ahead

<b>TB-B3a-03</b>	
<b>Existing use</b>	Construction site
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>• Masterplan including buildings up to 26 storeys (consented).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Nine Elms Vauxhall Opportunity Area.</li> <li>• Area Strategy: Nine Elms.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• None</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• None</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Wandsworth Road Conservation Area (LB Lambeth).</li> </ul>
<b>PTAL rating</b>	High (6b)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B3 Nine Elms Mixed Use - (Transform)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• None</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Very High
<b>Capacity</b>	Higher

### *Justification: Consented buildings*

This analysis tests the appropriate building height of tall building zone TB-B3a-03 within the Nine Elms Opportunity Area, using analysis of consented buildings within the area.

Nine Elms Opportunity Area is one of the largest major redevelopment sites in London and sits within the Central Activities Zone. It stretches from the Grosvenor Railway Bridge and Battersea Power Station in the west to the tall buildings at Vauxhall riverside in the east. The zone is located immediately south of the mainline railway between Clapham Junction and London Waterloo. The railway currently marks a transition point within LB Wandsworth where most tall buildings are located to the north and there is a smaller scale, older character to the south. However, recently there has been significant tall building development in neighbouring LB Lambeth, partially in response to the new Nine Elms London Underground station on the newly extended Northern Line, immediately adjacent to this zone.

The strategy is to continue to transform the character of the area, improving its fragmented and incoherent character by completing the remaining development plots. The design guidance states that new design should respect remnant historic features and protect the setting of and views towards Battersea Power Station.

### *Analysis*

The area is considered to have capacity for tall buildings due to its position next to the wide railway line and proximity to the new underground station and tall buildings in neighbouring LB Lambeth. The depth of the plot means tall buildings can be placed while still allowing for appropriate stepping down towards the smaller scale townscape to the south of the zone. However, with the zone very close to 2 storey residential terraces of mixed ages, it is important for the zone to provide a transition from the much taller buildings to the north of the railway in LB Wandsworth and to the east of the zone in LB Lambeth.

There is an existing consent which includes four buildings 10 storeys or over. These buildings are considered to sit suitably against the surrounding built form, but include a 26 storey tower which is not considered to be replicable within this zone. Buildings in excess of 10 storeys are considered to risk degradation of the urban realm to the south of the zone and also create an adverse canyoning effect along the railway which forms a gateway to central London for huge numbers of people every day.

The zone is set back from the southern edge of the zone to provide a further step down towards the 2 storey terraces and 5 storey mansion blocks immediately adjacent.

None of the valued features, views or key features would be adversely affected by developments of up to 7-10 storeys if well designed and planned.

### A.3.13 TB-B5-02: Clapham Junction Town Centre

Existing prevailing height: 1-5 storeys

Appropriate height: 7-12 storeys (21-36m)

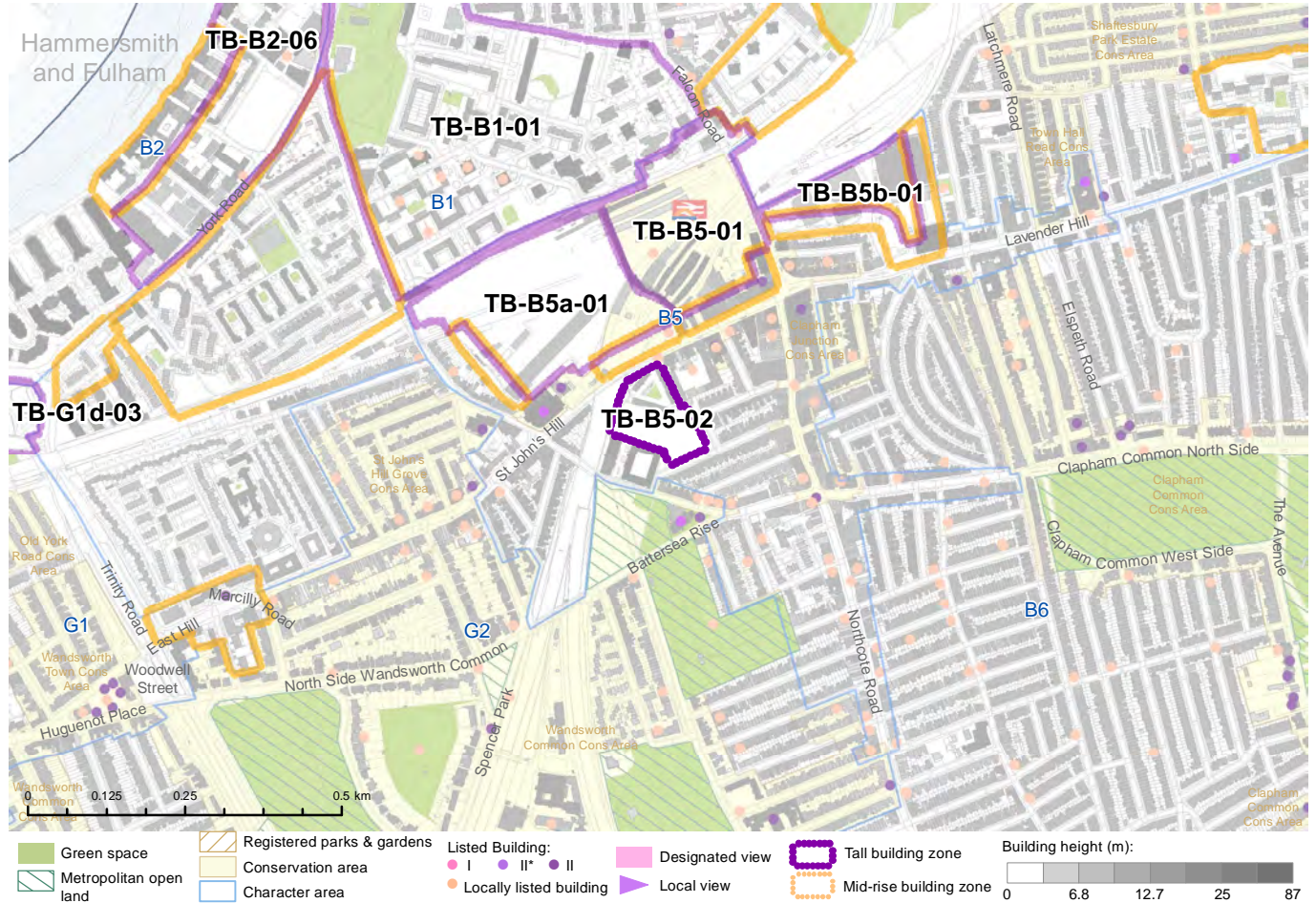


Fig. 304: TB-B5-02 context map

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Fig. 305: View into the zone from the east on Eckstein Road



Fig. 306: View south into the site from St John's Hill at the overbridge

<b>TB-B5-02</b>	
<b>Existing use</b>	Construction site
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Masterplan including buildings up to 12 storeys (consented).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Site Allocation: CJ6 Peabody Estate, St John's Hill</li> <li>Clapham Junction Town Centre.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Clapham Junction Conservation Area (B5).</li> <li>Wandsworth Common Conservation Area (G2).</li> <li>Number of locally listed buildings to the north east of the zone.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>Station Master's House and Grand Theatre (both grade II, within Clapham Junction CA).</li> <li>Church of St Mark (grade II*) and St Mark's Infant School (grade II), within Wandsworth Common CA.</li> <li>Locally listed buildings within Wandsworth Common CA and Clapham Junction CA.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Clapham Common Conservation Area (B6).</li> </ul>
<b>PTAL rating</b>	High (6b)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B5 Clapham Junction Town Centre - (Conserve/Restore)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>B6 Clapham Common and Residential</li> <li>G2 Wandsworth Common and Residential</li> </ul>
<b>Sensitivity</b>	Medium
<b>Probability</b>	Very High
<b>Capacity</b>	High

### Justification: Existing and consented buildings

This analysis tests the appropriate building height of tall building zone TB-B5-02 within the Nine Elms Opportunity Area, using analysis of existing buildings adjacent to the zone (phase 1 of the masterplan for the area) and consented buildings within the area (phases 2 and 3 of the same masterplan).

The first phase of the masterplan (built out to the south of this zone) includes blocks 5 and 7 storeys, with a 9 storey tower along the railway and furthest from the adjacent open space (also designated as Wandsworth Common Conservation Area). The buildings include quality façade materials and details. Phases 2 (in the south of the zone) and 3 (in the north of the zone) provide consent for blocks between 4 and 8 storeys with a 10 storey tower central to the plot and three towers between 10 and 12 storeys along the railway line.

The strategy is to conserve and restore the character of the area, protecting its key characteristics and strengthening existing character by providing new developments of high quality which respect the positive key details, materials and scale of the area.

### Analysis

The distribution of heights across the masterplan (illustrated in [Fig. 307](#) below) is considered to provide an appropriate level of development for this zone.



Fig. 307: Overview of building heights in the consented masterplan

The lower development heights are excluded from the tall building zone to ensure there is an appropriate interface with the small scale development to the south and east of the zone, including within Clapham Junction Conservation Area and Wandsworth Common Conservation Area.

The heat map shows the potential for the tallest buildings along the railway line, furthest from surrounding sensitive receptors. The height of buildings will appear greater when viewed from train passengers as the railway is in deep cutting at this location with a wall rising immediately on the edge of the tall building zone. Therefore it will be important for future development along the railway here to provide an interesting and quality façade to avoid presenting a monotonous wall of development.

Buildings in excess of 12 storeys within this zone may result in significant effects on the character of the town centre including the conservation area, and the setting of the northern end of Wandsworth Common Conservation Area.

None of the valued features, views or key features would be adversely affected by developments of up to 7-12 storeys in areas noted above if well designed and planned.

### A.3.14 TB-B5-01 and TB-B5a-01: Clapham Junction Town Centre

Existing prevailing height: 2-8 storeys

Appropriate height: 7-15 storeys (21-45m)

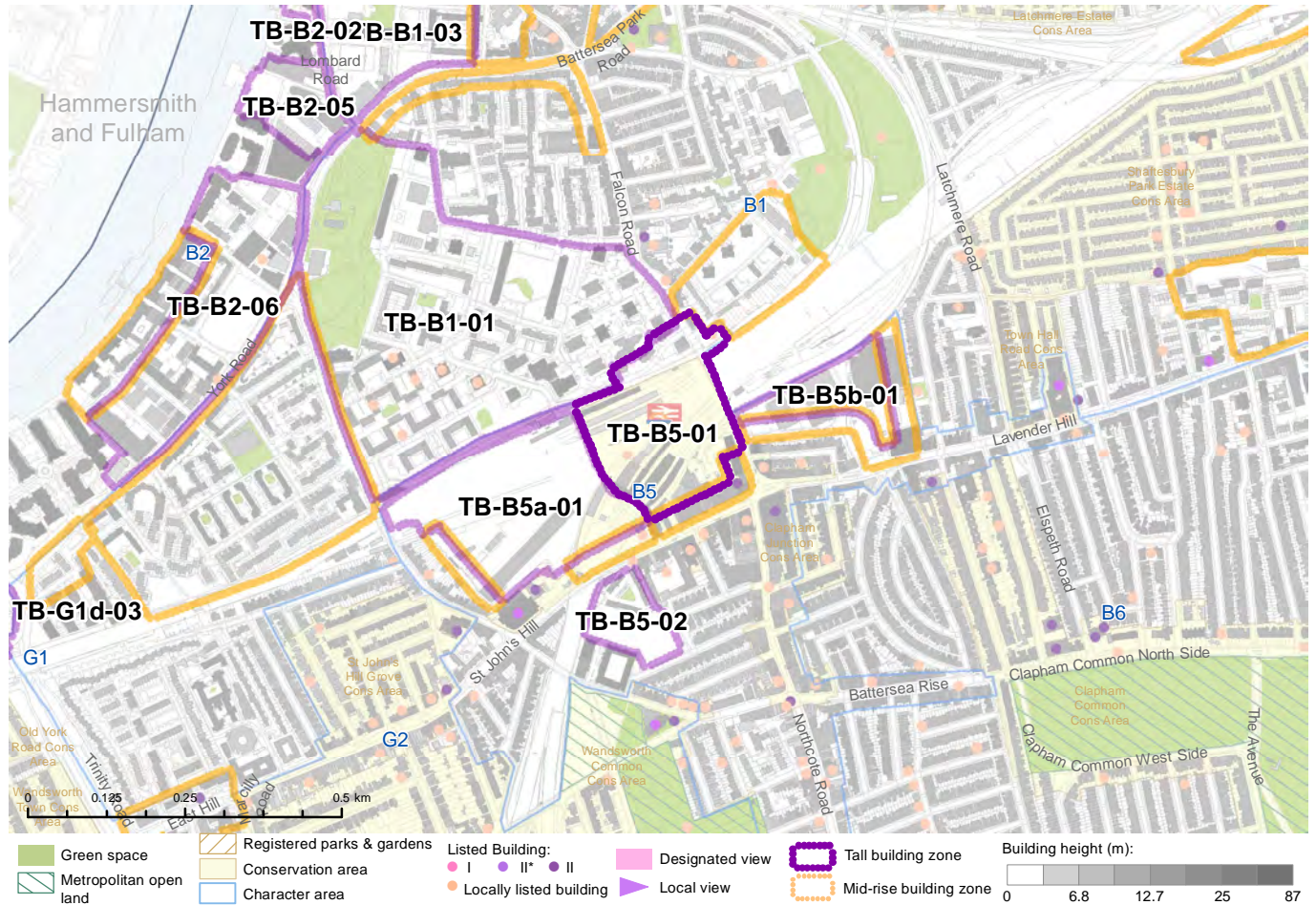


Fig. 308: TB-B5-01 and TB-B5a-01 context map

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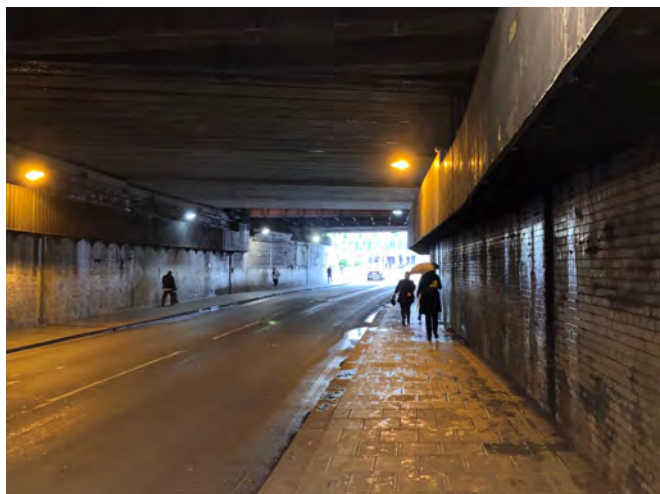


Fig. 309: Falcon Road underneath the railway

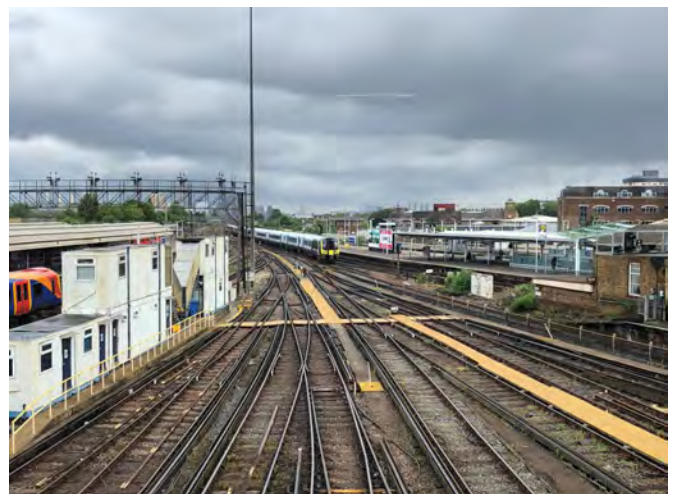


Fig. 310: Expansive views from the railway towards Falcon Lane

TB-B5-01 and TB-B5a-01	
<b>Existing use</b>	Commercial and car parking
<b>Existing/consented tall buildings</b>	• None
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Town centre (Clapham Junction);</li> <li>• Area Strategy: Clapham Junction and York Road/ Winstanley Regeneration Area.</li> <li>• Site allocation: CJ2 Clapham Junction Station Approach; CJ4 Land at Clapham Junction Station.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Within the zone: Clapham Junction Conservation Area (B5); Clapham Junction Railway Station (locally listed).</li> <li>• St John's Hill Grove Conservation Area (G2).</li> <li>• Former Gala Bingo Hall (grade II*).</li> <li>• Two grade II listed buildings (Grand Theatre and Falcon Hotel Public House).</li> <li>• Three locally listed buildings.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Three grade II listed buildings (within Clapham Junction CA and St John's Hill Grove CA).</li> <li>• 8 locally listed buildings.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Clapham Common Conservation Area (B6).</li> <li>• Wandsworth Common Conservation Area (G2).</li> <li>• Town Hall Road Conservation Area (B1).</li> <li>• Shaftesbury Park Estate Conservation Area (B1).</li> </ul>
<b>PTAL rating</b>	High (6b)
Urban design study	
<b>Character area - (and strategy)</b>	B5 Clapham Junction Town Centre - (Conserve/ Restore)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• B6 Clapham Common and Residential</li> <li>• B1 Battersea Residential</li> <li>• G2 Wandsworth Common and Residential</li> </ul>
<b>Sensitivity</b>	Low/Medium
<b>Probability</b>	Very high
<b>Capacity</b>	Higher
Clapham Junction Station Approach scenario	
<b>Site Area</b>	1.3ha
<b>Total residential units</b>	425 (Clapham Junction)
<b>Non Residential GEA</b>	7,000 sqm (Clapham Junction)
<b>Density (dph)</b>	326 (Clapham Junction)

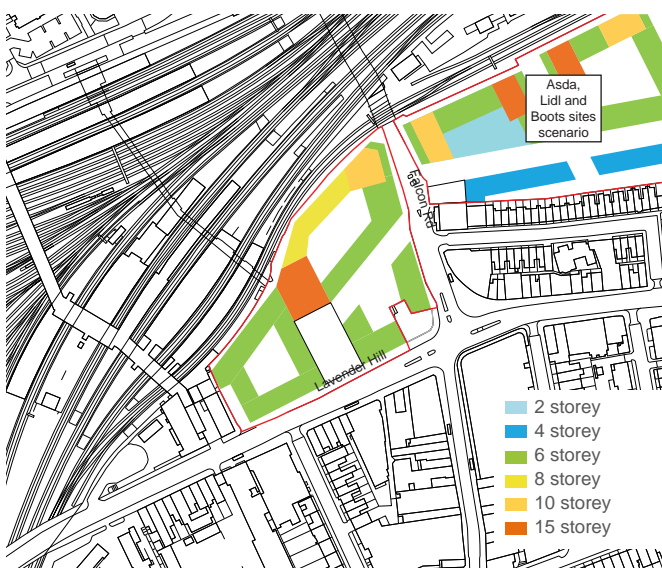


Fig. 312: Clapham Junction Station Approach scenario plan

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### Justification: Scenario

This scenario tests the appropriate building height of tall building zone TB-B5-01 and the adjacent TB-B5a-01. The potential for taller towers on this site was considered and found to give rise to adverse effects on townscape character, heritage assets and views. Therefore this scenario was developed on the basis of:

- Towers located along the railway corridor and away from St John's Hill to avoid affecting their existing character and to reduce impacts on the setting of listed buildings.
- Reduction in building heights towards east and west to respect the existing low-rise residential buildings.

The strategy is to conserve and restore the character of the area, protecting its key characteristics and strengthening existing character by providing new developments of high quality which respect the positive key details, materials and scale of the area.

TB-B5a-01 around Clapham Junction station is noted as being a key location for targeted change to enhance the character, legibility and sense of arrival due to the presence of more detracting existing features.



Fig. 313: Clapham Junction Station Approach massing model



Fig. 311: Clapham Junction Station Approach massing model

### ***Extent of visibility***

Widespread visibility along the extensive railway tracks and also along streets converging on Clapham Town Centre including Northcote Road, Lavender Hill and Falcon Road. Extensive visibility from Clapham Common and parts of Wandsworth Common and Battersea Park. Intermittent visibility from smaller open spaces and also frontages along the north bank of the River Thames in neighbouring boroughs.



**Fig. 314:** Clapham Junction Station Approach scenario ZTV  
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### ***High level townscape, visual and heritage assessment***

#### ***Baseline***

The following valued features may be affected:

#### ***B5 Clapham Junction Town Centre:***

- Clapham Junction Conservation Area;
- the scale and overall coherence of the terraced buildings and Victorian/Edwardian shop houses and their settings;
- landmark listed buildings, including the Falcon Hotel Public House and the Arding & Hobbs Store.
- Views from the railway bridge on St John's Hill to Lavender Hill which could be substantially altered by tall buildings close to Lavender Hill.
- Views from the listed overbridge at Clapham Junction which is focused on the tall buildings of Central and west London. The scenario would dominate parts of the foreground and would require careful design and massing to ensure they don't detract from the longer distance views.

#### ***B6 Clapham Common and Residential:***

- the setting of Clapham Common with its distinct sense of openness;

- New taller buildings are likely to be visible within long distance views with wooded skylines from Clapham Common, necessitating careful design and placement / clustering to ensure they don't become dominant.

#### ***B1 Battersea Residential:***

- the openness and green character of Latchmere Recreation Ground within Latchmere CA;
- the character of Shaftesbury Park Estate Conservation Area.

The following townscape features/characteristics/views may be affected:

- The small-scale character of period buildings along Falcon Road to the north of the railway line.
- The retained area of 2-3 storey period terraces along Mossbury Road and also facing onto Lavender Hill immediately east of TB-B5-01.
- Views from passengers as they pass through Clapham Junction station on foot and on train.
- The setting of the Grand Theatre along St John's Hill, south of the zones.
- 2-3 storey period terraces along Plough Road to the west of TB-B5a-01.
- The period building forming the main entrance to Clapham Junction station and the nearby 3.5 storey block by the bus turning area, both along St John's Hill.

#### ***Assessment***

#### ***Conclusion***

The massing illustrated along St John's Hill and adjacent to the Falcon pub is likely to be broadly appropriate in principle; buildings should continue the roofline of the existing built form and should not exceed the height of existing buildings in these locations. The tall building zone has therefore been set back from St John's Hill to ensure an appropriate step down in height towards this distinct street at the heart of the historic core of Clapham Junction Town Centre. This is further indicated by the inclusion of mid-rise building zones along St John's Hill and Falcon Road.

The height of the buildings at the northern end of Falcon Road may be able to increase towards the railway, with the heights shown considered to be broadly appropriate in principle. Any height here will need to specifically consider the modest scale of Falcon Road and the railway bridge, and the character of the landmarks at the junction with Lavender Hill. Taller

buildings in this location should be set back from the road to retain views along Falcon Road towards the railway bridge. The building heights shown in the scenario are considered more appropriate in principle than taller blocks reviewed, particularly in the context of Clapham Junction Conservation Area and landmark buildings. The location does not lend itself to a cluster of tall buildings, but rather one or two individual elements of appropriate heights.

Buildings in excess of 15 storeys within the Clapham Junction site may result in significant effects on the character of the town centre and setting of landmark buildings and heritage assets. It may also result in impacts on views and visual amenity, particularly on period landmarks such as the Arding & Hobbs listed store.

Development north of the railway line should respond to the more modest scale of the surrounding townscape in this location, particularly along Falcon Road. Development here should generally not exceed 7-8 storeys and be well set back from retained smaller-scale buildings and street frontages.

Any future development should also ensure appropriate stepping down in height towards the period terraces along Plough Road

The heights of the scenario are considered to be potentially appropriate in principle for the area, pending further detailed analysis at a specific site scale. The tallest elements within the zones should all be located towards the railway to protect the historic character and street frontages of St John's Hill. However, the pattern of development should avoid creating a canyon along the railway line, which would adversely affect the experience of users of the railway, including distinctive views towards central London from the overbridge.

None of the valued features, views or key features would be adversely affected by developments of up to 7-15 storeys in areas noted above if well designed and planned.



### A.3.15 TB-B5b-01: Clapham Junction Town Centre sub-area B5b

Existing prevailing height: 2-8 storeys

Appropriate height: 7-15 storeys (21-45m)

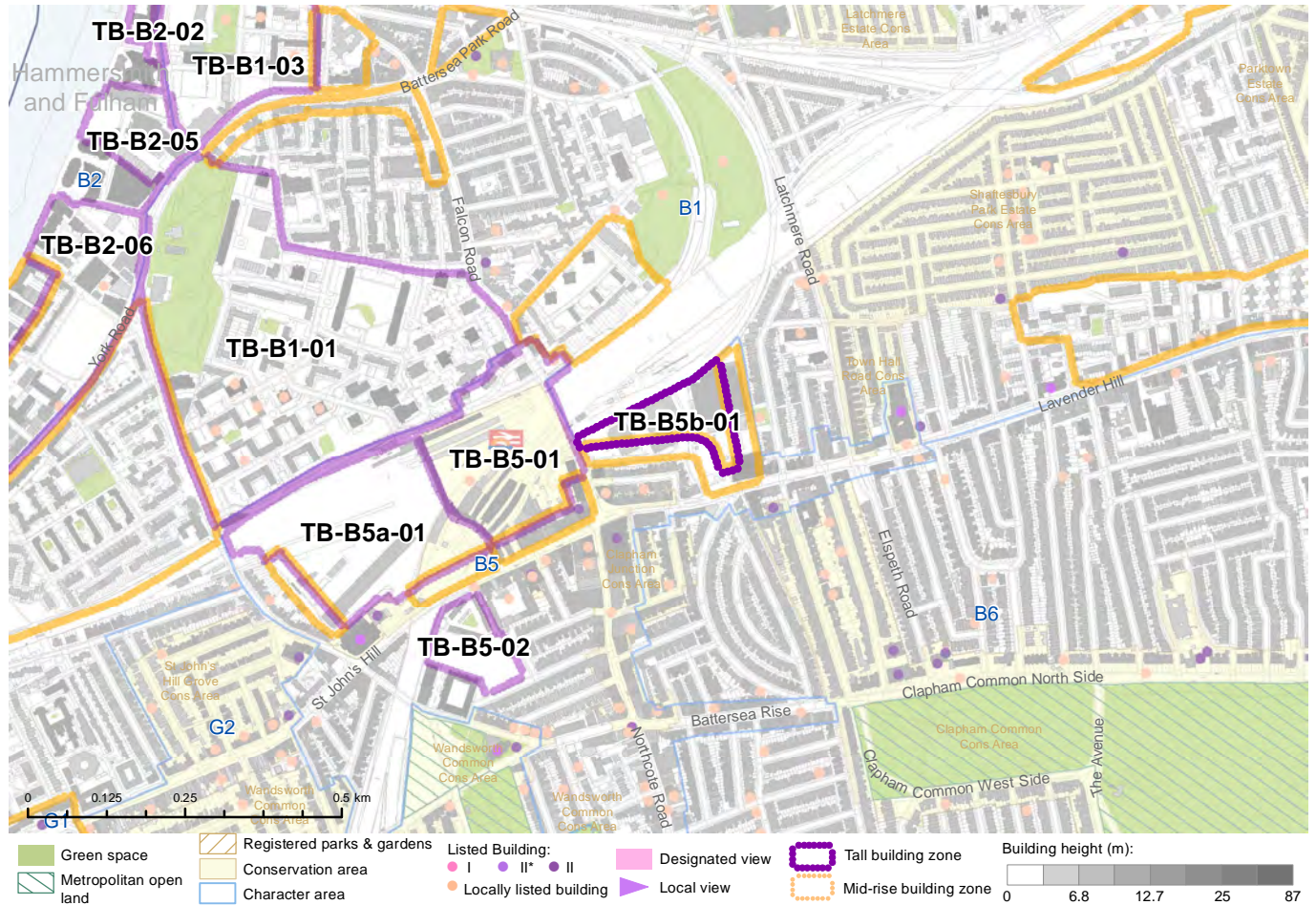


Fig. 315: TB-B5b-01 context map

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Fig. 316: Asda, Falcon Lane



Fig. 317: Boots, Falcon Lane

<b>TB-B5b-01</b>	
<b>Existing use</b>	Retail / commercial
<b>Existing/consented tall buildings</b>	• None
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Town centre (Clapham Junction);</li> <li>• Area Strategy: Clapham Junction and York Road/ Winstanley Regeneration Area.</li> <li>• Site allocation: CJ1 ASDA, LIDL and Boots sites, Falcon Lane.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Clapham Junction Conservation Area (B5).</li> <li>• Lavender Gardens (locally listed).</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• Clapham Common Conservation Area (B6).</li> <li>• Two grade II listed buildings (within Clapham Junction CA).</li> <li>• Two locally listed buildings, one within Clapham Common CA, one within Clapham Junction CA.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Shaftesbury park Estate Conservation Area (B1).</li> <li>• Town Hall Road Conservation Area (B1)</li> </ul>
<b>PTAL rating</b>	High (6b)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	B5 Clapham Junction Town Centre - (Conserve/ Restore)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• B6 Clapham Common and Residential</li> <li>• B1 Battersea Residential</li> <li>• G2 Wandsworth Common and Residential</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Very high
<b>Capacity</b>	Higher
<b>High density scenario</b>	
<b>Total residential units</b>	930
<b>Non Residential GEA</b>	9,700 sqm
<b>Density (dph)</b>	258

**Justification: Scenario**

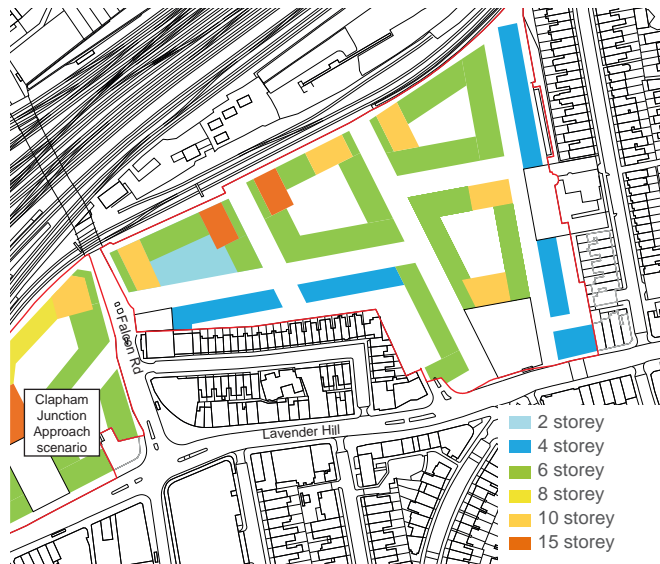
This scenario tests the appropriate building height of tall building zone TB-B5b-01. The potential for taller towers on this site was considered and found to give rise to adverse effects on townscape character, heritage assets and views. Therefore this scenario was developed on the basis of:

- Towers located along the railway corridor and away from Lavender Hill to avoid affecting their existing character and to reduce impacts on the setting of listed buildings.
- Reduction in building heights towards east and west to respect the existing low-rise residential buildings.
- The massing assumes removal of all existing buildings including Victoria signalling centre.

The strategy is to conserve and restore the character of the area, protecting its key characteristics and strengthening existing character by providing new

developments of high quality which respect the positive key details, materials and scale of the area.

This zone is noted as being a key location for targeted change to enhance the character, legibility and sense of arrival due to the presence of more detracting existing features, including modern large floorplate retail stores which this scenario develops.



**Fig.318:** Asda, Lidl and Boots sites scenario plan

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**Fig.320:** Asda, Lidl and Boots sites massing model



**Fig.319:** Asda, Lidl and Boots sites massing model

### ***Extent of visibility***

Widespread visibility along the extensive railway tracks and also along streets converging on Clapham Junction Town Centre including Northcote Road, Lavender Hill and Falcon Road. Extensive visibility from Clapham Common and parts of Wandsworth Common and Battersea Park. Intermittent visibility from smaller open spaces and also frontages along the north bank of the River Thames in neighbouring boroughs.



**Fig. 321:** Asda, Lidl and Boots sites scenario ZTV  
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### ***High level townscape, visual and heritage assessment***

#### ***Baseline***

The following valued features may be affected:

#### ***B5 Clapham Junction Town Centre:***

- Clapham Junction Conservation Area;
- the scale and overall coherence of the terraced buildings and Victorian/Edwardian shop houses and their settings;
- landmark listed buildings, including the Falcon Hotel Public House and the Arding & Hobbs Store.
- Views from the railway bridge on St John's Hill to Lavender Hill which could be substantially altered by tall buildings close to Lavender Hill.
- Views from the listed overbridge at Clapham Junction which is focused on the tall buildings of Central and west London. The scenario could dominate parts of the foreground and would require careful design and massing to ensure they don't detract from the longer distance views.

#### ***B6 Clapham Common and Residential:***

- the setting of Clapham Common with its distinct sense of openness;

- New taller buildings are likely to be visible within long distance views with wooded skylines from Clapham Common, necessitating careful design and placement / clustering to ensure they don't become dominant.

#### ***B1 Battersea Residential:***

- the openness and green character of Latchmere Recreation Ground within Latchmere Conservation Area;
- the character of Shaftesbury Park Estate Conservation Area.

The following townscape features/characteristics/views may be affected:

- The small-scale character of 2-3 storey period terraces along Dorothy Road to the east of the zone.
- The locally listed Lavender Gardens small open space along Dorothy Road, developed from a pocket of bomb damage following the war.
- The retained area of 2-3 storey period terraces along Mossbury Road and also facing onto Lavender Hill immediately south of the zone.
- Views from passengers as they pass through Clapham Junction station on foot and on train.
- The character of the Lavender Hill street scene characterised by 3 storey period shop-front terraces and landmark buildings at junctions such as the red brick Battersea Library.

#### ***Assessment***

#### ***Conclusion***

The massing illustrated along Falcon Road and behind the period terraces of Mossbury Road is likely to be broadly appropriate in principle. The tall building zone has therefore been set back in these locations behind a corresponding mid-rise building zone to ensure appropriate stepping down to the existing built fabric. The tall building zone is also set back from the period terraces and locally listed open space (Lavender Gardens) along Dorothy Road to the east. Development within these zones should be limited to five storeys and may not be able to exceed the four storeys illustrated in the scenario.

The tallest buildings within this zone should be located along the railway and set well back from surrounding retained smaller-scale buildings and the modest scale of Lavender Hill itself. The building heights shown in the scenario are considered more appropriate in principle than taller blocks reviewed, particularly in the context of Clapham Junction Conservation Area and landmark

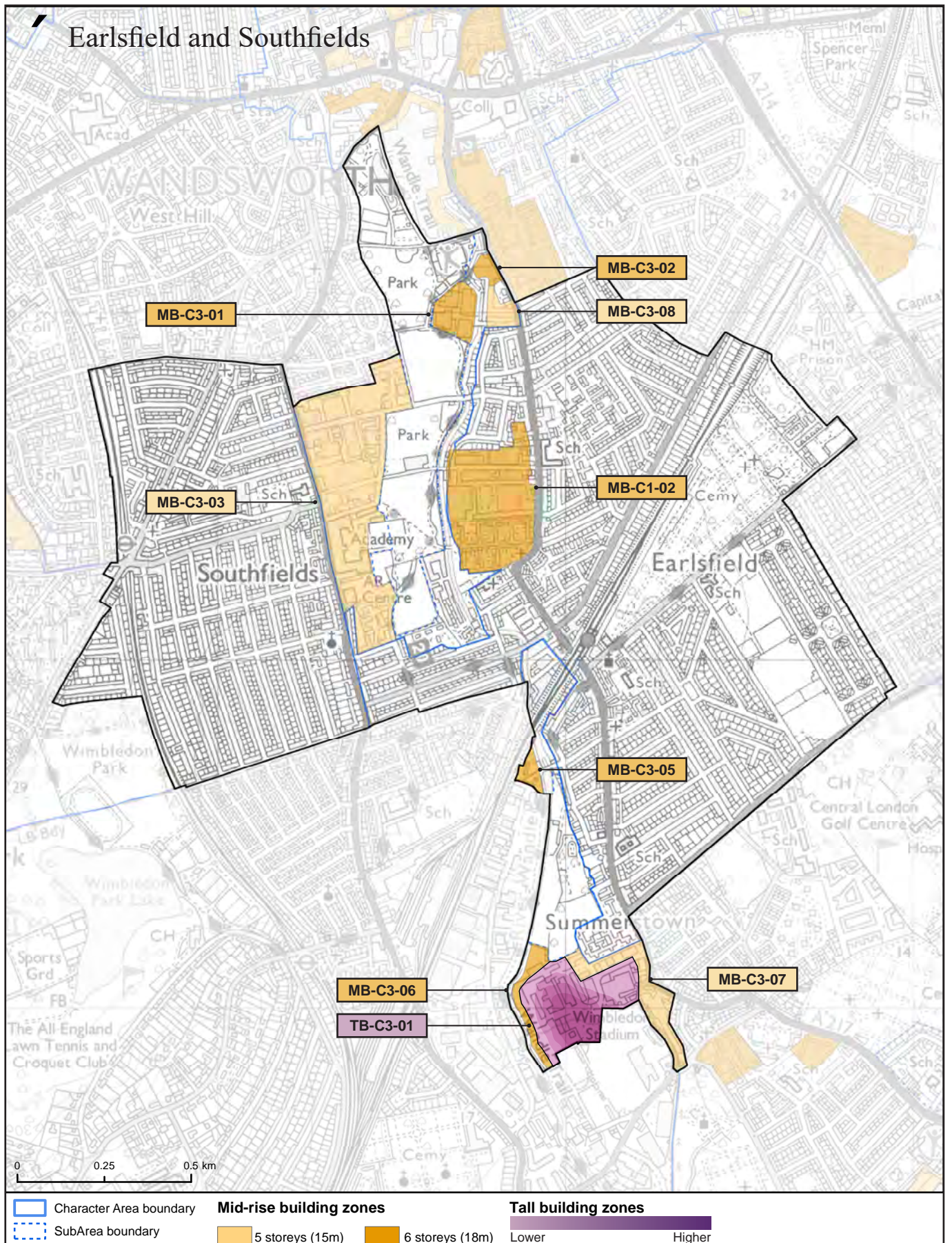
buildings. The location does not lend itself to a cluster of tall buildings, but rather one or two individual elements of appropriate heights.

Buildings in excess of 15 storeys within this zone may result in significant effects on the character of the town centre and setting of landmark buildings and heritage assets. In particular, it would sit uncomfortably with the 2-3 storey period terraces which surround the zone to the south and east.

Development along the railway should avoid creating a substantial "wall" of buildings affecting signature views such as the one from the overbridge in Clapham Junction which is experienced by huge numbers of people.

The heights of the scenario are considered to be potentially appropriate in principle for the area, pending further detailed analysis at a specific site scale. The tallest elements within the zones should all be located towards the railway to protect the historic character and street frontages of Lavender Hill and the period residential terraces. However, the pattern of development should avoid creating a canyon along the railway line, which would adversely affect the experience of users of the railway, including distinctive views towards central London from the overbridge.

None of the valued features, views or key features would be adversely affected by developments of up to 7-15 storeys in areas noted above if well designed and planned.



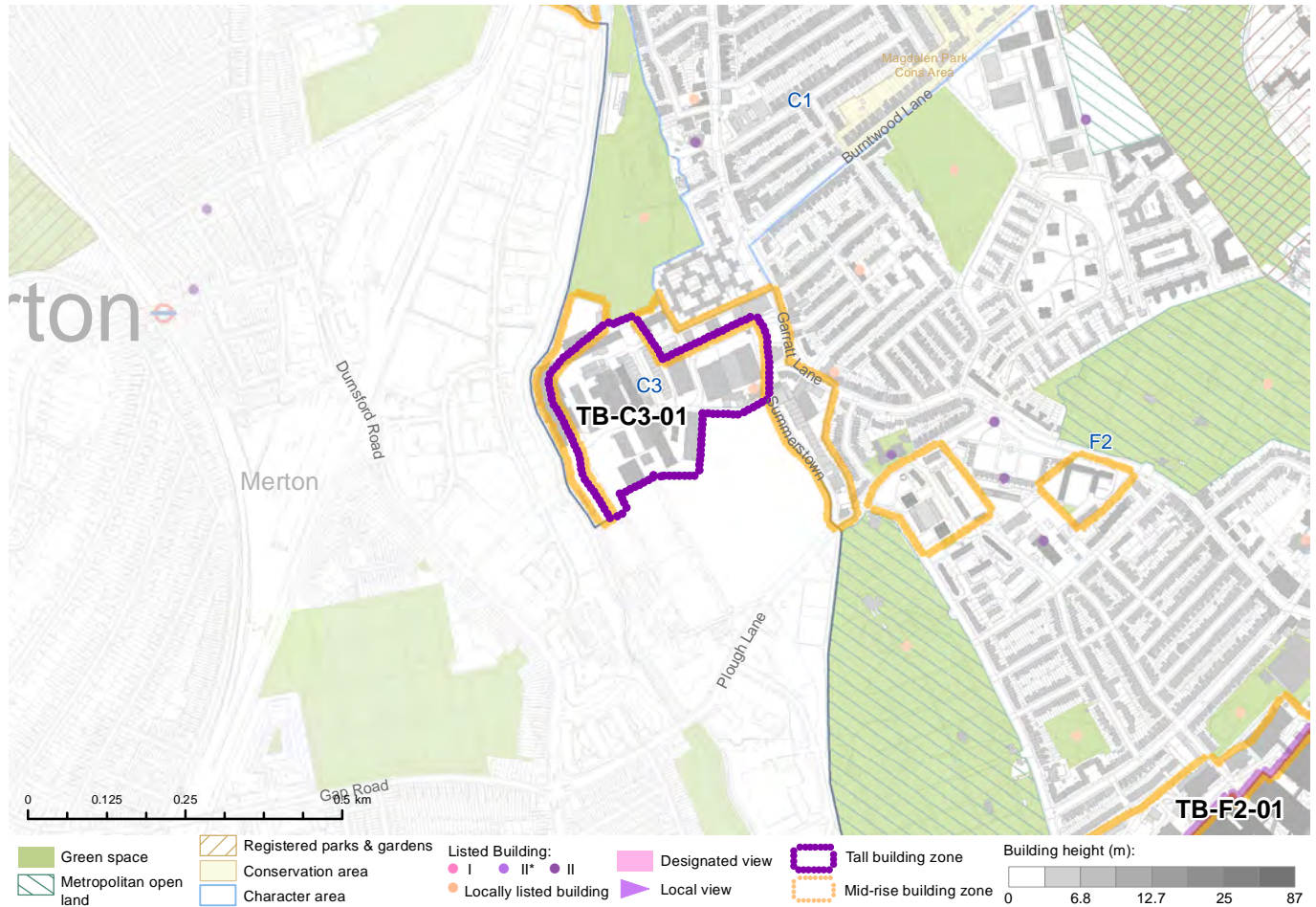
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## A.4 Earlsfield and Southfields justification

### A.4.1 TB-C3-01: Wandle Valley

Existing prevailing height: 2-3 storeys

Appropriate height: 7-10 storeys (21-30m)



**Fig. 323:** TB-C3-01 context map

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**Fig. 324:** The Corner Pin pub on the edge of the zone, with new development under construction within neighbouring LB Merton. In the foreground are two locally listed cottages dating from 1820.



**Fig. 325:** View east across the site from Riverside Road. Existing industrial buildings are mostly 2 storeys.

<b>TB-C3-01</b>	
<b>Existing use</b>	Industrial
<b>Existing/consented tall buildings</b>	• None
<b>Planning policy</b>	• Locally Significant Industrial Area. • Area Strategy: Wandle Valley.
<b>Designations (within 50m)</b>	• Locally listed building, within the zone. • Garratt Park (locally listed open space).
<b>Designations (within 100m)</b>	• The Wandle Trail.
<b>Conservation areas and high value designations within 500m</b>	• Magdalen Park Conservation Area (C1) • Wandle Valley Conservation Area (LB Merton) • Springfield Hospital grade II Registered Park and Garden (700m)
<b>PTAL rating</b>	Low-Medium (1b-2).
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	C3 Wandle Valley - (Improve)
<b>Other character areas within 250m</b>	• C1 Earlsfield Residential • F2 Tooting Residential
<b>Sensitivity</b>	Low
<b>Probability</b>	Low
<b>Capacity</b>	Lower

### *Justification: Existing and consented buildings*

Existing industrial buildings within the zone are 1-4 storeys, however, adjacent development at the Greyhound Track/Wimbledon AFC site (under construction) is up to 33m (10 storeys) in neighbouring LB Merton.

The character profile notes negative qualities in industrial areas such as this, particularly where the developments close proximity to the River Wandle leaves limited space for wildlife and no public access and which have little visual interest. The strategy for this area is therefore to improve the indistinct character through management and future planning. This should include improving the relationship with the River Wandle through access, building design and landscape interventions.

The tall building zone abuts small-scale residential streets to the east and the locally listed Garratt Park to the north.

### *Analysis*

While the surrounding context of Earlsfield is relatively small-scale in character, this particular zone is of sufficient size and depth to accommodate tall buildings in keeping with the height of the adjacent development in LB Merton. Provided these buildings step down towards the edges, and are well-designed in accordance with design principles in [Appendix A](#), they should not give rise to adverse townscape, visual or heritage



Fig.327: Aerial view

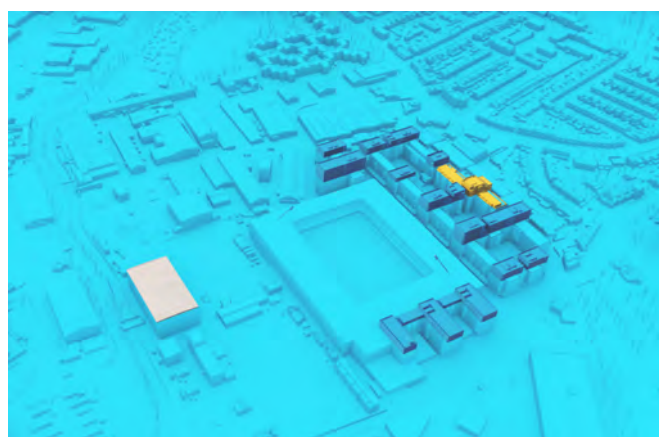


Fig.326: Consented tall buildings at the former greyhound racetrack and AFC Wimbledon stadium. Buildings under 21m shown masked in light blue.



Fig.328: ZTV of an illustrative 30m building in the eastern part of the zone  
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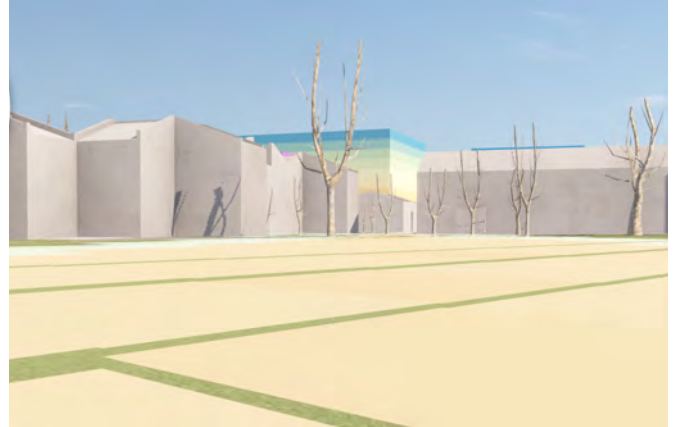
effects on the surrounding area.

The tall building zone has been set back from the River Wandle to ensure development does not dominate the natural character of this narrow watercourse. The tall building zone steps down towards a mid-rise buildings zone to further ensure this appropriate gradation.

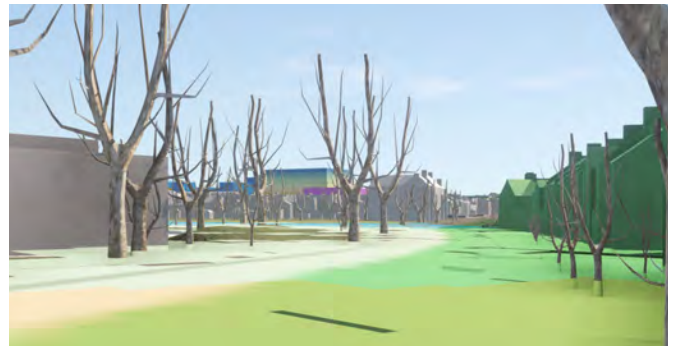
This similar approach is followed to the east of the zone, whereby buildings should step down towards Garratt Lane and the small-scale residential areas located further to the east.

The tallest buildings within this zone should be located within the plot and closest to the neighbouring development in LB Merton. Buildings taller than 10 storeys/30m would risk affecting the character of the nearby residential areas in addition to sensitive receptors such as the locally listed Garratt Park and Magdalen Road Conservation Area.

As long as any development is well planned and designed, heights of 21-30m are considered to be appropriate, in principle, for the zone.



**Fig. 330:** View from Garratt Park: the illustrative 30m block is visible above houses in the foreground. Any buildings with heights in excess of this risk significantly adversely affecting views from the park.



**Fig. 329:** View towards the zone from Magdalen Road Conservation Area (in green), with illustrative blocks of 10 storeys (in the centre, coloured yellow with blue top) and 7 storeys (to the right, purple) and the adjacent consented development in LB Merton (left, coloured blue).



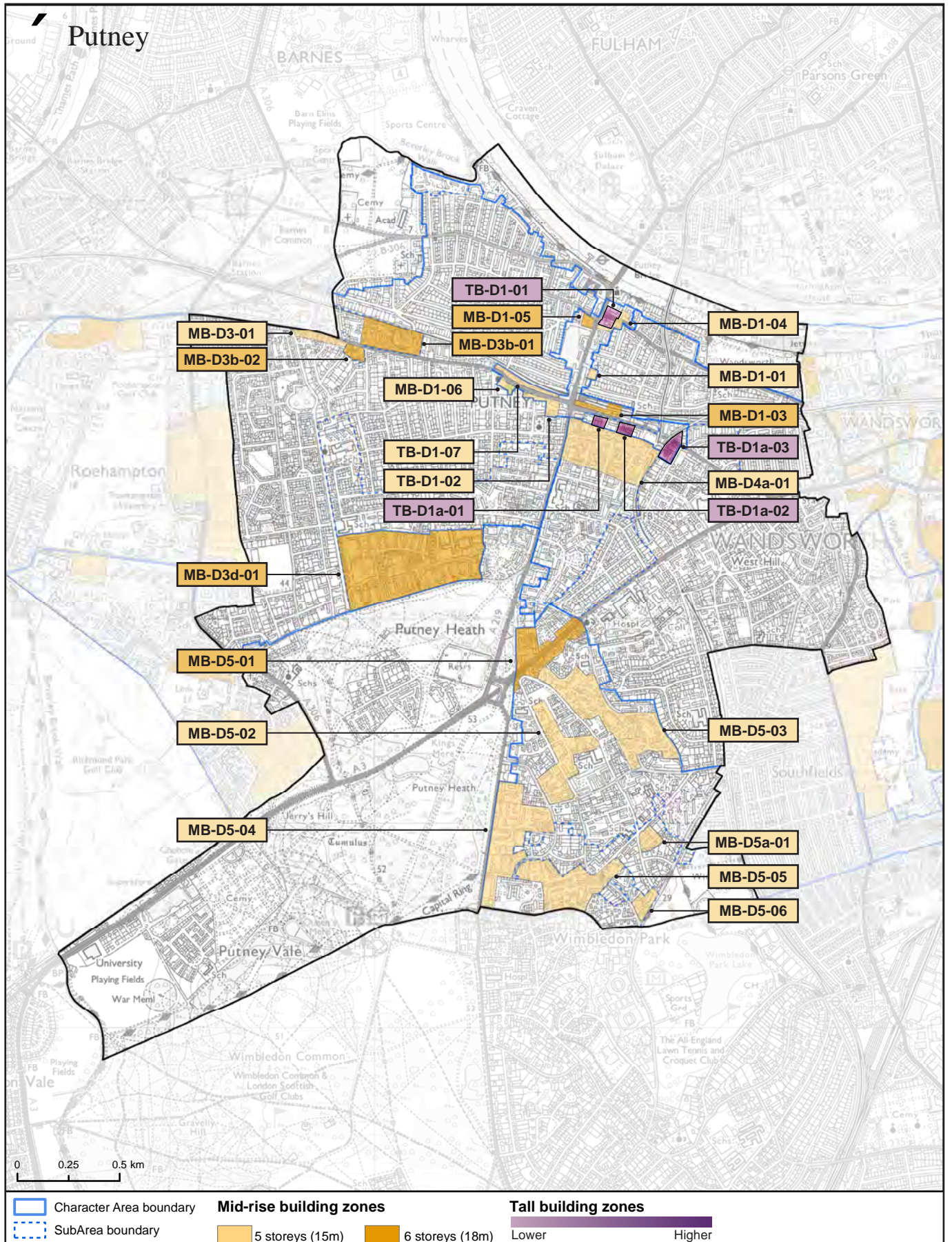


Fig. 331: Putney combined Tall and Mid-rise Buildings Zone map

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## A.5 Putney justification

### A.5.1 TB-D1-01: Putney Town Centre

Existing prevailing height: 3-9 storeys

Appropriate height: 7-8 storeys (21-24m)

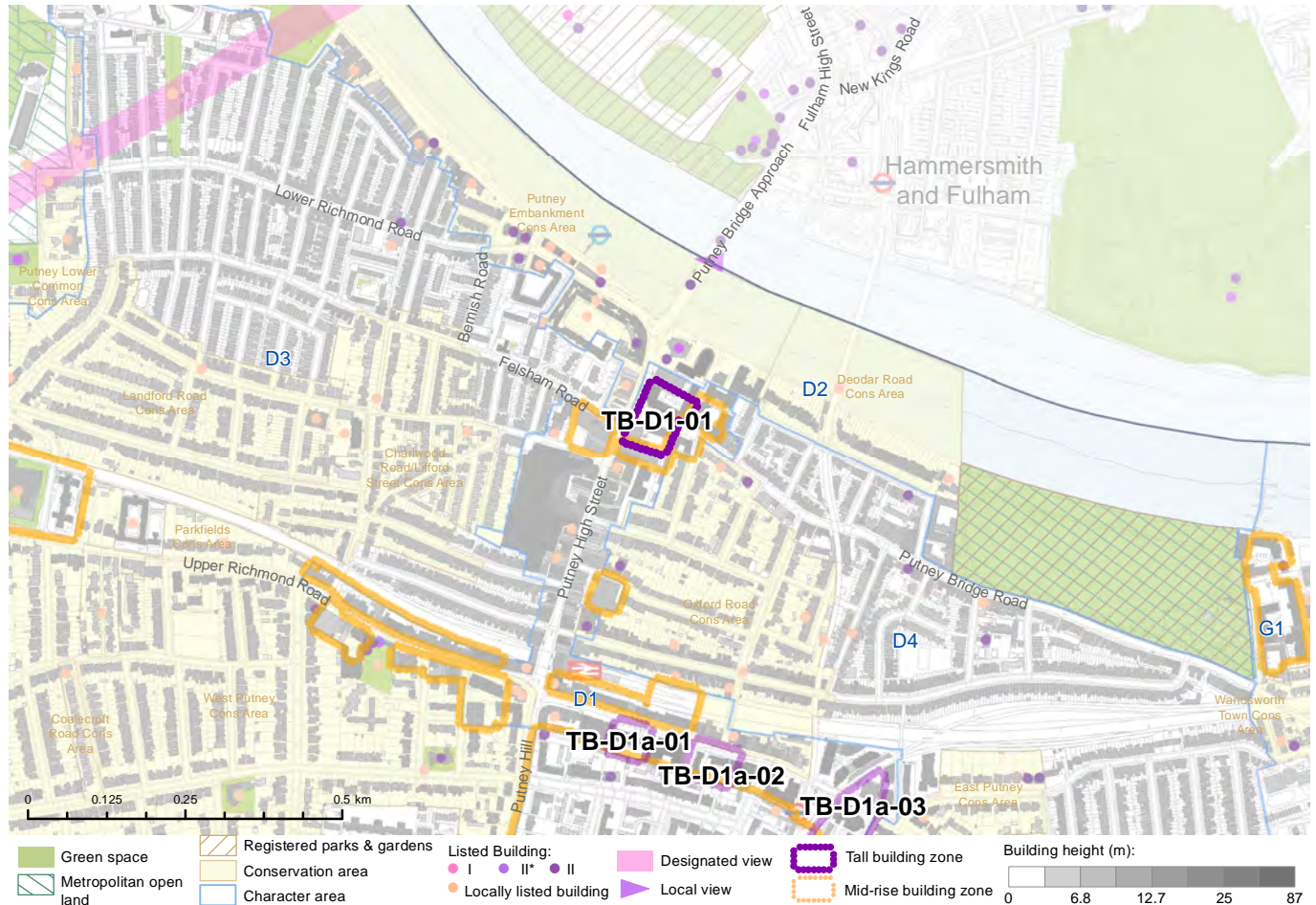


Fig. 332: TB-D1-01 context map

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Fig. 333: Retail units on Putney High Street, to the south of Putney Bridge Road



Fig. 334: View into the zone from Putney Bridge Road

<b>TB-D1-01</b>	
<b>Existing use</b>	Retail / commercial
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Office block on Brewhouse Lane (existing).</li> <li>329-339 and 45-53 Putney Bridge Road and Putney High Street.</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Town centre: Putney.</li> <li>Area Strategy: Putney.</li> <li>Thames Policy Area.</li> <li>Focal Point: Putney Wharf.</li> <li>Site allocation: PUT2 Jubilee House and Cinema; PUT3 Corner of Putney Bridge Road and Putney High Street.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Putney Embankment Conservation Area (D2).</li> <li>Oxford Road Conservation Area (D4).</li> <li>Putney War Memorial grade II listed structure.</li> <li>2 locally listed buildings on Putney High Street.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>Church of St Mary the Virgin grade II* listed building.</li> <li>White Lion Hotel Public House (grade II).</li> <li>Two locally listed buildings.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Deodar Road Conservation Area (D2).</li> <li>Wandsworth Park Registered Park and Garden (D2).</li> <li>West Putney Conservation Area (D3).</li> <li>Charlwood Road/Lifford Street Conservation Area (D3).</li> <li>Bishop's Park Registered Park and Garden (LB Hammersmith and Fulham).</li> <li>Fulham Palace Registered Park and Garden (LB Hammersmith and Fulham).</li> </ul>
<b>PTAL rating</b>	High (6a)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	D1 Putney Town Centre - (Restore)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>D2 Putney Riverside</li> <li>D3 West Putney Residential</li> <li>D4 East Putney Residential</li> </ul>
<b>Sensitivity</b>	Medium
<b>Probability</b>	Very high
<b>Capacity</b>	High
<b>High density scenario</b>	
<b>Site Area</b>	0.5 ha
<b>Total residential units</b>	154
<b>Non Residential GEA</b>	1,100sqm
<b>Density (dph)</b>	308

### Justification: Scenario

The Putney High Street Cluster scenario has been developed to test the tall building zone and appropriate building heights for the zone. The scenario consists of two sites: Putney Telephone Exchange and Jubilee House and Cinema. The scenario found that appropriate heights on the Putney Telephone Exchange site are between 2-6 storeys and therefore this site has not been defined as a tall building zone (it is included within mid-rise building zone MB-D1-01). The scenario for Jubilee House and Cinema site has informed the appropriate heights for TB-D1-01.

The scenario has been developed in line with the following principles:

- Positioning of the taller building elements facing Putney High street but away from St. Mary's Church to respect the setting of the listed building.
- Heights steps down towards existing low-rise residential buildings to the east of Telephone Exchange.

Regeneration in the area is planned, including the area in the Putney Wharf focal point. The character profile notes the taller blocks of Jubilee House and Putney Wharf Tower (within D2 Putney Riverside) detract from the consistency and character of the high street. The strategy is to restore existing character, ensuring future growth reinforces existing character.



Fig. 335: Putney High Street Cluster plan

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Fig. 336: View north towards the River Thames

### *Extent of visibility*

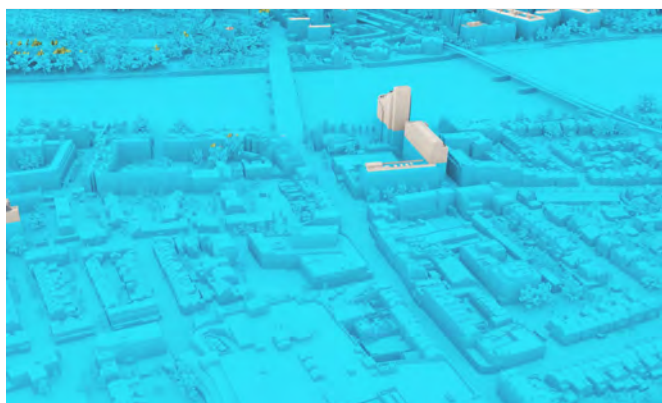
The ZTV indicates extensive visibility of the Jubilee House and Cinema site along the open expanse of the River Thames. Views would extend along Putney Bridge Road, partially into the Putney Embankment Conservation Area and part way south along Putney High Street. Visibility extends to the opposite side of the River Thames into LB Hammersmith and Fulham, including Bishop's Park and Fulham Palace Registered Parks and Gardens and Scheduled Monument.

Limited visibility within local streets (including Oxford Road CA, Charlwood Road/Lifford Street CA and Deodar Road CA) and in more distant views to the south from Putney Hill (including West Putney CA). Longer views from Wandsworth Park RPG are unlikely due to intervening built form and vegetation.



**Fig. 337:** Jubilee House and Cinema site scenario ZTV

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**Fig. 338:** Existing tall buildings in the area (buildings under 21m are shown masked in blue). Putney Wharf Tower (approximately 55m) and Jubilee House (approximately 32m and 9 storeys) extend far above the rest of the buildings in the area and have an adverse impact on the character of the high street and the listed St Mary's Church and Putney Bridge.

### *High level townscape, visual and heritage assessment*

#### **Baseline**

The following valued features, townscape characteristics and views may be affected:

#### **D1 Putney Town Centre:**

- Intact Victorian period shopfront terraces forming the dominant character of Putney High Street. Including the consistently modest proportions along Putney High Street, framed by 3-4 storey terraces; a distinct recognisable proportion to the street scene
- Views north along Putney High Street towards St Mary's Church, Putney Bridge and the River Thames.
- Views along side streets to the adjacent residential character areas of 2-3 storey properties in East and West Putney Residential, and a gradual stepping down in scale of built form.

#### **D2 Putney Riverside:**

- The setting of landmark listed buildings along the riverside close to Putney Bridge, particularly including St Mary the Virgin church, also Putney War Memorial;
- The setting of the grade II listed Putney Bridge;
- The open and green character of the river frontage.
- Views out from Putney Lower Common and Wandsworth Park, where the scenarios would be visible as part of the overall urban centre of Putney.
- The setting of Putney Embankment Conservation Area and Deodar Road Conservation Area.

#### **D3 West Putney Residential:**

- Unified streetscene of period houses, mature trees and front gardens;
- The setting of Charnwood Road / Lifford Street Conservation Area;

#### **D4 East Putney Residential:**

- The setting of Oxford Road Conservation Area.

#### **LB Hammersmith and Fulham**

- Bishop's Park and Fulham Palace Registered Parks and Gardens and Scheduled Monument.



**Fig. 339:** Putney High Street Cluster massing model

### Assessment

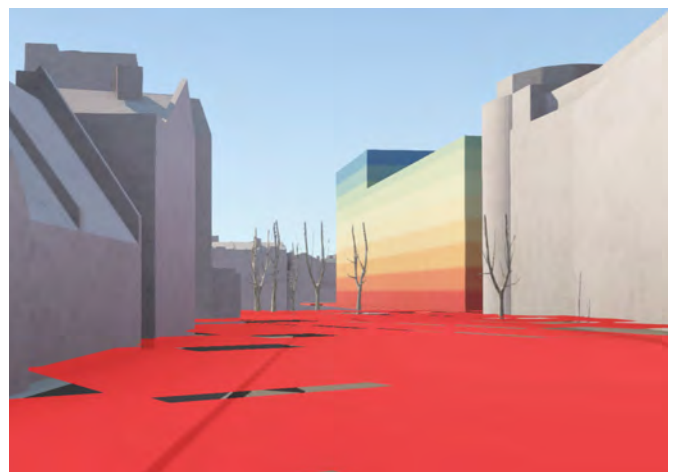
The character profile and site visits found that the heights of the existing Jubilee House (approximately 32m and 9 storeys) negatively impacts the character, views and setting of St Mary's Church. Tall buildings within the zone would be visible as part of the overall framing of the valued views towards St Mary's Church, Putney Bridge and the riverside, and heights in excess of 8 storeys may further obscure St Mary's Church which is already detrimentally affected by the presence of Putney Wharf Tower. The height and detailed massing and material choices on the corner closest to St Mary's Church requires the closest attention to detail to protect the remaining setting of the listed heritage asset.

Towards the east of the site, furthest from the High Street, heights should step down towards buildings on Brewhouse Lane. Extending the zone further east towards Burstock Road and east of Brewhouse Lane, risks any tall buildings having an impact on the low rise character, alms houses and Oxford Road CA, and on the Deodar Road CA.

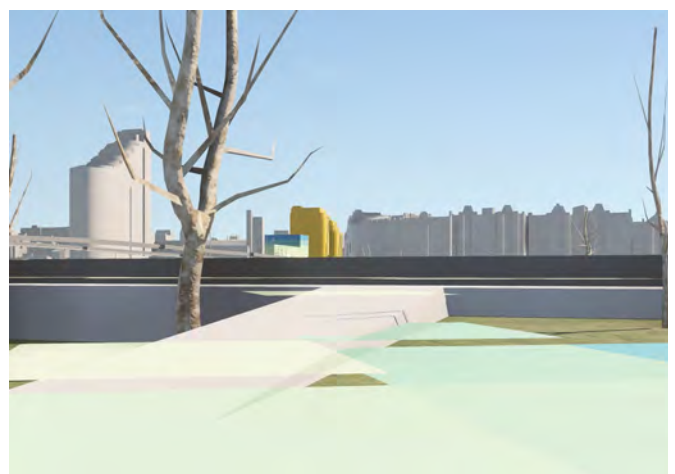
The heights illustrated are considered to be broadly appropriate in principle, although the massing of the Jubilee House and Cinema site should be sensitively designed to respect the character and heritage value of the church. Buildings should be set back from the street to provide public realm in this key location on the high street and maintain views towards the river. Heights in excess of 8 storeys are considered likely to give rise to adverse effects on the sensitive character and modest scale of the historic town centre.



**Fig. 340:** View north along Putney High Street towards the Church of St Mary the Virgin, Putney Bridge and the Parish Church of All Saints in LB Hammersmith and Fulham. The 8 storey element in the scenario fits in to the overall street scene and does not detract from the view or the two listed buildings.



**Fig. 341:** View west along Putney Bridge Road from the edge of the Oxford Road Conservation Area. The scenario steps up to the 8 storey element, allowing it to step down to the mid-rise building zone east of Brewhouse Lane, respecting the scale of the conservation areas, and in proportion with the overall street scene.



**Fig. 342:** View from Bishop's Park Registered Park and Garden - the scenario appears in proportion with the overall street scene. The 10 storey consented development rises above, but further buildings of this height risk negatively affecting the character of the street and balanced proportions in views.

## A.5.2 TB-D1a-01, TB-D1a-02 and TB-D1a-03: Putney Town Centre sub area D1a

Existing prevailing height: 6-11 storeys

Appropriate height: 7-11 storeys (21-33m)

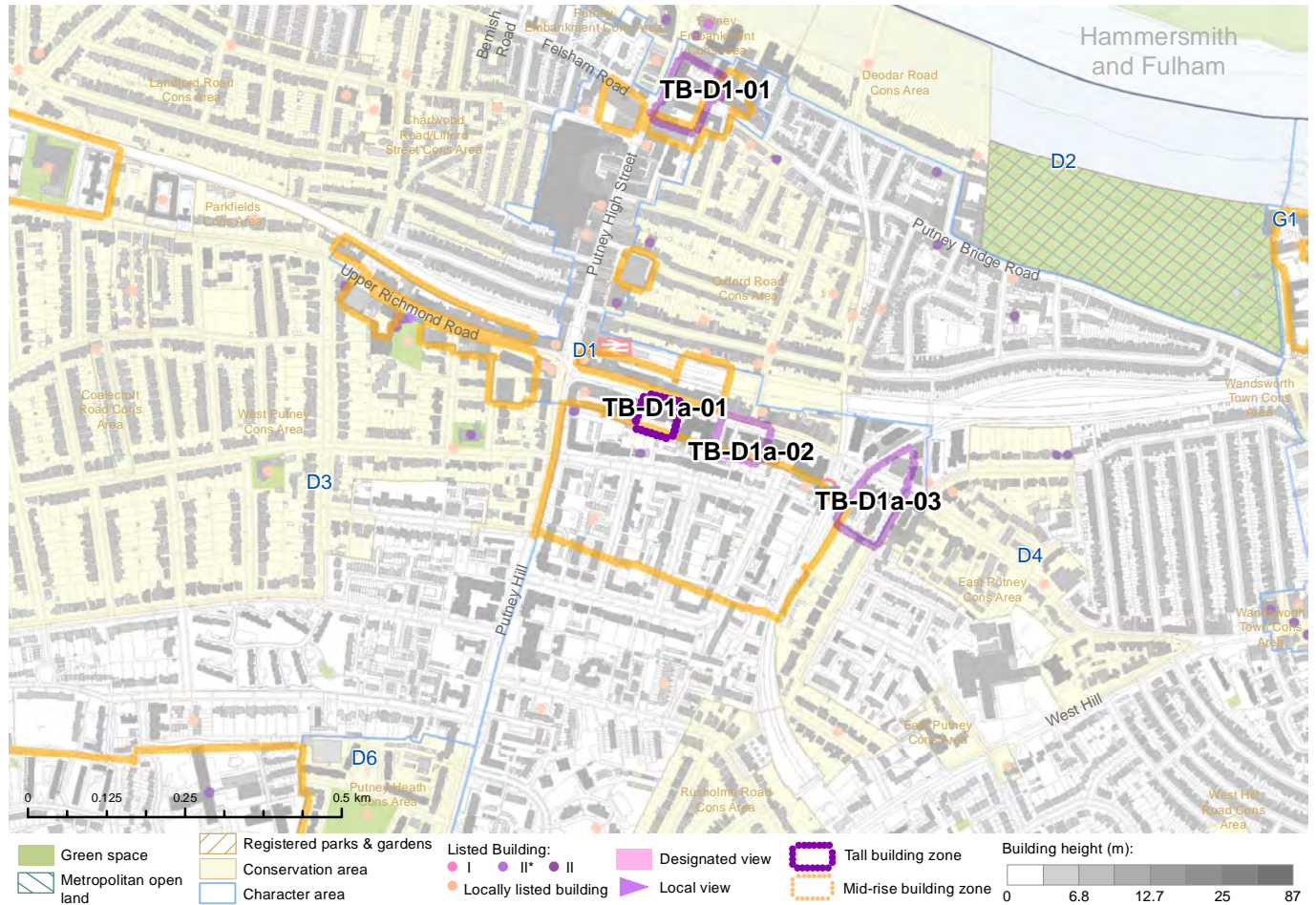


Fig. 343: TB-D1a-01 context map

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Fig. 344: Between the railway line and Woodlands Way



Fig. 345: Buildings up to 12 storeys on Upper Richmond Road near the junction with Carlton Drive

<b>TB-D1a-01</b>	
<b>Existing use</b>	Commercial/mixed use/residential
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>• Multiple blocks (existing).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Town Centre: Putney.</li> <li>• Area Strategy: Putney.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• East Putney Underground Station (locally listed), within the zone.</li> <li>• East Putney Conservation Area (D4).</li> <li>• Oxford Road Conservation Area (D4).</li> <li>• Rosslyn Tower (grade II).</li> <li>• 5 locally listed buildings.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• 4 locally listed buildings.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Rusholme Road Conservation Area (D4).</li> <li>• West Putney Conservation Area (D3).</li> <li>• Charlwood Road/Lifford Street Conservation Area (D3).</li> <li>• Deodar Road Conservation Area (D2).</li> <li>• Wandsworth Park Registered Park and Garden (D2).</li> </ul>
<b>PTAL rating</b>	Good - excellent (5-6b)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	D4 East Putney Residential - (Restore)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• D3 West Putney Residential</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	High
<b>Capacity</b>	Higher

### *Justification: Existing buildings*

This analysis tests the appropriate building height of tall building zone TB-D1a-01, using the existing buildings within the area, considered in the context of the surrounding built environment.

The area is characterised by a series of existing tall buildings from 6 to 11 storeys located along the south side of Upper Richmond Road and, in the east of the zone, extending north to the railway line around East Putney station. There is consent for a 12 storey building within this zone.

The strategy is to restore the character of the area which forms part of Putney Town Centre owing to its largely commercial land uses. However, this part of the town centre along the eastern extent of Upper Richmond Road has been identified as being of lower sensitivity owing to its large scale buildings which don't reflect the more historic grain found elsewhere in Putney.

The design guidance states that new design should seek to improve ground floor frontages and maintenance of façades, while creating wider pavements and seeking to reduce the dominance of cars.

### *Analysis*

Evidence gathered through site visits and the character area profile indicate that the taller development along this part of Upper Richmond Road is out of proportion with the surrounding period residential properties and present largely inactive frontages at ground floor. Therefore this zone has been drawn tightly around the existing area of tall buildings to restrict wider change in the area and in particular prevent further damage on Oxford Road Conservation Area which includes a run of period terraces on Upper Richmond Road itself.

Development in the east of the area should also respond positively to the nearby East Putney Conservation Area which is small-scale, residential and leafy in character.

Development along Upper Richmond Road should be no higher than existing blocks while providing a more positive address to the road through stepping down in height to the pavement, setting back further from the road to create more public realm and providing active frontages. Redevelopment of existing blocks would more positively respond to Oxford Road and West Putney Conservation Areas, and the overall character of Upper Richmond Road by limiting height at the front to no more than 5-6 storeys and stepping up within the plots, before stepping back down again in response to the mid-rise building zone to the south.

Development should ensure the character of the junction with Putney High Street is preserved, including through protecting the setting of and views towards corner plot landmark buildings.

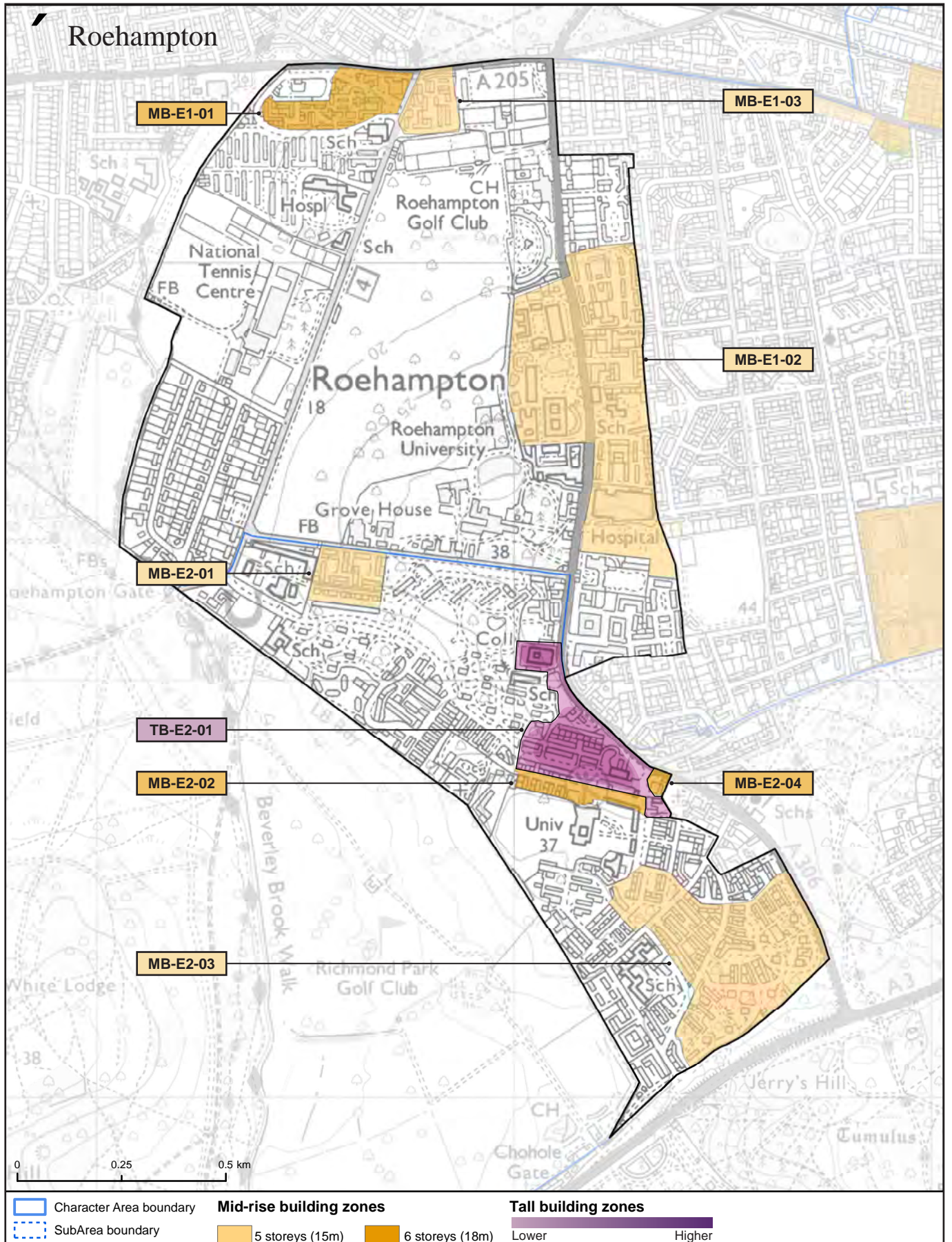


Fig. 346: Roehampton combined Tall and Mid-rise Buildings Zone map

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## A.6 Roehampton justification

### A.6.1 TB-E2-01: Roehampton Residential Estates

Existing prevailing height: 2-10 storeys

Appropriate height: 7-9 storeys (21-27m)

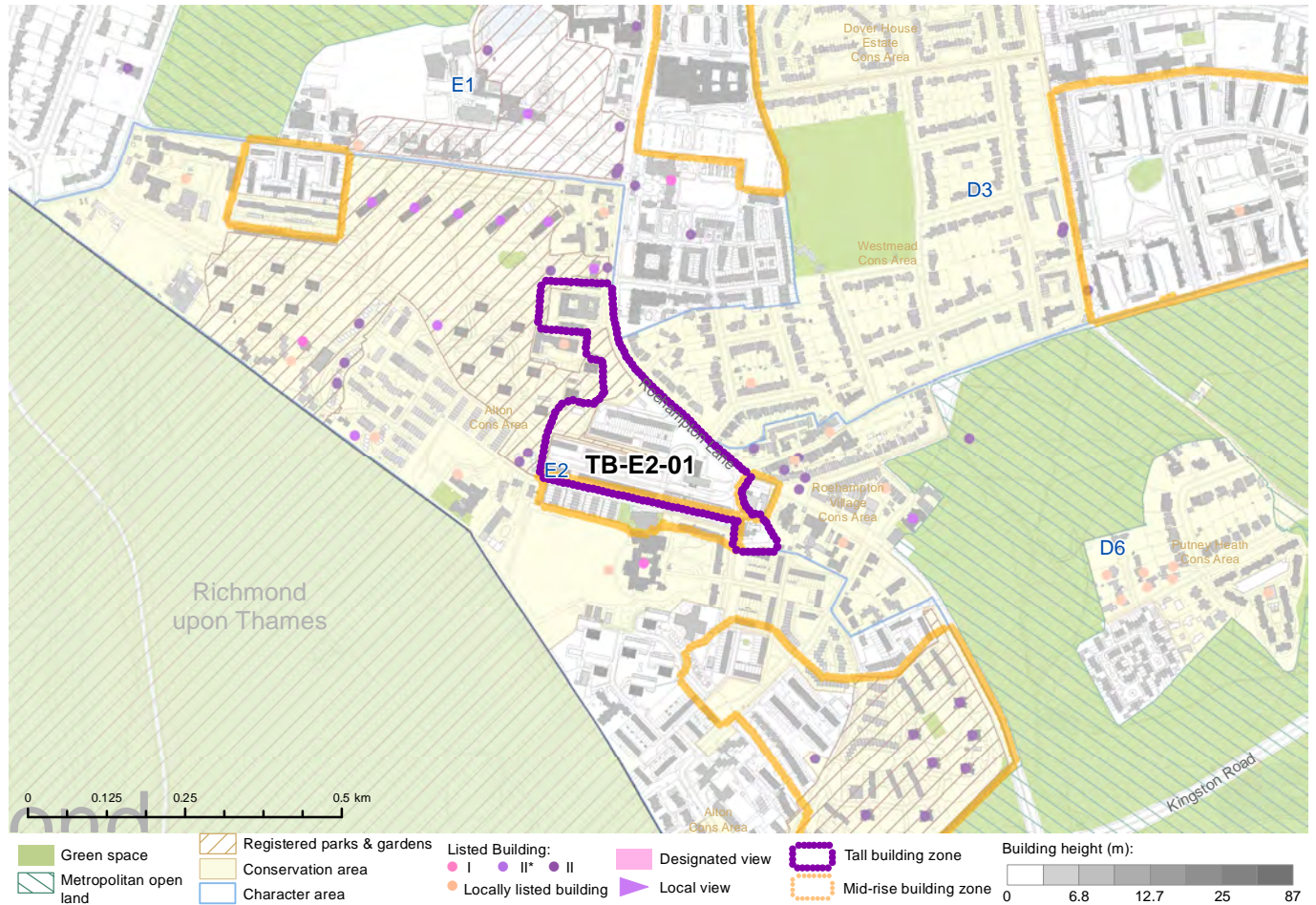


Fig. 347: TB-E2-01 context map

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Fig. 348: Blocks on Harbridge Avenue



Fig. 349: Roehampton library on Danebury Avenue

TB-E2-01	
<b>Existing use</b>	Residential area including estates and open spaces
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Residential block (over library), 30m (existing).</li> <li>9 storey buildings within Alton Estate masterplan (consented).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Local centre: Roehampton.</li> <li>Area Strategy: Roehampton.</li> <li>Site allocation: RO1 (Draft) Alton West Intervention Areas.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Within the zone: Alton Conservation Area (E2); Alton historic park and garden (local listing); Hartfield House Care Home (locally listed).</li> <li>Roehampton Village Conservation Area (D6).</li> <li>Westmead Conservation Area (D3).</li> <li>Parkstead House (grade I) (E2).</li> <li>Downshire House (grade II*).</li> <li>5 listed buildings (grade II).</li> <li>Parkstead House gardens (locally listed).</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>4 grade II listed buildings and one locally listed building within Roehampton CA.</li> <li>Maryfield Convent (locally listed).</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Richmond Park Registered Park and Garden (LB Richmond upon Thames).</li> <li>Mount Clare (grade I) (E2).</li> <li>Grove House Registered Park and Garden (E1).</li> <li>Roehampton House (grade I) (E1).</li> </ul>
<b>PTAL rating</b>	Medium (3)
Urban design study	
<b>Character area - (and strategy)</b>	E2 Roehampton Residential Estates (Conserve/ Transform)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>D3 West Putney Residential</li> <li>D6 Putney Heath and Vale</li> </ul>
<b>Sensitivity</b>	Medium
<b>Probability</b>	Very high
<b>Capacity</b>	Higher

### Justification: Consented masterplan

The tall building zone is covered by the area of the consented Alton Estate masterplan. The masterplan was supported by an Environmental Statement, including a Heritage, Townscape and Visual Amenity assessment (HTVA Chapter 7, Barton Willmore, May 2019). This assessment and the accompanying verified views have been analysed alongside the urban design study and site visits for analysis of the existing buildings on the site.

The character profile notes the influence of the historic landscape character on this area, including the legacy of Georgian country estates such as grade I listed Parkstead House to the south of the zone. The character of the area has retained a parkland setting which is important to the historic context and local distinctiveness.

The character profile notes the area could be improved, and the strategy for the character area is to conserve/ transform. The character of the area has become degraded over time and unrecognisable from its former state. However, the strategy should also conserve

and enhance the many positive assets of the area. It has been identified as an area for regeneration by the Council.

Within the zone the library building and Allbrook House reaches over 30m whilst the existing blocks are 4 storeys. The taller buildings are noted as detractors to the area in the character profile.

Valued features that may be affected include:

- Alton Conservation Area and its setting;
- Alton historic park and garden (locally listed);
- The sense of openness from the parkland setting;
- The setting of grade I Parkstead House;
- The setting of Roehampton Village Conservation Area including views of the Church of the Holy Trinity;
- The setting of Richmond Park Registered Park and Garden, including views from the park and appearance of buildings on the skyline.

### Analysis

The HTVA finds no significant effects on built heritage or townscape from the proposed masterplan development. Two significant (beneficial) effects on views were found, for views from Roehampton Lane looking west (Viewpoint 4) and for views from Ellisfield Drive within the Alton Conservation Area (Viewpoint 12).

Existing 12 storey towers in the adjacent Alton West estate are visible above the tree line from a number of sensitive viewpoints including Richmond Park, and look incongruous with the small scale and relatively suburban character of the surrounding area. Therefore heights in excess of 9 storeys would be considered inappropriate.

View 04 proposed



**Fig. 350:** View west along A306 Roehampton Lane (Viewpoint 4 from the HTVA, Barton Willmore, 2019). The assessment finds the new buildings will introduce a beneficial change.

View 10 proposed



**Fig. 355:** View south east from A306 Roehampton Lane, at the boundary between Alton Conservation Area and Westmead Conservation Area (Viewpoint 10 from the HTVA, Barton Willmore, 2019).

View 07 proposed



**Fig. 352:** View from Roehampton Recreation Centre within the conservation area. The assessment finds a negligible effect.

View 12 proposed



**Fig. 351:** View from Ellisfield Drive (Viewpoint 12 from the HTVA, Barton Willmore, 2019). The assessment finds the new buildings will introduce a beneficial change.

View 22 proposed



**Fig. 353:** View from Richmond Park (Viewpoint 22 from the HTVA, Barton Willmore, 2019)- the buildings do not rise above the tree line.

View 02 proposed



**Fig. 354:** View from Medfield Street on the edge of Putney Heath (Viewpoint 2 from the HTVA, Barton Willmore, 2019). Buildings will form a new skyline.

View 05 proposed



**Fig. 356:** View from Roehampton Hight Street, within the conservation area (Viewpoint 5 from the HTVA, Barton Willmore, 2019).

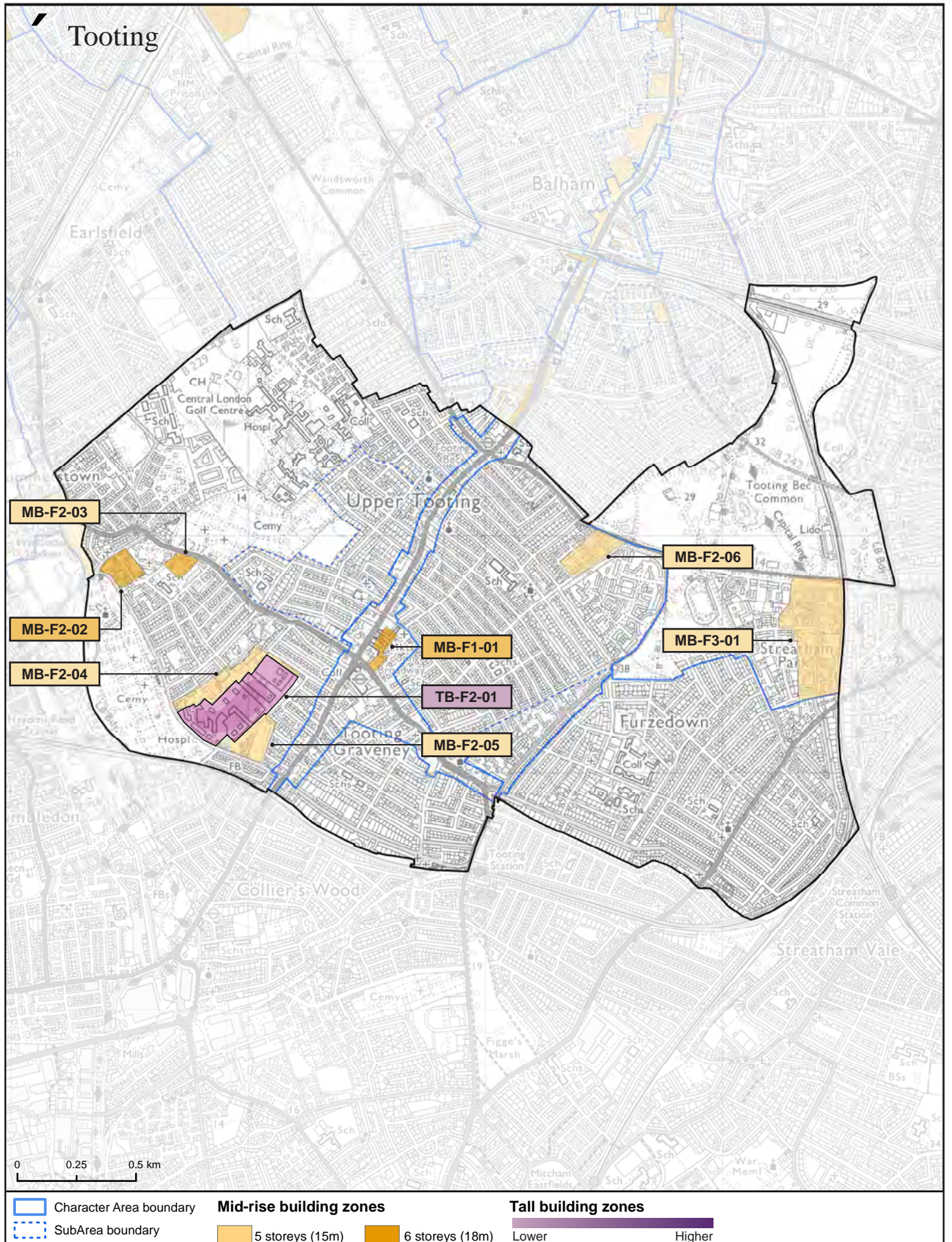


Fig. 357: Tooting combined Tall and Mid-rise Buildings Zone map

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## A.7 Tooting justification

### A.7.1 TB-F2-01: Tooting Residential

Existing prevailing height: 2-13 storeys

Appropriate height: 7-8 storeys (21-24m)

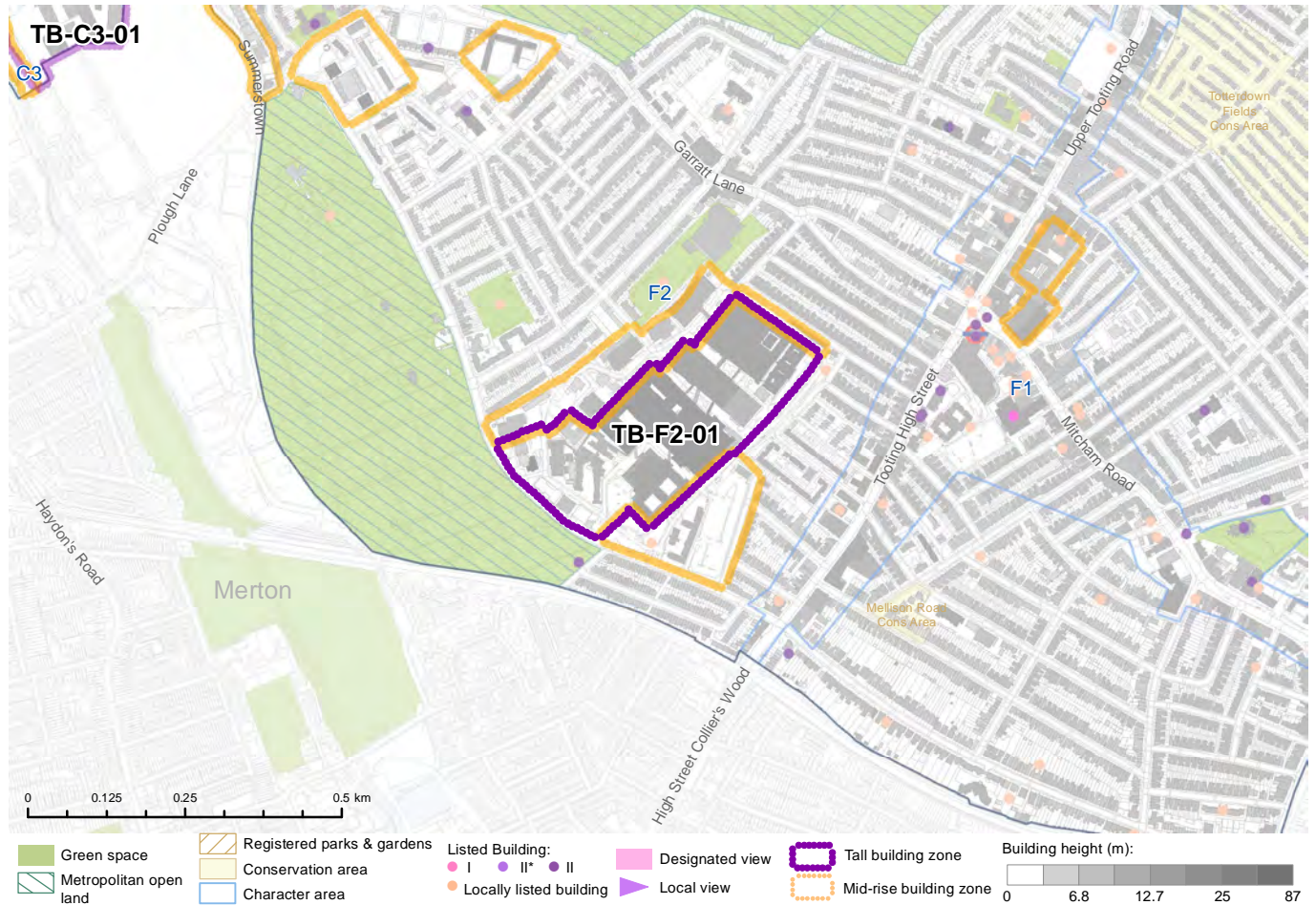


Fig. 358: TB-F2-01 context map

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Fig. 359: View from Blackshaw Road to the south of the zone



Fig. 360: Looking into the zone from the north west

<b>TB-F2-01</b>	
<b>Existing use</b>	Hospital and car park
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Hospital buildings (existing).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Area Strategy: Tooting.</li> <li>Site allocation: TO2 St George's Hospital, Blackshaw Road.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Lambeth Cemetery Metropolitan Open Land.</li> <li>Locally listed structure in the grounds of St George's Hospital.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>Tooting Gardens (locally listed and protected open space).</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Gala Bingo Club (grade I) (F1).</li> <li>Melison Road Conservation Area (F2).</li> </ul>
<b>PTAL rating</b>	Medium (3-5)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	F2 Tooting Residential - (Improve)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>F1 Tooting Town Centre</li> </ul>
<b>Sensitivity</b>	Medium
<b>Probability</b>	Medium-very high
<b>Capacity</b>	Medium-high
<b>High density scenario</b>	
<b>Site Area</b>	2.5 ha
<b>Total residential units</b>	600
<b>Non Residential GEA</b>	0sqm
<b>Density (dph)</b>	240

### Justification: Scenario

This scenario has been developed to test the appropriate building height of tall building zone TB-F2-01. The character profile notes the area around the hospital as having a larger footprint than the rest of the 2-3 storey period terraces of the character area, although complemented by large green spaces of the cemeteries nearby. Near the zone 3-4 storey mansion blocks provide a transition in scale down to the smaller terraces. The character area is fragmented in places, and the character area strategy is to improve character, particularly along main roads.

The scenario has been developed in line with the following principles:

- The positioning of taller buildings towards the centre of the site facing the Lambeth cemetery and away from existing, low-rise residential buildings.
- Reduction in building heights towards east and north west to respect the existing low-rise residential buildings.

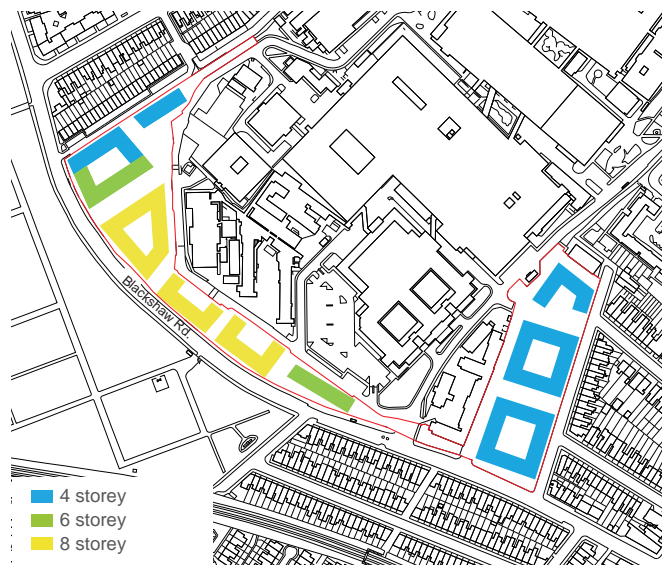


Fig. 361: St George's Hospital Car Park plan

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Fig. 363: The existing zone is occupied by the hospital, including a 14 storey tower which has a negative impact on the surrounding townscape



Fig. 362: St George's Hospital Car Park massing model

### Extent of visibility

The ZTV indicates that within the 2-3 storey residential streets surrounding the zone, visibility of the scenario is relatively contained to the streets with a direct line of sight, including along Blackshaw Road, and the upper parts of Broadwater Road and Fishponds Road. There

is extensive visibility of the scenario from Lambeth Cemetery to the west and the northernmost parts of Streatham Cemetery to the north. Visibility extends beyond the borough into residential areas within LB Merton.



**Fig. 365:** St George's Hospital Car Park scenario ZTV  
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### High level townscape, visual and heritage assessment

#### Baseline

The following townscape features/characteristics/views and valued features may be affected:

#### F2 Tooting Residential:

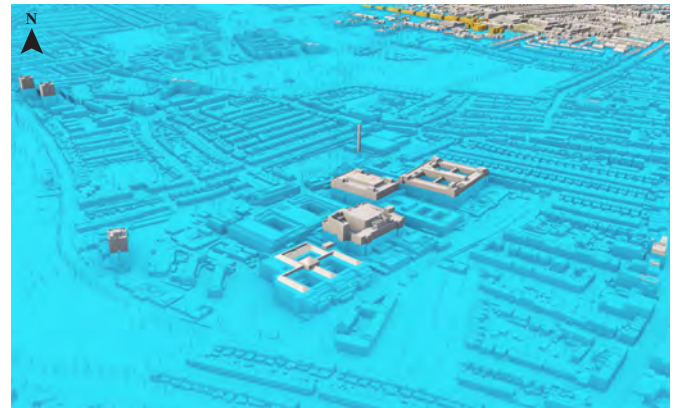
- Mellison Road Conservation Area;
- The open character of Lambeth Cemetery.
- Views downhill towards the hospital from the northern end of Fishponds Road, along tree-lined street. The hospital site is a major feature in the view, but development on the southern side as in the scenario is unlikely to be visible unless much higher.

#### Assessment

Existing tall buildings of 24m at the northern edge of the zone were noted on site as being imposing on the street, contrasting with the consistent streets of 2-3 storey terraces to the north and north east. Were the buildings to step down to the surrounding streets, they would be likely to integrate better with character and views.

The 14 storey tower block was also found to be overly intrusive on the character of the immediate area and longer views along Blackshaw Road. The illustrated heights of the scenario step down on all sides to surrounding buildings and would therefore not negatively impact local character and views.

The heights are found to be appropriate in principle in the context of the location along the main road, the hospital and opposite the large open space of the cemetery. None of the sensitive and valued features are likely to be significantly adversely affected by the heights indicated, if well-designed. Buildings should be set back from the road behind an area of landscaping/trees/boundary walls which should create a distinct and continuous frontage to the road.



**Fig. 367:** Existing tall buildings within the zone (buildings under 21m are shown masked in blue).



**Fig. 366:** St George's Hospital Car Park massing model



**Fig. 364:** Hospital site viewed from Fishponds Road. The scenario is visible in the distance, with only the top of it protruding above the surrounding townscape, creating a balanced composition in the view.

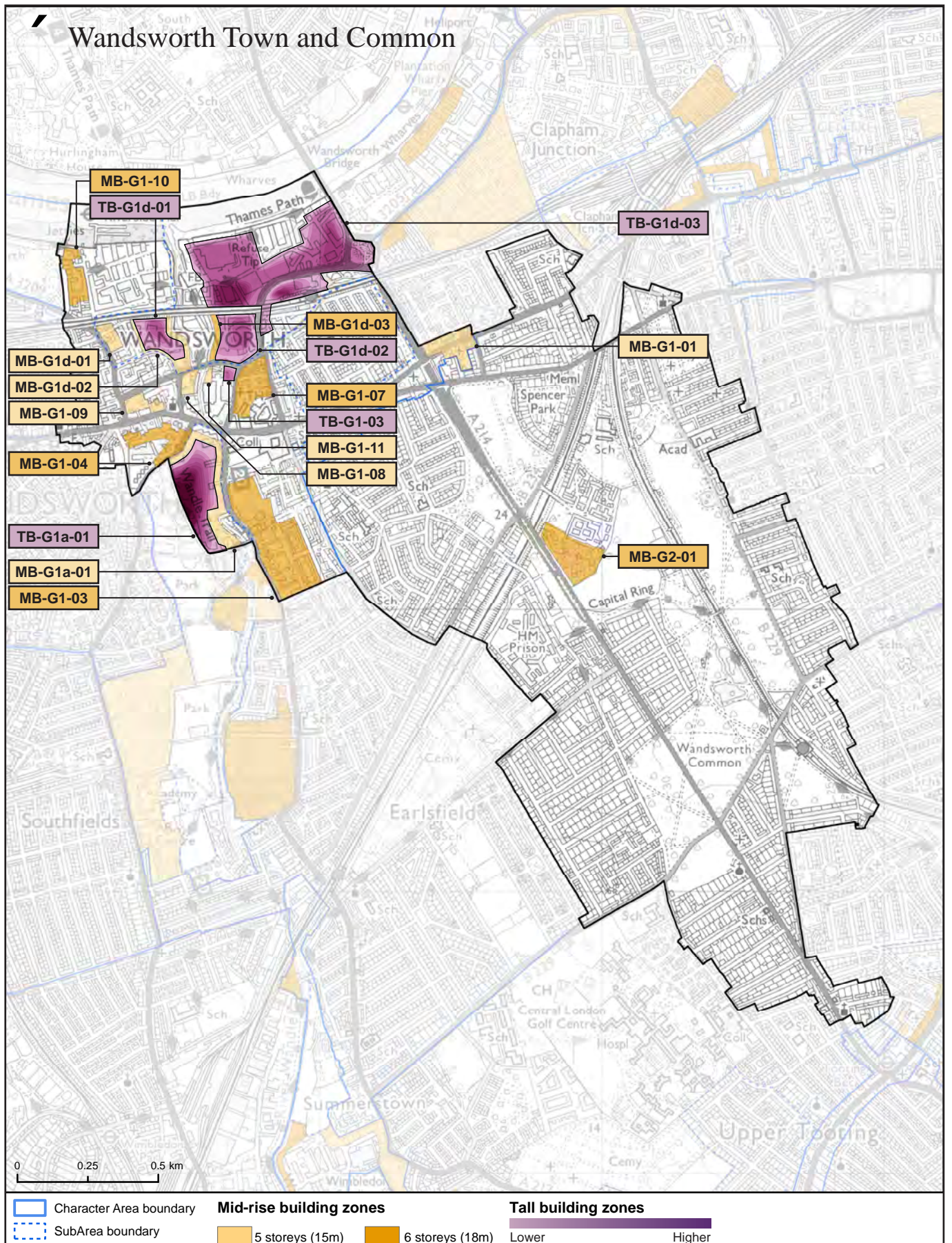


Fig. 368: Wandsworth Town and Common combined Tall and Mid-rise Buildings Zone map

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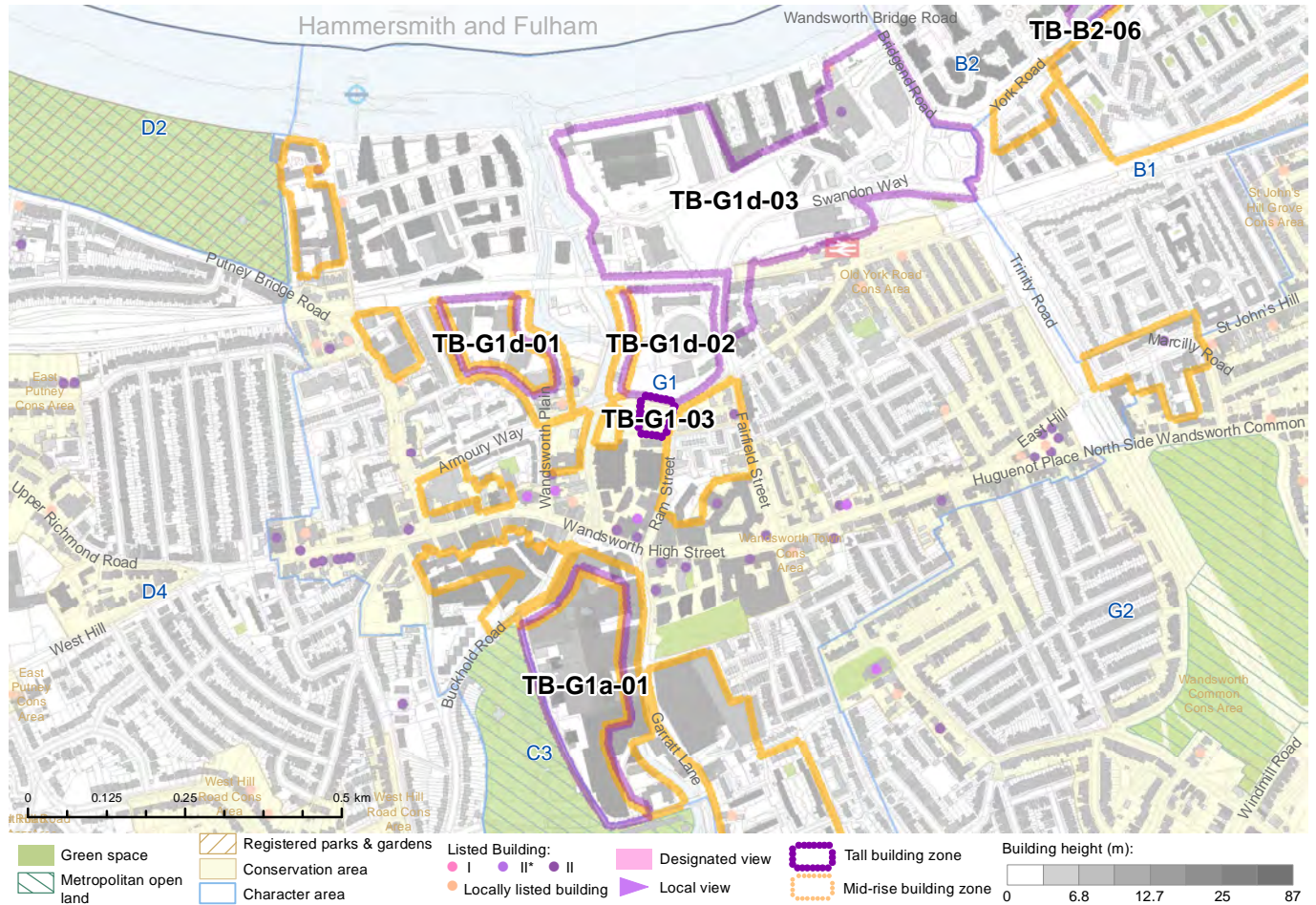


## A.8 Wandsworth Town and Common justification

### A.8.1 TB-G1-03: Wandsworth Town and Riverside

Existing prevailing height: 3-10 storeys

Appropriate height: 7-10 storeys (21-30m)



**Fig. 369:** TB-G1-03 context map

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**Fig. 370:** View into the zone to the Ram Brewery development from Old York Road (A3) to the north



**Fig. 371:** View into the zone from Wandsworth High Street the south

TB-G1-03	
<b>Existing use</b>	Residential/mixed use
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Multiple buildings of Ram Quarter (existing).</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Town Centre: Wandsworth Town</li> <li>Area Strategy: Wandsworth Town</li> <li>Site allocation: WT2 Ram Brewery/Capital Studios/Former Dexion/Duval site, Ram Street/Armoury Way.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Wandsworth Town Conservation Area (within the zone) (G1).</li> <li>Ram Brewery (grade II*).</li> <li>3 listed buildings (grade II) to the south and north of the site.</li> <li>1 locally listed building to the west of the site.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>Church Row (grade II*).</li> <li>5 listed buildings (grade II).</li> <li>2 locally listed buildings.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Old York Road Conservation Area (G1).</li> <li>Wandsworth Common Conservation Area (G2).</li> </ul>
<b>PTAL rating</b>	Good (5-6a)
Urban design study	
<b>Character area - (and strategy)</b>	G1 Wandsworth Town and Riverside - (Restore)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>C3 Wandle Valley</li> </ul>
<b>Sensitivity</b>	Medium
<b>Probability</b>	Medium-high
<b>Capacity</b>	Medium-high

### *Justification: Existing buildings*

This analysis tests the appropriate building height of tall building zone TB-G1-03, using the existing buildings within and in close proximity to the area, considered in the context of the surrounding built environment.

The area is a largely empty development plot adjacent to a series of newly developments between 4 and 10 storeys, set between the grade II\* Ram Brewery Complex (including the grade II listed Brewery Tap) and the grade II listed Ram Brewery Complex Stables. The historic buildings are between 2 and 5 storeys and include a landmark brick chimney along Ram Street. The new developments step up away from these retained listed buildings towards the core of the zone and closest to the River Wandle running along the western boundary.

The strategy is to restore the character of the area which sits within Wandsworth Town Centre. The character profile notes the area as having many elements with a strong and distinctive character, but that these have been degraded in places through inappropriate development. Part of the zone is located within Wandsworth Town Conservation Area.

The overall strategy is therefore noted as improving the maintenance of characteristic features, restoring historic features and ensuring future growth reinforces the historic character.

The design guidance states that new design should seek to retain and enhance the prominence of existing landmark and historical buildings, respect the small scale of the River Wandle corridor and set back buildings taller than 4 storeys well within plots to preserve the overall character of the old town.

### *Analysis*

Evidence gathered through site visits and the character area profile indicates that the existing developments within this zone are at an appropriate height for the area, generally with appropriate stepping down towards the street frontage and retained historic buildings. The tallest building in the area immediately around the zone (10 storeys) sits within the centre of the plot and responds positively to the surrounding character. Buildings of up to 8 storeys sit quite close to the narrow River Wandle character and would have benefited from providing this environment with more space to reduce overshadowing. The River Wandle is also designated as a Site of Importance for Nature Conservation.

The area to the west of this zone is likely to be developed but is not considered appropriate for tall buildings due to the presence of the grade II listed Ram Brewery stables.

Further development of this remaining plot within the zone should therefore follow the principles of positive set backs and step down towards the listed buildings, the River Wandle and the mid-rise building zone located to the east. Individual buildings should assess their impact on views from terraced residential streets further to the east. Development should also respect views along Wandsworth High Street which, while wide and dominated by cars, is characterised by small scale and generally older buildings, including the nearby Town Hall.

## A.8.2 TB-G1a-01: Wandsworth Town and Riverside

Existing prevailing height: 3-20+ storeys

Appropriate height: 7-20 storeys (21-60m)



Fig. 372: TB-G1a-01 context map

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Fig. 373: View into the southern part of the zone from St George's Park



Fig. 374: View into the zone from the junction of Buckhold Road/Neville Gill Close to the north west

<b>TB-G1a-01</b>	
<b>Existing use</b>	Shopping centre
<b>Existing/consented tall buildings</b>	• Towers within the zone (as shown on <a href="#">Fig.356</a> ).
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Town centre: Wandsworth.</li> <li>• Area Strategy: Wandsworth.</li> <li>• Site allocation: WT20 Southside Shopping Centre.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• King George's Park Metropolitan Open Land and Wandle Trail.</li> <li>• Wandsworth Town Conservation Area (G1).</li> <li>• One grade II listed building, to the north east of the zone.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>• 3 grade II listed buildings, within Wandsworth Town CA.</li> <li>• 2 locally listed buildings, within Wandsworth Town CA.</li> <li>• Garratt Lane Burial Ground protected open space.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>• Wandsworth Common Conservation Area (G2).</li> <li>• West Hill Road Conservation Area (D4).</li> </ul>
<b>PTAL rating</b>	High (4-6a)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	G1 Wandsworth Town and Riverside - (Restore)
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>• D4 East Putney Residential</li> <li>• C3 Wandle Valley</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Very high
<b>Capacity</b>	Higher
<b>Southside Shopping Centre scenario</b>	
<b>Site Area</b>	5.4 ha
<b>Total residential units</b>	1,745 (including existing buildings)
<b>Non Residential GEA</b>	29,000 sqm
<b>Density (dph)</b>	323 (including existing buildings)

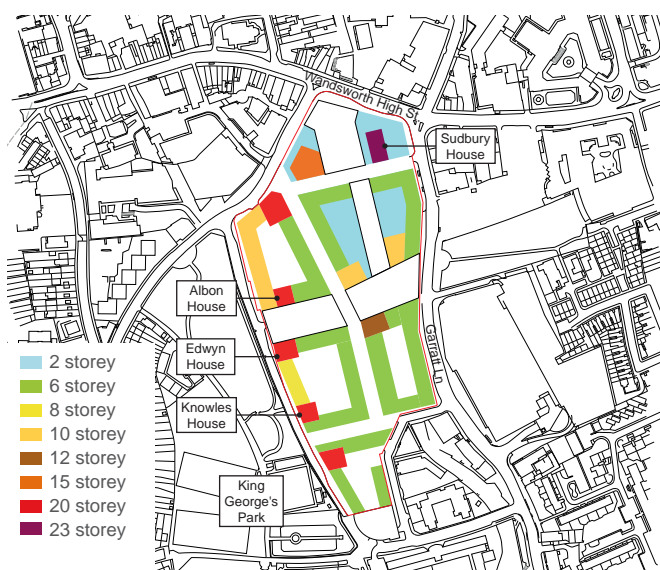


Fig. 375: Southside Shopping Centre scenario plan

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### Justification: Scenario

This scenario has been developed to test the appropriate building height of tall building zone TB-G1a-01. The character profile notes that the Southside area has a low sensitivity and potential for taller buildings around King George's Park, as long as they positively address the park and are softened with trees. The strategy for the area is to restore the existing character and ensuring future growth reinforces existing character. The scenario has been developed in line with the following principles:

- Taller buildings focussed along King George's Park to create a positive frontage to the park and maximise park views.
- Taller buildings along a central open space that connects King George's Park to Garratt Lane Old Burial Ground open space.
- Building heights step down towards Garratt Lane to respects its existing character.
- Retail and commercial uses along open spaces and Garratt Lane are included at ground floors within 2 storey podiums.



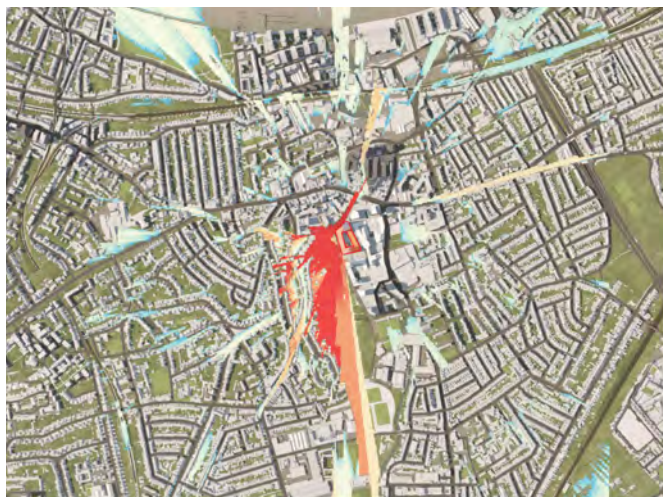
Fig. 377: Southside Shopping Centre massing model



Fig. 376: Southside Shopping Centre massing model

### ***Extent of visibility***

With its valley position, visibility is relatively contained in built-up parts of the borough, with long distance views along streets orientated towards Wandsworth Town Centre, and from large open spaces such as King George's Park, Wandsworth Common and the River Thames.



**Fig. 378:** Southside Shopping Centre scenario ZTV  
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### ***High level townscape, visual and heritage assessment***

#### ***Baseline***

The following valued features may be affected:

#### ***G1 Wandsworth Town and Riverside:***

- the historic old town including listed buildings;
- the Garratt Lane burial ground open space;
- landmarks including:
  - All Saints Church and Church Row;
  - Ram Brewery and its distinctive chimney;
  - Spread Eagle Pub
- Views from East Hill towards the town centre are affected by existing tall buildings, detracting from the character of the old town and listed features such as All Saints Church. Further tall buildings close to the main road may create the perception of a dense cluster which would be detrimental on the character of the distinctive views.
- Views from terraced residential streets close to the town centre may be affected by towers being visible above the roofline of otherwise relatively consistent tight street patterns with only sky visible above. Impacts will depend on the exact position of tall buildings, but should take account of glimpsed linear views along period terraced streets such as Tonsley

Rd and Eglantine Rd.

#### ***C3 Wandle Valley:***

- the sense of openness of King George's Park MOL
- Wide open views across the park are already characterised by a number of tall buildings in the vicinity (and on) the scenario site. Further tall buildings may be acceptable if designed well and in proportion to the specific scale of the park and vegetation for the area.

#### ***D4 East Putney Residential:***

- Further tall buildings would be visible in long distance views east from Upper Richmond Road, but are unlikely to be detrimental if designed well.

#### ***Assessment***

The building heights illustrated in the southern half of the site are considered to be broadly appropriate in principle if designed well, taking into account design principles in [Appendix A](#). In particular, development at this site must take into account the local presence of a number of heritage assets. Tall buildings should step down from the existing towers towards Garratt Lane burial ground.

The western edge of the site along King George's Park is also likely to be able to accommodate the approximate heights shown although these should step down towards Wandsworth historic core.

Plots closest to Wandsworth High Street should be limited to no more than 10% higher than existing buildings (excluding Sudbury House), to avoid creating a cluster in close proximity to heritage assets such as All Saints Church and detracting from the overall character of the town centre.

Existing tall development beyond the zone threatens the historic relatively small-scale character of the town centre - particularly in respect of the 23 storey Sudbury House.

The quantity of tall buildings in this scenario are assessed to be broadly appropriate in principle for the location, but some of the heights must be fully analysed within the local context to protect the character of Wandsworth Town Centre, in particular in relation to those close to Wandsworth High Street.

### A.8.3 TB-G1d-01: Wandsworth Town and Riverside

Existing prevailing height: 2-3 storeys

Appropriate height: 7-10 storeys (21-30m)



Fig. 379: TB-G1d-01 context map

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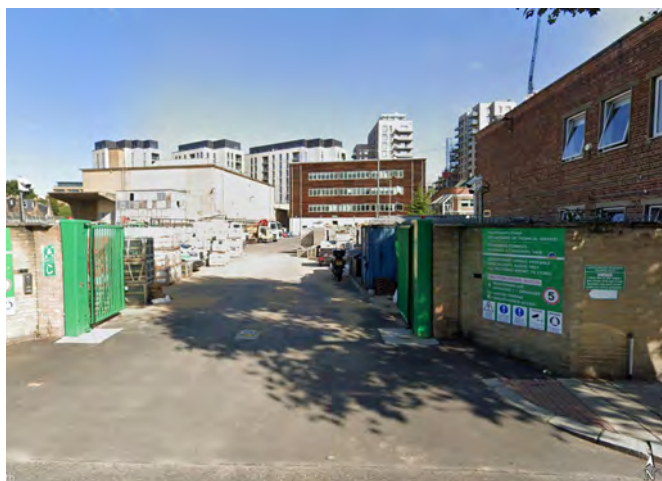


Fig. 380: Looking into the zone from Frogmore, to the south



Fig. 381: Looking into the zone from the east at Dormay Street

<b>TB-G1d-01</b>	
<b>Existing use</b>	Industrial.
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Wandle Delta masterplan which includes blocks between 2 and 5 storeys in the west of the plot, and 3 and 10 storeys to the east along Bell Lane Creek</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Area Strategy: Wandsworth Town.</li> <li>Economic Use Intensification Area.</li> <li>Site allocation: WT6 Frogmore Cluster.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Wandsworth Town Conservation Area (G1).</li> <li>Wentworth House (grade II).</li> <li>Locally listed building to the south of the zone.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>One locally listed building, to the west of the zone.</li> <li>Wandle Valley Trail.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Old York Road Conservation Area (G1).</li> <li>Wandsworth Park Registered Park and Garden (D2).</li> </ul>
<b>PTAL rating</b>	Good (4-5)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	G1 Wandsworth Town and Riverside (Restore)
<b>Other character areas within 250m</b>	• -
<b>Sensitivity</b>	Low
<b>Probability</b>	Very high
<b>Capacity</b>	Higher

### Justification: Consented masterplan

This analysis tests the appropriate building height of tall building zone TB-G1d-01, using the consented Wandle Masterplan masterplan development (see table above for associated heights).

The character profile notes this part of the character area as being of lower sensitivity representing a planner area of change where new development positively enhance the character of the area. However, much of the surrounding town centre is noted as nearing capacity for tall buildings. The character profile notes negative qualities including heavy traffic, dominance of industrial buildings with associated palisade fencing and blank façades, and poor maintenance. Recent tall buildings in the area are noted as adversely affecting the character of the narrow River Wandle character.

The strategy is to restore the character of the area which is currently fragmented, particularly in this sub-area. The design guidance states that new development should enable good public access to the River Thames and Wandle, restore historic character, respect the small scale of the Wandle corridor and respect the prevailing building height of the old town area (3-4 storeys). The guidance also notes that new development should consider views from residential areas and historic buildings.

### Analysis

This tall building zone falls entirely within a part of the Wandle Delta Masterplan SPD (January 2021). The masterplan includes blocks of 3-5 storeys in the south of the area along Armoury Way, which includes preservation of some existing buildings fronting onto the street. These heights preserve the character of the old town and are considered appropriate. The tall building zone (buildings 7 storeys and over) therefore excludes this part of the masterplan.

The masterplan also includes blocks of 2-5 storeys along the western edge of the masterplan, close to 2 storey period terraces along Sudlow Road. Taller buildings would risk adversely affecting the character of this older residential street and the zone has therefore been offset from this.

The tallest blocks in the masterplan (up to 10 storeys) are located along Bell Lane Creek and the railway line to the north of the plot.



Fig. 382: Extract of the Wandle Delta masterplan SPD showing building heights

Views of the consented masterplan are provided on the following page.

While the overall Wandle Delta masterplan has quite widespread visibility, this is primarily due to the taller blocks located close to the River Thames frontage. Visibility from this part of the masterplan is largely limited to the immediate area, with some linear views down streets and along the railway line. Refer to [Fig. 383](#) below.



**Fig. 383:** ZTV of the Wandle Delta masterplan SPD

The closest sensitive receptor to this zone is Wandsworth Town Conservation Area. The offset provided from the zone to Armoury Way protects the street scene which is a key part of the character of this area, while the taller parts of the zone would be viewed as stepping up towards the denser and taller riverside development.

With buildings up to 10 storeys, this zone should also not give rise to adverse effects on wider sensitive receptors including Old York Road Conservation Area to the east and Wandsworth Park Registered Park & Garden to the west.



**Fig. 384:** View of the section of the Wandle Delta masterplan to the west of Bell Lane Creek. The blocks preserve the height of the old town and frontage along Armoury Way, while stepping up to provide height close to the railway line and the river.

Development here would generally not be apparent within wider views from the opposite river bank (including Hurlingham Park and Fulham Palace Scheduled Monument).



**Fig. 385:** The tallest blocks within the section of the Wandle Delta masterplan to the west of Bell Lane Creek. The stepping down towards the edges of the plot protects the more historic core of the town centre while maximising density where there are fewer constraints.

Therefore, the zone is considered to have the capacity for tall buildings between 7 and 10 storeys, provided they are set back appropriately from Bell Lane Creek and step down towards the edges of the plot and the surrounding mid-rise building zones, including to Armoury Way. The tallest buildings should generally be located closest to the railway line and along the river corridor provided they are set back to minimise overshadowing.



**Fig. 386:** View of the Wandle Delta masterplan to the west of Bell Lane Creek from a pedestrian bridge over the river. The scale of the blocks suit the character of the river, provided sufficient set back is provided for public realm. The tallest block sits well alongside the railway to the right of the image.



### A.8.4 TB-G1d-02: Wandsworth Town and Riverside

Existing prevailing height: 2-3 storeys

Appropriate height: 7-10 storeys (21-30m)

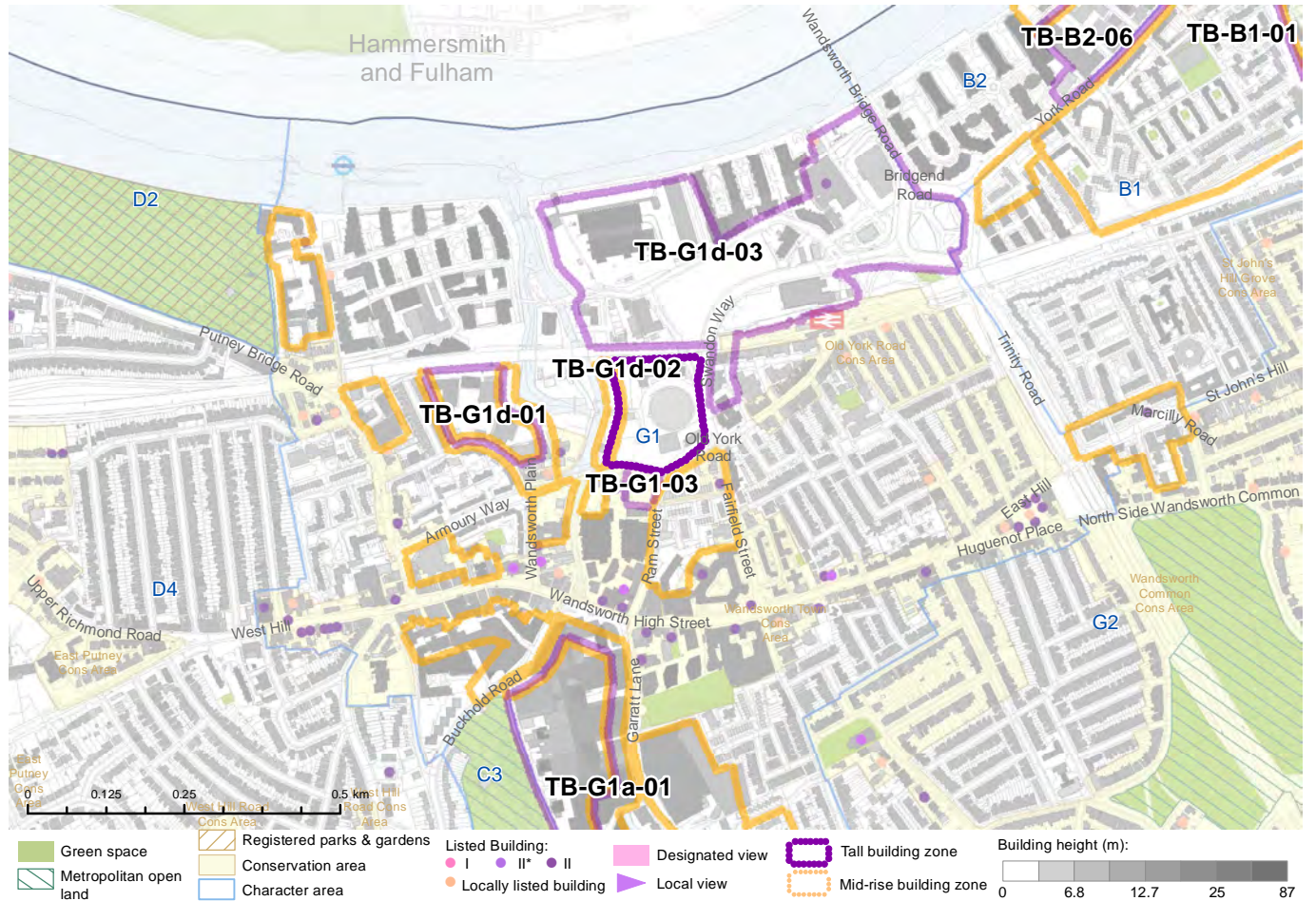


Fig. 387: TB-G1d-02 context map

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Fig. 388: Office building in the north east of the zone, from Swandon Way



Fig. 389: View into the zone from the south at Old York Road (A3)

<b>TB-G1d-02</b>	
<b>Existing use</b>	Industrial.
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Wandle Delta masterplan which includes blocks between 2 and 8 storeys in the west of the plot close to the River Wandle, and 4 and 10 storeys to the east along Swandon Way.</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Area Strategy: Wandsworth Town.</li> <li>Economic Use Intensification Area.</li> <li>Site allocation: WT4 Gasholder Site, Armoury Way.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Wandsworth Town Conservation Area (G1).</li> <li>Stables at Ram Brewery Complex (grade II).</li> <li>Locally listed building, to the south east of the zone.</li> </ul>
<b>Designations (within 100m)</b>	<ul style="list-style-type: none"> <li>2 grade II listed buildings, to the south east and west of the zone.</li> <li>One locally listed building.</li> <li>Wandle Valley Trail.</li> </ul>
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Old York Road Conservation Area (G1).</li> </ul>
<b>PTAL rating</b>	Good (5)
<b>Urban design study</b>	
<b>Character area - (and strategy)</b>	G1 Wandsworth Town and Riverside (Restore)
<b>Other character areas within 250m</b>	-
<b>Sensitivity</b>	Low
<b>Probability</b>	Very high
<b>Capacity</b>	Higher

**Justification: Consented masterplan**

This analysis tests the appropriate building height of tall building zone TB-G1d-02, using the consented Wandle Masterplan masterplan development (see table above for associated heights).

The character profile notes this part of the character area as being of lower sensitivity representing a planner area of change where new development positively enhance the character of the area. However, much of the surrounding town centre is noted as nearing capacity for tall buildings. The character profile notes negative qualities including heavy traffic, dominance of industrial buildings with associated palisade fencing and blank façades, and poor maintenance. Recent tall buildings in the area are noted as adversely affecting the character of the narrow River Wandle character.

The strategy is to restore the character of the area which is currently fragmented, particularly in this sub-area. The design guidance states that new development should enable good public access to the River Thames and Wandle, restore historic character, respect the small scale of the Wandle corridor and respect the prevailing building height of the old town area (3-4 storeys). The guidance also notes that new development should

consider views from residential areas and historic buildings.

**Analysis**

This tall building zone falls entirely within a part of the Wandle Delta Masterplan SPD (January 2021). The masterplan includes blocks between 4 and 10 storeys, generally stepping down from Swandon Way towards the River Wandle, and from the elevated railway line towards Armoury Way. The blocks are set back from



Fig. 390: Extract of the Wandle Delta masterplan SPD showing building heights

Armoury Way behind an area of public realm. These heights preserve the character of the old town along the Armoury Way frontage and are considered appropriate.

Views of the consented masterplan are provided on the following page.

While the overall Wandle Delta masterplan has quite widespread visibility, this is primarily due to the taller blocks located close to the River Thames frontage. Visibility from this part of the masterplan is largely



Fig. 391: ZTV of the Wandle Delta masterplan SPD



**Fig. 392:** Overview of the Wandle Delta masterplan to the east of the River Wandle looking towards the riverside zone and across to the northern bank of the Thames

limited to the immediate area, with some linear views down streets and along the railway line. Refer to [Fig. 391](#) below.

The closest sensitive receptor to this zone is Wandsworth Town Conservation Area. The offset provided from the zone to Armoury Way protects the street scene which is a key part of the character of this area, while the taller parts of the zone would be viewed as stepping up towards the elevated railway and the denser, taller riverside development.

With buildings up to 10 storeys, this zone should also not give rise to adverse effects on wider sensitive receptors including Old York Road Conservation Area to the east and Wandsworth Park Registered Park & Garden to the west.

Development here would generally not be apparent within wider views from the opposite river bank (including Hurlingham Park and Fulham Palace Scheduled Monument). Views would generally be obscured by other existing riverside development and future blocks in the riverside tall building zone (TB-G1a-03).

Therefore, the zone is considered to have the capacity for tall buildings between 7 and 10 storeys, provided they are set back appropriately from the River Wandle and step down towards the edges of the plot and the surrounding mid-rise building zones, including to Armoury Way. The tallest buildings should generally be located closest to the railway line and along Swandon Way provided good urban design principles are followed to create a positive experience for people travelling along the road. Additional height beyond 10 storeys would risk adverse effects on the character of Wandsworth Town Conservation Area particularly.



**Fig. 393:** View of the Wandle Delta masterplan to the east of the River Wandle looking south towards Wandsworth Town Conservation Area



**Fig. 394:** View of the Wandle Delta masterplan to the east of the River Wandle looking along Armoury Way with the tall building zone on the left of the image, and Wandsworth Town Conservation Area to the right.

### A.8.5 TB-G1d-03: Wandsworth Town and Riverside

Existing prevailing height: 4-8 storeys

Appropriate height: 7-15 storeys (21-45m)

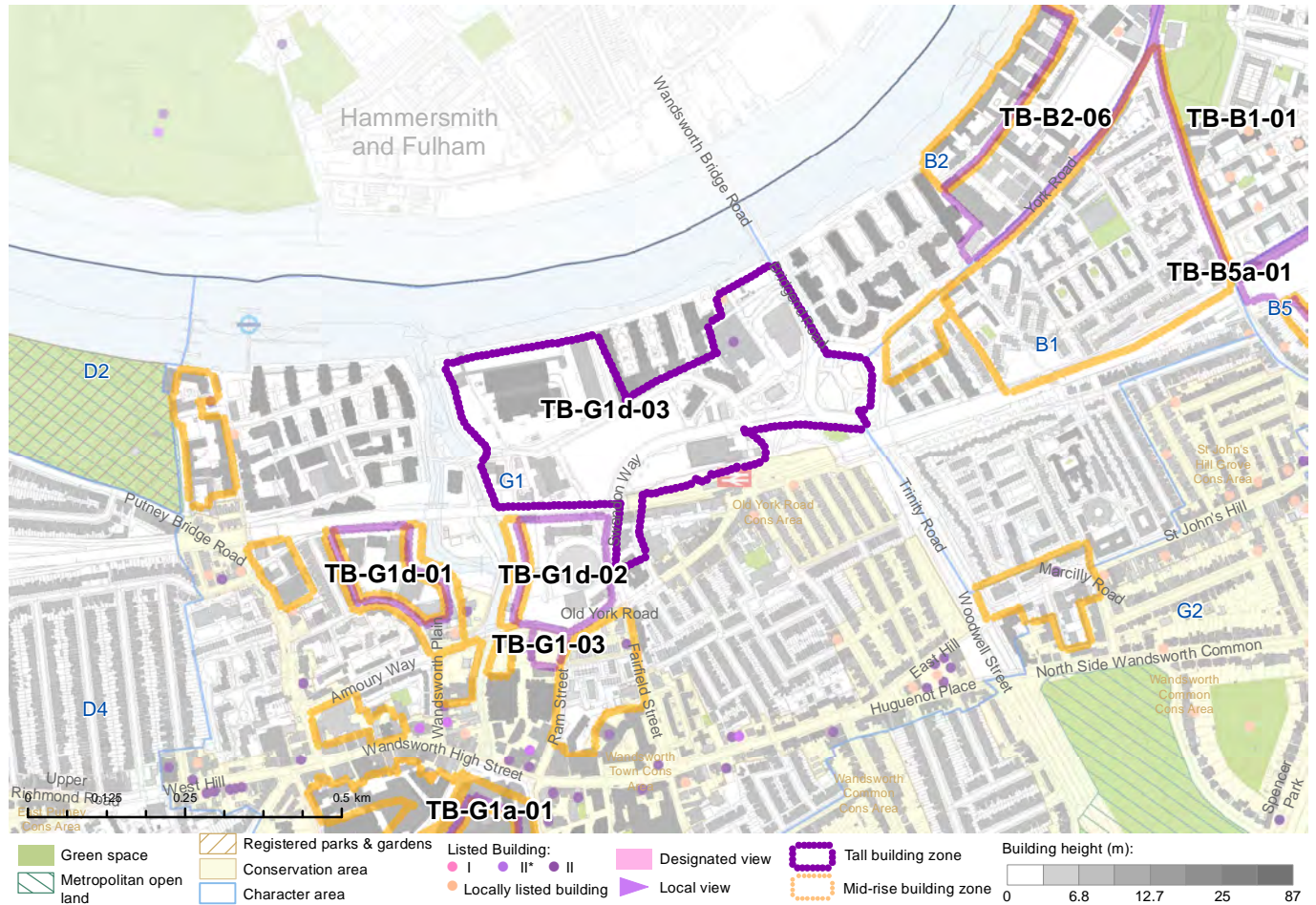


Fig. 395: TB-G1d-03 context map

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Fig. 396: The waste transfer site in the north west of the zone



Fig. 397: Residential riverside blocks in the north east of the zone, from the Thames Path

<b>TB-G1d-03</b>	
<b>Existing use</b>	Industrial/residential.
<b>Existing/consented tall buildings</b>	<ul style="list-style-type: none"> <li>Residential blocks by the riverside (existing).</li> <li>Refuse tip (existing).</li> <li>B&amp;Q depot, Smugglers Way (under construction).</li> <li>Wandle Delta masterplan which includes blocks between 4 and 15 storeys in the west of the plot close to the River Wandle, and 6 to 12 storeys to the east close to the Wandsworth gyratory. The development preserves the listed bus garage in the area.</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>Area Strategy: Wandsworth.</li> <li>Site allocation: WT9 Feather's Wharf/Smugglers Way Cluster; WT11 Western Riverside Waste Transfer Station; WT13 Swandon Way Cluster; Wandsworth Bridge Cluster; WT16 Wandsworth Bridge Roundabout.</li> <li>Safeguarded Wharves.</li> <li>Thames Policy Area.</li> <li>Focal Point: Wandsworth Riverside Quarter and Wandle Delta.</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>Wandsworth Garage Bus Depot (grade II), within the zone; and 41 The Ship, Jews Row (locally listed), within the zone.</li> <li>Old York Road Conservation Area (G1).</li> <li>Bramford Road protected open space.</li> <li>Thames Path.</li> </ul>
<b>Designations (within 100m)</b>	-
<b>Conservation areas and high value designations within 500m</b>	<ul style="list-style-type: none"> <li>Wandsworth Town Conservation Area (G1).</li> <li>Wandsworth Park Registered Park and Garden (D2).</li> </ul>
<b>PTAL rating</b>	Medium (3-5)
<b>Urban design study</b>	
<b>Character area</b>	G1 Wandsworth Town and Riverside
<b>Other character areas within 250m</b>	<ul style="list-style-type: none"> <li>B1 Battersea Residential</li> <li>B2 Battersea Riverside</li> </ul>
<b>Sensitivity</b>	Low
<b>Probability</b>	Medium/very high
<b>Capacity</b>	Higher

**Justification: Consented masterplan**

This analysis tests the appropriate building height of tall building zone TB-G1d-03, using the consented Wandle Masterplan masterplan development (see table above for associated heights).

The character profile notes this part of the character area as being of lower sensitivity representing a planner area of change where new development positively enhance the character of the area. However, much of the surrounding town centre is noted as nearing capacity for tall buildings. The character profile notes negative qualities including heavy traffic, dominance of industrial buildings with associated palisade fencing and blank façades, and poor maintenance. Recent tall buildings close to the area are noted as adversely affecting the character of the narrow River Wandle character.

The strategy is to restore the character of the area which is currently fragmented, particularly in this sub-area. The design guidance states that new development should enable good public access to the River Thames and Wandle, restore historic character, respect the small scale of the Wandle corridor and respect the prevailing building height of the old town area (3-4 storeys). The guidance also notes that new development should consider views from residential areas and historic buildings.



**Fig. 398:** Extract of the Wandle Delta masterplan SPD showing building heights in the west of the tall building zone



**Fig. 399:** Extract of the Wandle Delta masterplan SPD showing building heights in the east of the tall building zone near the Wandsworth gyratory

**Analysis**

This tall building zone falls within a part of the Wandle Delta Masterplan SPD (January 2021). The masterplan includes two different development areas either side of the Riverside West modern residential blocks. The plots in the west of the zone, close to the existing waste

transfer station, range 4 and 15 storeys, with the tallest blocks generally closest to the railway and stepping down towards the relatively small-scale River Wandle corridor. The plots to the east of the zone are between 6 and 12 storeys high, generally stepping down from Swandon Way towards the listed bus garage in the west which is retained and incorporated into the overall development vision. The masterplan notes future opportunities for improvements to the riverside at the Pier Wharf safeguarded wharf site and around the Wandsworth gyratory which represents a key gateway into the borough.

Views of the consented masterplan are provided on the following page.

While the overall Wandle Delta masterplan has quite widespread visibility, this is primarily due to the taller blocks located close to the River Thames frontage. Visibility from this part of the masterplan is largely limited to the immediate area, with some linear views down streets and along the railway line. Refer to [Fig.400](#) below.



**Fig. 400:** ZTV of the Wandle Delta masterplan SPD

The masterplan heights in both zones respond positively to sensitive existing features including the River Wandle and the listed bus garage; while also providing additional height in keeping with the scale of the infrastructure (Swandon Way and the railway) and the surrounding land uses (the waste transfer station and surrounding tall residential development including towers over 20 storeys high).

The tall building zone promotes the tallest buildings being located closest to the elevated railway line and the Wandsworth gyratory on Swandon Way. Buildings should step down towards the River Thames frontage to ensure there is space between the individual tall residential developments which

are present immediately adjacent to, and within, the tall building zone. This is important to prevent over development and canyoning along the River Thames, which has its own distinct character as a cultural resource and identifying feature for London as a whole. Development in this plot also needs to preserve the character of sensitive features on the north bank of the river including Hurlingham Park and Fulham Palace Scheduled Monument which is upstream of the grade II listed Putney Bridge. The heights indicated, provided they step down towards the River Thames frontage and are of a good design quality in line with the guidelines provided in [Appendix A](#), are considered to be appropriate for the zone and would not give rise to adverse effects on views from or the setting of the riverside environment and north bank.

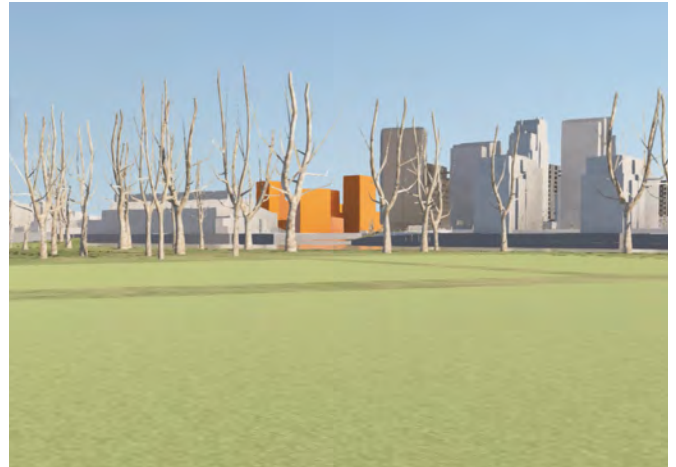
Development closest to Swandon Way must respect the small scale character of Old York Road Conservation Area, which includes a small local centre around Wandsworth Town train station. Development to the south of Swandon Way must also step down towards Bramford Gardens. The building heights tested (particularly close to the Wandsworth gyratory) are considered to sit appropriately as a step up towards other existing taller riverside development.

With buildings up to 15 storeys close to the railway and Swandon Way, this zone should also not give rise to adverse effects on wider sensitive receptors including Wandsworth Town Conservation Area to the south and Wandsworth Park Registered Park & Garden to the west. Buildings towards the River Thames frontage should generally be no taller than 10 storeys to protect the character of the river (in response to nearby existing tall buildings and therefore considering the potential cumulative impacts) and sensitive assets on the north bank. The plot that extends just south of the railway has capacity for buildings no taller than 8 storeys due to the proximity to Old York Road Conservation Area.

Therefore, the zone is considered to have the capacity for tall buildings between 7 and 15 storeys, provided they are set back appropriately from the River Wandle and River Thames, and step down towards the edges of the plot, Old York Road Conservation Area and also Bramford Gardens. The tallest buildings should generally be located closest to the railway line and along Swandon Way provided good urban design principles are followed to create a positive experience for people travelling along the road. Additional height beyond 15 storeys would risk adverse effects on the character of the nearby conservation areas, the River Thames character itself and sensitive features on the north bank of the Thames.



**Fig. 401:** Overview of the Wandle Delta masterplan looking north towards the River Thames and LB Hammersmith & Fulham



**Fig. 404:** View towards the Wandle Delta masterplan from Hurlingham Park in LB Hammersmith & Fulham on the north bank of the River Thames



**Fig. 402:** View of the Wandle Delta masterplan from the retained listed bus garage



**Fig. 405:** View towards the Wandle Delta masterplan from Fulham Palace Scheduled Monument. The tall building zone would be barely visible in views from across this Bishop's Park



**Fig. 403:** View north towards the Wandle Delta masterplan from the retained Dalby Road within Old York Road Conservation Area. The design quality of the tallest block in this part of the masterplan will be particularly important, along with providing an interesting and varied roofline



**Fig. 406:** Oblique view towards the Wandle Delta masterplan from Hurlingham Park. The consented masterplan blocks would provide a visual step down from existing taller riverside development, and protect the river frontage itself from being over-developed

Note that the above images were generated for the purpose of this study and are not taken from the Wandle Delta Masterplan SPD.

## A.9 Tall building design guidance

### A.9.1 Introduction

Tall buildings can play an important role in supporting the borough's growth. However, owing to their scale, they can significantly alter the character of an area as well as the skyline of the city. Policy D8 in the emerging London plan provides the following definition for tall buildings:

*'Tall buildings are generally those that are substantially taller than their surroundings and cause a significant change to the skyline. In large areas of extensive change, such as Opportunity Areas, definitions of tall buildings should relate to the evolving context. For the purpose of assessing applications referable to the Mayor, a tall building is a development that meets one or more of the following descriptions:*

- *it falls within the Thames Policy Area and is more than 25m in height*
- *it falls anywhere else within the City of London and is more than 150m in height*
- *it is more than 30m in height elsewhere in London.'*

This section of the Urban Design Study provides high level guidance with regards to the design and siting of tall buildings. It is not intended to provide exhaustive guidance on how tall buildings should look within the borough, but rather provide an indication of some of the key elements that tall building developers will need to demonstrate have been considered in any future applications. Guidance in this section has been provided against nine themes shown on the following pages. These themes have been developed to respond to policies and guidance included within the emerging London Plan:

This section should be read alongside the character area specific design guidance provided in [Section 3](#), which sets out elements for consideration for each individual character area. Tall building developers should respond to the design guidance for the relevant character areas in addition to the guidance within this section.

This design guidance should also be read alongside the following documents which provide planning guidance for London as a whole:

#### The London Plan

At the time of writing the current version of the Intend to Publish London Plan (December 2019)

<https://www.london.gov.uk/what-we-do/planning/>

[london-plan](#)

#### London View Management Framework

Guidance on protected linear views, panoramas, river prospects and townscape views across London. Linear View 9 from King Henry VIII's Mound in Richmond to St Paul's Cathedral crosses parts of LB Wandsworth.

<https://www.london.gov.uk/what-we-do/planning/implementing-london-plan/london-plan-guidance-and-spgs/london-view-management>

#### Good Quality Homes for All Londoners, Draft Supplementary Planning Guidance (SPG)

At the time of writing the following documents are available as pre-consultation drafts (2020):

##### Module A: Optimising site capacity: a design-led approach

[https://www.london.gov.uk/sites/default/files/hdspg\\_2020\\_module\\_a.pdf](https://www.london.gov.uk/sites/default/files/hdspg_2020_module_a.pdf)

##### Module B: Small housing developments: assessing quality and preparing design codes

[https://www.london.gov.uk/sites/default/files/hdspg\\_2020\\_module\\_b.pdf](https://www.london.gov.uk/sites/default/files/hdspg_2020_module_b.pdf)

##### Module C: Housing design quality and standards

[https://www.london.gov.uk/sites/default/files/hdspg\\_2020\\_module\\_c.pdf](https://www.london.gov.uk/sites/default/files/hdspg_2020_module_c.pdf)

##### Module D: Housing design case studies and appendices

[https://www.london.gov.uk/sites/default/files/hdspg\\_2020\\_module\\_d\\_part\\_1.pdf](https://www.london.gov.uk/sites/default/files/hdspg_2020_module_d_part_1.pdf)

The following national design guidance should also be referred to in the development of tall building proposals:

#### National design guide (2019)

<https://www.gov.uk/government/publications/national-design-guide>

#### Tall Buildings: Historic England Advice Note 4 (May 2019)

At the time of writing this is a consultation draft.



## A.9.2 Visual impacts

Tall buildings can have a considerable visual impact, both in their immediate context as well as on a city scale. This impact can be positive or negative, and this may change depending on their exact position and where they are viewed from. Different people will also have different responses to tall buildings.

The following guidelines are drawn from the Policy D8 of the emerging London Plan with regards to visual impact:

*The views of buildings from different distances need to be considered, including:*

1. *Long-range views – these require attention to be paid to the design of the top of the building. It should make a positive contribution to the existing and emerging skyline and not adversely affect local or strategic views*
2. *Mid-range views from the surrounding neighbourhood – particular attention should be paid to the form and proportions of the building. It should make a positive contribution to the local townscape in terms of legibility, proportions and materiality*
3. *Immediate views from the surrounding streets – attention should be paid to the base of the building. It should have a direct relationship with the street, maintaining the pedestrian scale, character and vitality of the street. Where the edges of the site are adjacent to buildings of significantly lower height or parks and other open spaces there should be an appropriate transition in scale between the tall building and its surrounding context to protect amenity or privacy.*

In addition to the above, the following aspects should be considered:

- The proposed design must respect key view corridors towards strategic landmarks across the borough and in neighbouring boroughs. This would also include distinctive roof line features such as spires and turrets. Refer to [Section 3](#) for landmarks and key views within LB Wandsworth and relevant characterisation or urban design studies for neighbouring boroughs.
- Appropriate 3D modelling and analysis must be carried out to study the impact of the building on the overall skyline. The study must also incorporate buildings that are approved but not yet built to ensure



**Fig.408:** Protected view corridor towards St. Paul's Cathedral  
© Kunstlerbob (Robert Bauer)



**Fig.407:** One Canada Square, Canary Wharf was designed as a landmark with views framed by surrounding background buildings  
© Photo by DAVID ILIFF. License: CC BY-SA 3.0

that the future nature of views are considered as far as practicable. Refer also to [Section A.9.10](#) on the cumulative impacts of tall buildings.

- It is important to ascertain whether the proposed structure is a background building or a landmark building. Background buildings should respect the surrounding context and preserve the hierarchy of existing prominent view corridors. Landmark structures should respond to analysis of key view corridors towards the site to ensure the precise placement, form, skyline and detailing accentuate its prominence within the wider context.
- The location of tall buildings must consider their strategic context within the borough to enhance its urban character. For example, a tall building can provide an interesting termination to an otherwise monotonous view corridor such as a long linear street.
- The placement of the building must avoid visual interruptions in areas with otherwise very consistent building and/or roof lines.

### A.9.3 Spatial hierarchy

Tall buildings can make a significant contribution to the identity of an area. Positioned poorly they can dominate areas of smaller grain urban fabric, while positioned well they can create a hierarchy that responds to the proportions of adjacent buildings, streets, open spaces and watercourses.

The following guidelines are drawn from the Policy D8 of the emerging London Plan with regards to spatial hierarchy:

*Whether part of a group or stand-alone, tall buildings should reinforce the spatial hierarchy of the local and wider context; and aid legibility and wayfinding.*

In addition to the above, the following aspects should be considered:

- Tall buildings can act as key landmarks making a positive contribution towards the skyline. However, their placement with respect to each other as well as their height needs to be studied to ensure they are appropriate for the scale of the local area.
- The design and location of tall buildings must consider their role in wayfinding, such as, acting as landmarks or gateway features marking town centres or local centres.
- Tall buildings must be in proportion to their local environment. This includes consideration of the width of adjacent streets as well as public open spaces, parks and watercourses. For example, in Wandsworth buildings beside the River Thames have the potential to be of a much greater scale than the River Wandle running through the town centre.
- The massing of the building must be designed so



**Fig.410:** This mass and height of this building adversely dominates the consistent street scene along Wandsworth High Street

as not overwhelm the street and adjacent context.

Where a building is higher than some of the existing built form, it may be acceptable to consider stepping back or tapering upper storeys to protect the consistent height of the street frontage.

- The design of tall buildings must consider any potential privacy intrusion into private or communal gardens and neighbouring developments.
- Tall buildings require more space around them to ensure they integrate well into the overall townscape. In many estates this is managed through extensive parkland settings with mature trees. New tall buildings closer to streets should still consider how to soften their edges and provide positive public spaces at their base through the use of generous walkways and mature planting.



**Fig. 409:** Du Cane Court in Balham, at eight storeys is taller than the prevailing building heights in the area of 3-4 storeys, but it successfully integrates through stepping down and being well integrated with trees and planting. It is well-sited on the wide Balham High Road.



**Fig.411:** Mature planting and large open spaces help to integrate and can better accommodate tall buildings

#### A.9.4 Architectural quality and materials

This section provides guidance for the architectural quality, character and use of materials for tall buildings.

The following guidelines are drawn from the Policy D8 of the emerging London Plan:

*Architectural quality and materials should be of an exemplary standard to ensure the appearance and architectural integrity of the building is maintained through its lifespan. A tall building can be considered as being made up of three main parts: a top, middle and base.*

- *The top includes the upper floors, and roof-top mechanical or telecommunications equipment and amenity space. The top should be designed to make a positive contribution to the quality and character of the skyline, and mechanical and telecommunications equipment must be integrated in the total building design. Not all tall buildings need to be iconic landmarks and the design of the top of the building (i.e. the form, profile and materiality) should relate to the building's role within the existing context of London's skyline. Where publicly accessible areas, including viewing areas on upper floors, are provided as a public benefit of the development, they should be freely accessible and in accordance with part G of Policy D7 Public realm. Well-designed safety measures should be integrated into the design*



**Fig. 412:** Blackfriars Circus, 'The use of brick cladding on this tall building gives it a character that fits well into its historic surroundings, and a texture that is human scale and tactile where the building meets the ground.'

© National Design Guide

*of tall buildings and must ensure personal safety at height.*

- *The middle of a tall building has an important effect on how much sky is visible from surrounding streets and buildings, as well as on wind flow, privacy and the amount of sunlight and shadowing where is in the public realm and by surrounding properties.*
- *The base of the tall building is its lower storeys. The function of the base should be to frame the public realm and streetscape, articulate entrances, and help create an attractive and lively public realm which provides a safe, inclusive, interesting, and comfortable pedestrian experience. The base should integrate with the street frontage of adjacent buildings, and where appropriate enable the building to transition down in height.*

In addition to the above, the following aspects should be considered:

- The architectural massing and materials must respect the character of the surrounding built environment. While landmark developments may express themselves in a dramatic or interesting way, this should still consider the use of quality materials appropriate for the local vernacular or responding to a historic or cultural association with the site.
- The building façade design and articulation must pay careful attention and respond to small details which are often important in the townscape character of Wandsworth, for example windows, doors, lintels, roof gable details, chimneys.
- Tall buildings still need to make a positive contribution to the street scene at ground level. Details at this level should be of a pedestrian scale and use materials that are of a quality that can stand up to close visual attention and also be robust for a busy city environment.



**Fig. 413:** Careful use of brick detailing within the semi-public space at the base of this development (Peabody Estate, St John's Hill) near Clapham Junction provides a human scale to the building.

### A.9.5 Heritage

London's built and landscape heritage is an integral part of the city's identity and a significant contributor towards its culture and economy. Hence, any tall building proposal must respect and respond to any heritage sites or structures in its vicinity.

The following guidelines are drawn from the Policy D8 of the emerging London Plan:

*Proposals should take account of, and avoid harm to, the significance of London's heritage assets and their settings. Proposals resulting in harm will require clear and convincing justification, demonstrating that alternatives have been explored and there are clear public benefits that outweigh that harm. The buildings should positively contribute to the character of the area*

*Buildings in the setting of a World Heritage Site must preserve the Outstanding Universal Value of the World Heritage Site, and the ability to appreciate it. In relation to Wandsworth, this potentially applies to some locations where views or the setting of the Houses of Parliament and Westminster Palace are potentially impacted.*

In addition to the above, the following aspects should be considered:

- The construction of tall buildings generally requires deeper excavation for foundations which in turn can affect the structural integrity of adjacent heritage structures. Adequate surveys and studies must be undertaken and the proposed design must avoid any damage to the heritage sites as well as buried archaeology.
- The proposed architectural character and materials must respect the tonality, texture and rhythm of the adjacent heritage buildings so as not to contrast with their character.
- The proposed location and design must not only respect historical structures but also historic street proportions and building lines.
- Heritage structures are not only important for their architecture, any proposed buildings must also respect their historic function and their prominence within the borough. New tall buildings must preserve this important historical and cultural associations and not overwhelm the historic setting of the heritage assets. Within Wandsworth many landmark buildings are historic churches and town halls which are important skyline features. Therefore, tall buildings



**Fig. 414:** This new development in Granfield Street successfully responds to the nearby Grade II Church of the Sacred Heart in Battersea Square Conservation Area, through use of high quality bricks, and use of subtly different coloured bands of bricks. Successful use of sensitive architectural detailing and its subservient scale to the church landmark, alongside the courtyard space which frames views to the spire.

need to avoid both obscuring important views of these landmarks but also avoid altering the skyline by becoming features of the backdrop.

- Any conversions or extensions to heritage assets should differentiate between the new and old parts of the overall structure sympathetically. Depending on the significance of the heritage asset it may also be important to ensure any alterations are entirely reversible in the future.



**Fig. 415:** St Mary's Church next to Putney Bridge is overwhelmed by the adjacent Putney Wharf Tower

### A.9.6 River Thames frontage

Wandsworth has one of the longest frontages to the River Thames of the London boroughs. While the scale of the river presents opportunities for height, any tall structure will still have a major influence due to the long sweeping panoramic views across and along the banks, which extend well beyond the borough.

The following guidelines are drawn from the Policy D8 of the emerging London Plan:

*Buildings near the River Thames, particularly in the Thames Policy Area, should not contribute to a canyon effect along the river which encloses the open aspect of the river and the riverside public realm, or adversely affect strategic or local views along the river.*

In addition to the above, the following aspects should be considered:

- Buildings fronting the River Thames are likely to have a prominent presence in the city skyline and a high visibility from several parts of the city. Hence, their design must respond to both, views towards them as well as from them.



**Fig. 416:** View across the River Thames to the Battersea frontage including the architecturally distinctive Norman Foster building

© Chris Seager

- The building form must strike a balance between achieving optimal riverfront views without creating a dense wall of development that blocks visibility from buildings and public spaces behind it.
- Development in Wandsworth quickly reduces in height away from the river, into the typical period terraces and modest housing scale. Therefore riverfront buildings must still consider their landward facing orientation and step down appropriately to provide a transition towards smaller building types. Materials may also transition from the river frontage where views are cherished, towards the more traditional natural materials of housing stock within the borough.
- The design must maintain the importance of the river frontage as a public resource. The river front should not feel private and too heavily overlooked or shaded. Developments must be set back to physically

and visually ensure the Thames Path acts and feels like a welcoming public route without heavy overlooking from adjacent riverside residences. Ground floor uses should seek to activate the space as far as possible.



**Fig. 417:** Along Wandsworth's riverside the design and width of the Thames Path feels like a private space for residents rather than an important continuous public resource for London

- The building design must consider its role as an important marker for legibility/identity of the borough and wayfinding owing to its high visibility along the riverfront. In particular, riverside development viewed from bridges are one of the ways that the greatest number of people experience the borough.
- Riverside development must carefully consider the surrounding context up and downstream and along both banks. Development must avoid creating canyons of development which would harm the overall cultural and historical identity of the river and its sense of openness at the heart of London.

### A.9.7 Microclimate and lighting

Tall buildings inevitably have an impact on the local microclimate which needs considering during the design development. Factors such as wind comfort, sunlight and daylight availability and air quality will be influenced by a tall development. How a building is perceived at night is also important to understand to avoid adverse impacts arising from lighting.

#### *Lighting*

- The building design must avoid lighting features which would negatively impact on surrounding buildings (particularly residential) and also overall night time vistas and panoramas.
- The building façade design and glazing must consider the building use at night and minimise light spill that can exacerbate light pollution.

#### *Microclimate*

- The local microclimate needs to be considered holistically and at a masterplanning scale. Microclimatic issues are not usually limited to the bounds of an isolated building or development but will likely influence the neighbouring sites as well.

#### *Wind*

- A wind- desk study should be carried out at master planning level to determine if there are any areas of concern in terms of pedestrian wind comfort. This can be used to establish a tall building strategy for instance clustering tall building together to prevent downdrafts. It will also help to establish where extra space may need to be accommodated for mitigation.
- It is recommended that a peer reviewed set of wind climate data is prepared to act as a common and consistent basis for all win studies. This would include data for all win directions such as Weibull parameters, mean wind speeds and gusts.
- Reference should be made to the City of London's [guidance on tall buildings in relation to wind](#).

#### *Daylight and sunlight*

- The BRE 209 Site Layout for Daylight and Sunlight guide gives advice on layout for achieving good sunlighting and daylighting within buildings and in open spaces. It also offers guidance for overshadowing of adjacent buildings and sunlight in public spaces. Although it is not mandatory to comply with it is the most commonly referenced

guidance by planning authorities.

- BRE 209 references BS 8206-2 British Standard Code of practice for daylighting for more detailed evaluation of daylight such as to determine daylighting quality inside buildings.

#### *Urban heat island*

- The choice of building materials must consider their contribution to urban heat island effect through thermal radiation as well as release of anthropogenic (waste) heat. Vertical greening systems into the building façade and green roofs can help to reduce the impact of the building on urban heat island effect.
- Shade analysis must be carried out to ensure that the building shadow does not adversely impact solar gain and thermal comfort in key public spaces during times of busy use.

#### *Glare*

- Building façade design and glazing must avoid any harsh solar glare onto any public areas as well as surrounding development.



**Fig.418:** Harsh solar glare from a tall building

© New York Times, <https://static01.nyt.com/images/2012/05/02/arts/design/GLARE/GLARE-superJumbo.jpg?quality=90&auto=webp>

### A.9.8 Access and ground floor uses

This section provides guidance on design considerations where a tall building meets the ground. While tall buildings can influence the character of a wide area and have extensive visibility, they still have a local impact on the pedestrian and street environment at which they sit.

- The ground floor design must consider the access and egress requirements for all type of uses including pedestrians, cyclists, public transit users, private vehicles as well as service vehicles and personnel.
- The building design must comply with Regulatory Reform (Fire Safety) Order 2005 (RRO) and the Management of Health and Safety at Work Regulations 1999 (MHSWR).
- The main access to the building must be provided along the frontage facing primary streets and must provide an engaging arrival experience. Entrances/exits along quiet alleyways or areas away from general public use and visibility must be avoided.



**Fig. 420:** An example of a poor ground floor frontage at a development on the corner of Balham High Road and Upper Tooting Park

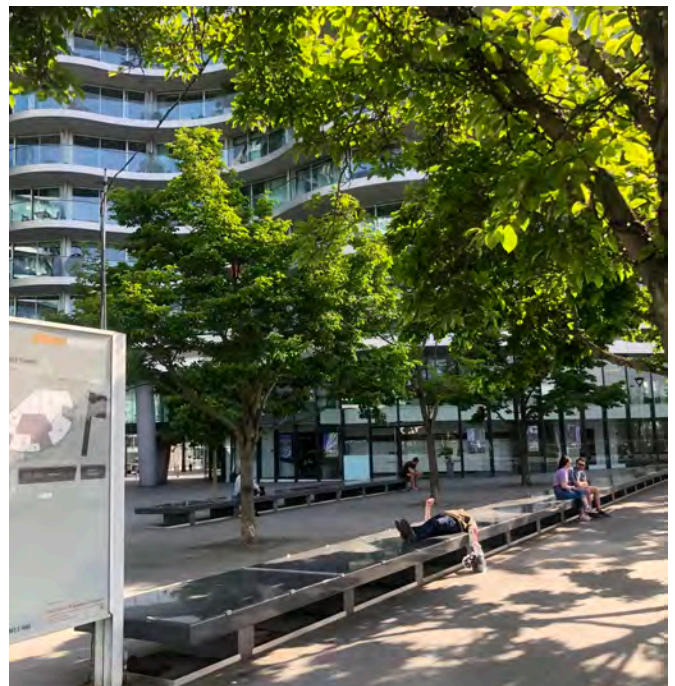
- The service access including those for uses such regular maintenance, waste collection, deliveries must be separated from the primary access locations and screened away from key public areas.
- Entrance to car parks and basements must consider their impact traffic on the surrounding roads and adequate traffic studies must be carried out at the outset to mitigate any adverse impact on the existing infrastructure.
- Active ground floor uses must be integrated within the design based on the intended use of the buildings. For example, the provision of a café or a shop at the ground level of an office building can help activate

the area around it during non-office hours. Similarly, integration of facilities such as shops, nurseries or clinics at ground level could help supplement the existing community amenities in an area.



**Fig. 419:** The vibrant and normally busy public realm in the Nine Elms masterplan area at the base of numerous tall buildings. Active uses include restaurants and bars, with events such as Art Night (shown here in 2018)

- Active ground floor uses should be along the main public frontage of the building for ease of access and encourage greater footfall.
- In case of residential uses on the ground floor, adequate design measures must be taken to protect the privacy of the residents in these units from the street or any other public space.



**Fig. 421:** A broad and welcoming public realm at the base of the Norman Foster building at Albion Riverside

### A.9.9 Public uses and public realm

This section provides guidance for the integration of public uses, impact of tall buildings on the surrounding public realm as well as the design of public realm around tall buildings.

- Consideration should be given to creating publicly accessible areas open to general public within tall buildings. Typically this would be at ground or mezzanine levels, but upper storeys could also be considered where they can provide fantastic views of London. These can include a number of uses such as libraries, community centres, leisure facilities and restaurants.
- In addition to above, consideration must be given to provision of public spaces and amenities for the building users/residents. Examples include, roof gardens/terraces, gymnasiums, cafeterias or food courts.
- Residential buildings must consider provision of balconies to provide residents opportunities to interact with the external environment while maintaining a sense of enclosure and privacy at the same time.
- The design of the building must not adversely impact the visual and thermal comfort of the surrounding public areas including alleys, streets, plazas and any other open spaces.
- Where possible, buildings with commercial use must supplement the existing public realm through the incorporation of public spaces such as plazas at their entrance.



Fig. 422: Vibrant ground floor uses within Nine Elms

- The building design at the ground level must avoid any blank façades, especially where they face the public realm.

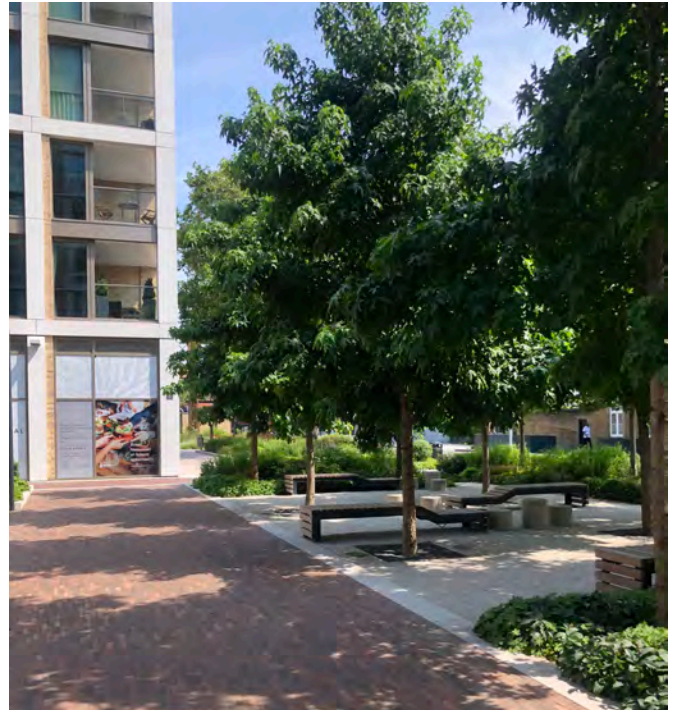


Fig.423: Attractive public realm with quality materials, integrated seating and a generous allocation of space at a new residential development at Prince of Wales Drive

- Where possible, the building design must maintain through access for ease of pedestrian movement and permeability. This is especially applicable for developments on large plots and close to public assets such as parks and river corridors.

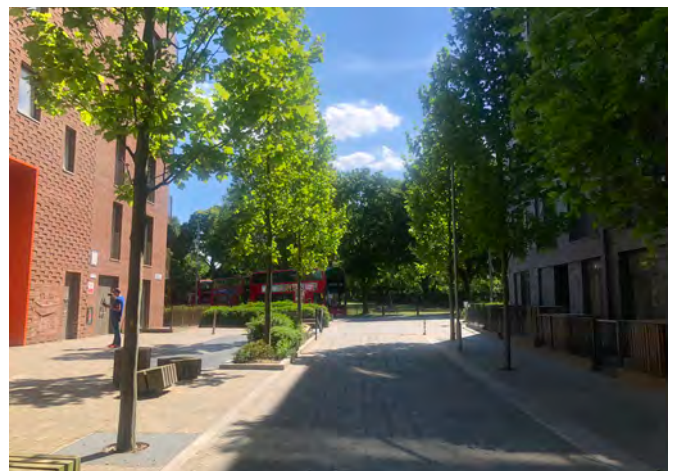


Fig.424: Planting and public routes in a Peabody development improve connectivity to Wandsworth Common



### A.9.10 Cumulative impacts

Taller buildings can provide a higher density of development where it is appropriate to do so, playing an important role in the regeneration of a city. In certain situations, it may be feasible to provide a cluster of tall buildings as opposed to a single structure. The following section provides guidance with regards to the cumulative impacts of tall building clusters.

- The clustering of multiple buildings can significantly alter the character of a neighbourhood. Certain parts of the neighbourhood might benefit from a tall structure. However, that doesn't mean it necessarily suits a cluster of tall buildings which can completely transform the character of the whole area.
- Tall building clusters can significantly alter the city skyline. Hence, any such proposals must consider their visual impact on the existing and emerging skyline.



**Fig. 426:** An emerging cluster of tall buildings within Wandsworth at the confluence of the River Wandle and the River Thames

- A tall building cluster is likely to accommodate substantially higher density uses and potentially be key employment or commercial centre. Hence, its location and use must consider the future capacity of infrastructure to support the development as well as any adverse impact on other employment and

commercial centres across to city.

- A tall building cluster must preferably be located within areas of good PTAL to avoid overwhelming the existing public transport network. Such development must also consider if the enhancement of the existing public transit infrastructure is required to accommodate a higher number of future users (e.g. the Northern Line extension in Nine Elms / Battersea).



**Fig. 425:** Canary Wharf, one of the most prominent tall buildings clusters in London and a key financial centre

© King of Hearts, [https://upload.wikimedia.org/wikipedia/commons/c/c5/Canary\\_Wharf\\_from\\_Limehouse\\_London\\_June\\_2016\\_HDR.jpg](https://upload.wikimedia.org/wikipedia/commons/c/c5/Canary_Wharf_from_Limehouse_London_June_2016_HDR.jpg)

- The edges of tall building clusters are particularly important to consider to ensure they continue to appropriately address the scale of neighbouring development and spaces. It is often appropriate to define a clear line of development beyond which tall buildings should be avoided - for example a main road or rail infrastructure corridor or significant open space.
- In particular, tall buildings should avoid acting together to create a canyoning effect along the River Thames which is a significant cultural, historical and recreational resource at the heart of London.

# Appendix B

## Small sites

### B.1 Introduction

This section presents a series of scenarios for options to develop small sites (defined as being under 0.25ha) alongside design guidance for small sites.

The London Plan places significant emphasis on ensuring the potential of small sites are maximised. It recognises that for London to deliver the housing it needs, small sites below 0.25ha must make a substantially greater contribution to new supply across the city, making the delivery of housing from small sites a strategic policy. The London Plan also highlights that increasing provision of housing on these sites will:

- revive the role of small and medium sized developers in delivering new homes;
- diversify the sources, locations, type and mix of housing supply;
- increase housing provision in accessible parts of outer London to help address the substantial housing need in these areas and deliver more affordable homes;
- provide opportunities for custom-build housing and community-led housing projects; and
- support town centre economies.

Policy H2 Small sites of the London Plan is provided in [Fig. 427](#) opposite.

The London Plan provides further guidance on the types of small site developments that should be considered, based on an understanding of the needs of the area. For example, small sites could provide a number of small flats or could be developed to provide family-sized units (three bedrooms +). It also highlights additional considerations such as accessibility (particularly for ground floor homes), impacts on biodiversity and green space and the ability and constraints around provision of affordable homes.

Design guidance is provided in Section [B.3](#).

#### B.1.1 Approach to site selection

#### Policy H2 Small sites

- A** Boroughs should pro-actively support well-designed new homes on small sites (below 0.25 hectares in size) through both planning decisions and plan-making in order to:
- 1) significantly increase the contribution of small sites to meeting London's housing needs
  - 2) diversify the sources, locations, type and mix of housing supply
  - 3) support small and medium-sized housebuilder
  - 4) support those wishing to bring forward custom, self-build and community-led housing
  - 5) achieve the minimum targets for small sites set out in Table 4.2 as a component of the overall housing targets set out in Table 4.1.
- B** Boroughs should:
- 1) recognise in their Development Plans that local character evolves over time and will need to change in appropriate locations to accommodate additional housing on small sites
  - 2) Where appropriate, prepare site-specific briefs, masterplans and housing design codes for small sites
  - 3) identify and allocate appropriate small sites for residential development
  - 4) list these small sites on their brownfield registers
  - 5) grant permission in principle on specific sites or prepare local development orders.

**Fig. 427:** Policy H2 Small sites

© The London Plan

A sample of six small sites locations have been identified in agreement with the Council. Sensitivity differs across the areas, but all present small-scale opportunities for development on an individual plot basis.

#### B.1.2 Approach to scenario development

For the six sites, scenarios for smaller scale types such as infill and conversions/extensions have been development. These are supported by massing models embedded into the plots using the Vu City software.



## B.2 Scenarios

### B.2.1 Brierley Road, Balham

Existing site	
Site Area	0.02 ha
Existing use	Residential
Existing site description	See <a href="#">Fig. 429</a> . One storey post-war block with a largely blank solid façade fronting onto the street adjacent to a modest scale terraced block of two storey post-war houses. The remainder of the street is generally period two storey terrace properties of a larger scale with many attics converted into bedrooms. Small area of hardstanding along the frontage between the building and pavement. Very small rear garden plot.
Existing building types (street)	<ul style="list-style-type: none"> <li>E.1.1 Period terraces</li> <li>E.1.2 Semi-detached</li> </ul>
Planning policy	<ul style="list-style-type: none"> <li>None</li> </ul>
Designations (within 50m)	<ul style="list-style-type: none"> <li>Culverden Road Conservation Area (south and west)</li> <li>Tooting Common Metropolitan Open Land (east)</li> </ul>
Urban design study	
Character area	A2 Balham Residential
Sensitivity	High
Probability	High
Capacity	Medium
Small sites scenario	
Scenario description	See <a href="#">Fig. 431</a> . Three storey row of three storey terraced houses set to the same frontage line as adjacent properties and using materials and details sympathetic to the surrounding period terrace properties. Precedent image included on <a href="#">Fig. 430</a> .
Total residential units	3
Proposed building types	<ul style="list-style-type: none"> <li>E.1.1 Period terraces (new build interpretation of)</li> </ul>
Additional considerations	<ul style="list-style-type: none"> <li>To maintain the quality of the street building frontage, off-street parking or ground floor garage units should be avoided; and planted front gardens with boundary walls encouraged.</li> <li>Rear gardens are small so alternative building types may include two semi-detached properties in the traditional Victorian "L-shape" or a small block of flats.</li> </ul>



Fig. 428: Brierley Road, Balham small sites scenario location plan

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Fig. 429: Brierley Road, Balham small sites existing plot

© 2020 Google.



Fig. 430: Precedent new build terrace row along Balham New Road



Fig. 431: Brierley Road, Balham small sites scenario massing model

## B.2.2 Upper Richmond Road, Putney

<b>Existing site</b>	
<b>Site Area</b>	0.04 ha
<b>Existing use</b>	Residential car parking
<b>Existing site description</b>	See Fig. 432. One storey garage building at the end of a plot, backing onto a railway line to the north of Upper Richmond Road. The building fronting the road is a period detached house of three storeys set back from the to street. Adjacent to the west is a similarly proportioned detached period house fronting the street with four narrow, two storey terraced houses occupying part of the plot to the rear. Surrounding buildings are generally five to six storey apartment blocks of varying configurations and periods.
<b>Existing building types (street)</b>	<ul style="list-style-type: none"> <li>• E.1.3 Detached</li> <li>• E.1.9 Mid-rise flats</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• None</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Parkfields Conservation Area (within)</li> <li>• Landsford Road Conservation Area (north)</li> <li>• 634 Ormond Court locally listed building (east)</li> </ul>
<b>Urban design study</b>	
<b>Character area</b>	D3 West Putney Residential
<b>Sensitivity</b>	High
<b>Probability</b>	Medium
<b>Capacity</b>	Low
<b>Small sites scenario</b>	
<b>Scenario description</b>	See Fig. 434 and Fig. 435. Six storey apartment block replacing existing garages at the rear of the plot adjacent to the railway line.
<b>Total residential units</b>	Depending on the size of the flats (number of bedrooms) this could provide around 24 units.
<b>Proposed building types</b>	<ul style="list-style-type: none"> <li>• E.1.9 Mid-rise flats</li> </ul>
<b>Additional considerations</b>	<ul style="list-style-type: none"> <li>• Depending on housing priorities in the local area, the plot could also be developed as two family size units (3 bedrooms +).</li> <li>• Access to the plot will require careful consideration to ensure it does not detract from the existing street scene along Upper Richmond Road.</li> </ul>



Fig. 432: Existing potential access to the scenario via Upper Richmond Road  
© 2020 Google.

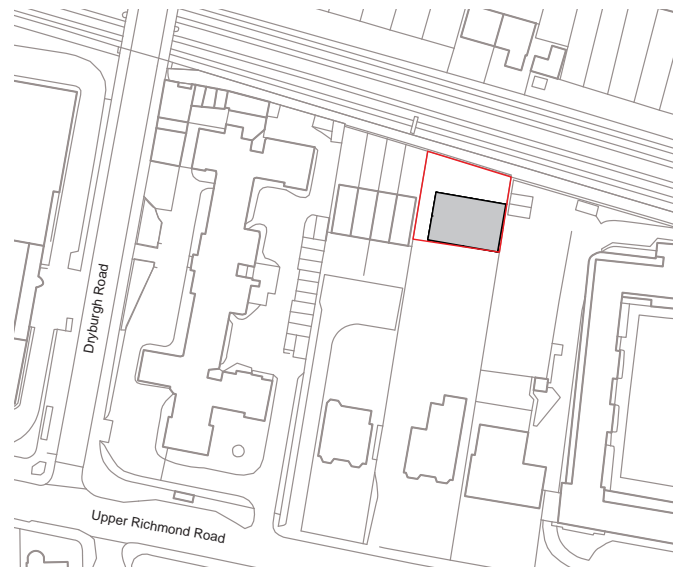


Fig. 433: Upper Richmond Road plan  
© Crown copyright and database right 2021



Fig. 434: Upper Richmond Road small sites scenario model



Fig. 435: Upper Richmond Road small sites scenario model

## B.2.3 Old Telephone Exchange, Balham

Existing site	
Site Area	0.13 ha
Existing use	Former telephone exchange
Existing site description	See Fig.437. Five storey former telephone exchange dating from the 1960s, set slightly back from the street close to Balham High Road. Small area of raised hardstanding along the frontage between the building and pavement. Adjacent to a four storey modern block of flats (identical flats occur on opposite side of the street). Towards Balham High Road buildings are a mixture of retail and light industrial, ranging from one to four storeys. The remainder of the street is generally period terrace properties of two to three storeys. Small area of parking and a garden area to the rear of the plot.
Existing building types (street)	<ul style="list-style-type: none"> <li>E.1.1 Period terraces</li> <li>E.1.5 Low-rise flats</li> <li>E.1.14 Small scale light industrial</li> </ul>
Planning policy	<ul style="list-style-type: none"> <li>None</li> </ul>
Designations (within 50m)	<ul style="list-style-type: none"> <li>Old Devonshire Road Conservation Area (north and east)</li> <li>24 and 26 Old Devonshire Road grade II listed building</li> </ul>
Urban design study	
Character area	A2 Balham Residential
Sensitivity	High
Probability	High
Capacity	Medium
Small sites scenario	
Scenario description	See Fig.438. Removal of the existing building and erection of a six storey apartment block, with upper two floors set back to preserve the consistency of the building frontage and roofline along the street.
Total residential units	A mix of one and two bedroom apartments could provide around 36 units.
Proposed building types	<ul style="list-style-type: none"> <li>E.1.9 Mid-rise flats</li> </ul>
Additional considerations	<ul style="list-style-type: none"> <li>With the existing height of the telephone exchange building, flats are the most likely scenario in this position. However, the sizing of the flats could be adjusted to suit the local housing demands.</li> <li>There may be potential to provide some affordable housing in this location.</li> </ul>

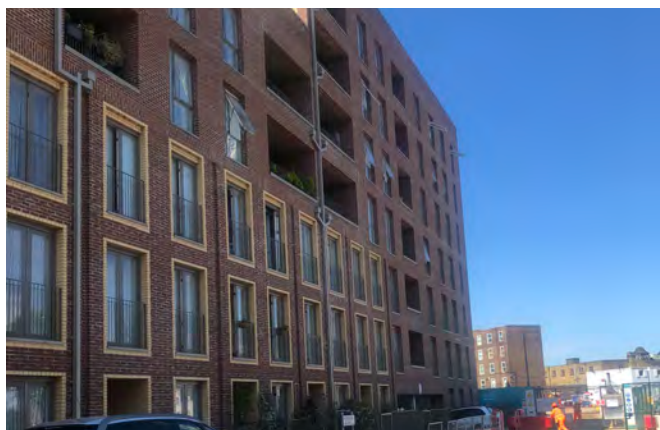


Fig. 439: Precedent mid-rise new build flats near Clapham Junction

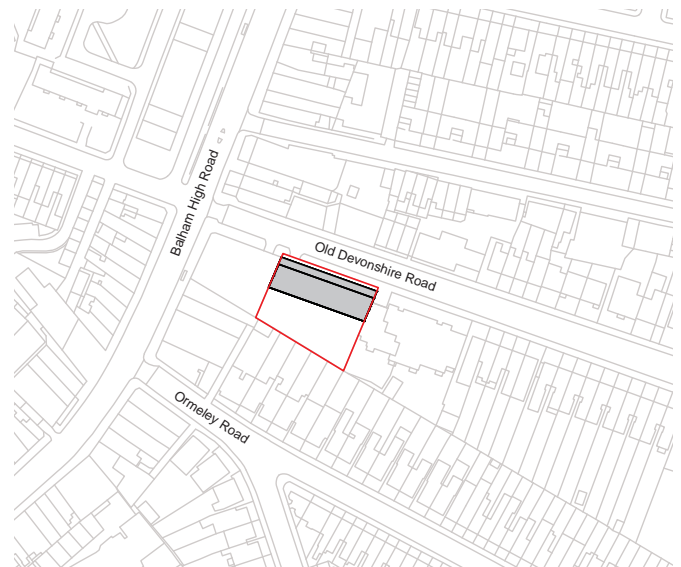


Fig. 436: Old Telephone Exchange small sites location plan

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Fig. 437: Old Telephone Exchange small sites existing site

© 2020 Google.



Fig. 438: Old Telephone Exchange small sites massing model

## B.2.4 Parma Crescent, Battersea

Existing site	
<b>Site Area</b>	0.03 ha
<b>Existing use</b>	Residential
<b>Existing site description</b>	See <a href="#">Fig.441</a> . Corner plot on the junction between Parma Road and Eccles Road. Two-storey residential end of terrace property set back from the remainder of the terrace with a large side garden plot to end of road. Terrace is a post-war infill development within a street of Victorian terraces. Buildings on the surrounding streets are also two-storey Victorian terraces, often with converted attics.
<b>Existing building types (street)</b>	<ul style="list-style-type: none"> <li>• E.1.1 Period terraces (post-war development)</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• None</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Clapham Common Conservation Area (east)</li> <li>• Clapham Junction Conservation Area (west)</li> <li>• Battersea District Building grade II listed building (east)</li> </ul>
Urban design study	
<b>Character area</b>	B1 Battersea Residential
<b>Sensitivity</b>	High
<b>Probability</b>	High
<b>Capacity</b>	Medium-High
Small sites scenario	
<b>Scenario description</b>	See <a href="#">Fig.442</a> . Development of a single residential dwelling in the side garden plot adjacent to the existing end terrace house. Care would need to be taken to ensure a positive frontage onto Eccles Road as well as the set back frontage to Parma Crescent.
<b>Total residential units</b>	1
<b>Proposed building types</b>	<ul style="list-style-type: none"> <li>• E.1.1 Period terraces (new build interpretation of)</li> </ul>
<b>Additional considerations</b>	<ul style="list-style-type: none"> <li>• Increased density on a large scale could be achieved through redevelopment of the whole run of post-war terraces, which are smaller in scale than the older Victorian properties adjacent along the streets. This could deliver terraced houses, flats or a mix of the two.</li> <li>• If the housing market required more larger family homes then the plot could also accommodate an extension to the existing end terrace dwelling.</li> </ul>



**Fig.440:** Parma Crescent, Battersea small sites plan

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**Fig.441:** Parma Crescent, Battersea small sites existing site

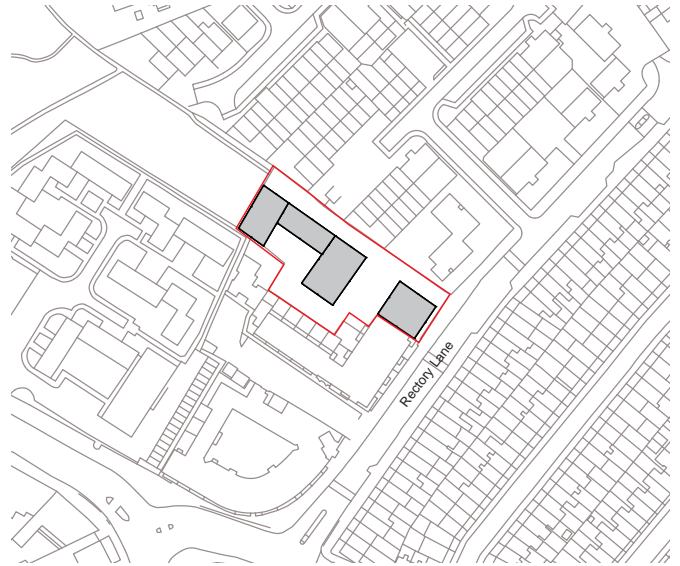
© 2020 Google



**Fig.442:** Parma Crescent, Battersea small sites scenario model

## B.2.5 Rectory Lane, Tooting

<b>Existing site</b>	
<b>Site Area</b>	0.25 ha
<b>Existing use</b>	Open space and circulation within residential area
<b>Existing site description</b>	See <a href="#">Fig. 444</a> . Narrow plot of green space with hardstanding and a small building beyond; adjoining onto a linear green space at the rear of other properties that leads to St Nicholas Church. The site has some mature trees within and adjacent to it. Adjacent plots include modern flats and post-war low density terraces; with period terraces opposite.
<b>Existing building types (street)</b>	<ul style="list-style-type: none"> <li>• E.1.1 Period terraces</li> <li>• E.1.5 Low-rise flats</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• None</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• St Nicholas Churchyard grade II listed building and protected open space (west)</li> </ul>
<b>Urban design study</b>	
<b>Character area</b>	F2 Tooting Residential
<b>Sensitivity</b>	Low
<b>Probability</b>	High
<b>Capacity</b>	High
<b>Small sites scenario</b>	
<b>Scenario description</b>	See <a href="#">Fig. 445</a> . Development of the plot, including removal of the existing building and replacing with a six storey apartment block with set back in the upper two floors. One block to sit along the road frontage adjacent to the existing flats.
<b>Total residential units</b>	Approximately 48 units depending on the scale of the individual units.
<b>Proposed building types</b>	<ul style="list-style-type: none"> <li>• E.1.9 Mid-rise flats</li> </ul>
<b>Additional considerations</b>	<ul style="list-style-type: none"> <li>• There will be a number of different configurations to the layout within this site, which could include a small number of individual mid-sized or large family homes.</li> </ul>



**Fig.443:** Rectory Lane small sites plan

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**Fig.444:** Rectory Lane small sites existing site

© 2020 Google.



**Fig.445:** Rectory Lane small sites massing model



## B.2.6 Dental Surgery

Existing site	
<b>Site Area</b>	0.05 ha
<b>Existing use</b>	Medical / leisure
<b>Existing site description</b>	See <a href="#">Fig. 449</a> . Small plot of land between Jaggard Way and the railway line close to Wandsworth Common train station. The plot currently has a period two storey block on it with commercial uses (doctor's surgery and leisure uses) with quality brickwork and details such as sash windows. It sits opposite the rear of period semi-detached properties, with fences and garage blocks.
<b>Existing building types (street)</b>	<ul style="list-style-type: none"> <li>• E.1.1 Period terraces</li> <li>• E.1.2 Semi-detached</li> <li>• E.1.20 Community/leisure</li> </ul>
<b>Planning policy</b>	<ul style="list-style-type: none"> <li>• Employment Protection Area</li> </ul>
<b>Designations (within 50m)</b>	<ul style="list-style-type: none"> <li>• Wandsworth Common Conservation Area (within)</li> <li>• Wandsworth Common Metropolitan Open Land (south and west)</li> </ul>
Urban design study	
<b>Character area</b>	G2 Wandsworth Common and Residential
<b>Sensitivity</b>	High
<b>Probability</b>	Medium
<b>Capacity</b>	Medium-Low
Small sites scenario	
<b>Scenario description</b>	See <a href="#">Fig. 447</a> and <a href="#">Fig. 448</a> . Upward extension of the existing two storey building with an additional two floors. Existing commercial / leisure uses retained on ground floor.
<b>Total residential units</b>	Approximately 12 units depending on the scale of the individual units. This assumes retention of the commercial / leisure uses across the lower two floors.
<b>Proposed building types</b>	<ul style="list-style-type: none"> <li>• E.1.5 Low-rise flats</li> </ul>
<b>Additional considerations</b>	<ul style="list-style-type: none"> <li>• Depending on the viability of the commercial premises than a different commercial/residential mix could be explored at the site, including consideration of more commercial space.</li> <li>• Houses as opposed to flats are unlikely to be suitable with the limited space for garden plots and the protection of the employment uses.</li> </ul>



**Fig. 449:** Dental surgery small sites existing site  
© 2020 Google.



**Fig. 446:** Dental surgery small sites plan  
© Crown copyright and database right 2021



**Fig. 447:** Dental surgery small sites massing model



**Fig. 448:** Dental surgery small sites massing model

### B.3 Small sites design guidance

Small sites provide the opportunity for communities to grow organically while maintaining their original character or evolving it incrementally based on a consensual approach. The following sections provide guidance for design on small sites.

There are a number of useful sources of information on developing small sites within London, in addition to the policies provided in the London Plan:

#### **Good Quality Homes for All Londoners, Draft Supplementary Planning Guidance (SPG)**

At the time of writing the following documents are available as pre-consultation drafts (2020):

#### **Module B: Small housing developments: assessing quality and preparing design codes**

[https://www.london.gov.uk/sites/default/files/hdspg\\_2020\\_module\\_b.pdf](https://www.london.gov.uk/sites/default/files/hdspg_2020_module_b.pdf)

#### **Module C: Housing design quality and standards**

[https://www.london.gov.uk/sites/default/files/hdspg\\_2020\\_module\\_c.pdf](https://www.london.gov.uk/sites/default/files/hdspg_2020_module_c.pdf)

#### **Module D: Housing design case studies and appendices**

[https://www.london.gov.uk/sites/default/files/hdspg\\_2020\\_module\\_d\\_part\\_1.pdf](https://www.london.gov.uk/sites/default/files/hdspg_2020_module_d_part_1.pdf)

#### **B.3.1 Context**

- The design of the building must blend in with the surrounding context, unless it can be demonstrated that a different approach would not negatively impact the character of the street.
- The design must take into consideration the functional character of the street that includes access, circulation, land use etc.
- In certain areas, where the need for evolving the existing character is identified, the design must consider the objectives of doing so and help setting a precedent for similar developments in the future.

#### **B.3.2 Massing**

- A volumetric study must be carried out for the buildings surrounding the site to ascertain the general massing attributes such as height, depth, recesses, roof lines to develop the emerging form of the proposed building.
- The building form does not need to mimic the

existing buildings but must be a well articulated interpretation of the predominant urban character that is adapted to the functional and aesthetic requirements of the proposed development.



**Fig.451:** Earl's court house - A contemporary design on a small site that discreetly slots into a conservation area.

© Annabel Elston

- The building massing must demonstrate that good levels of daylight, sunlight and privacy are achieved without adversely affecting the internal comfort parameters of the surrounding buildings.
- Where possible, the design must explore the potential to provide an additional floor(s) to accommodate higher density or additional units without compromising the character and comfort of the surrounding buildings or public realm.



**Fig.450:** Selfridges, Duke street - The new addition respects existing massing and materials while incorporating a contemporary architectural character.

© Simon Menges

### B.3.3 Materials

- A visual assessment must be carried out to ascertain the predominant materials used in the vicinity of the site to develop a range of colour tones and textures that can be used for the proposed structure.
- The proposed materials must be durable, of high quality and must age well to properly integrate into the wider context.
- The objective of the proposed building materials to blend in visually within their context must not limit their functional performance or in adoption of construction technology that minimises their environmental impact.



**Fig. 452:** Shepherdess walk - The project blends into the existing context through the effective use of complementary materials as well as architectural articulation.

© Héliène Binet

### B.3.4 Heritage

- The proposed design must take into account the presence of any heritage structures in the vicinity in addition to any relevant heritage designations such as conservation areas. Where a site is within a CA it should follow guidance within the conservation area character appraisals and management plans.
- The proposed architectural character and materials must respect the tonality, texture and rhythm of the adjacent heritage buildings so as not to contrast with their character.
- The prominence of a heritage building as a visual and cultural landmark within the neighbourhood

must be respected. The proposed building design must be subservient to the architectural and functional hierarchy of the heritage structure.

- The new buildings that are constructed today shall play an important role in evolving the heritage of their place and shall be representative of the early 21<sup>st</sup> century architecture and urban character. They must be designed to last, which must be reflected in their detailing, craftsmanship, quality and durability.



**Fig. 453:** New terraced homes adjacent to period terraces at Hafer Road



**Fig. 454:** While the detailing of this building along Wandsworth High Street uses sympathetic materials, its mass conflicts with the proportions of the street and the landmark heritage asset of the church to the right.

### B.3.5 Frontage

- The building design must respect the existing frontage line established in a street.
- The building frontage includes the predominant frontage line at the street level as well as any setbacks beyond a certain height.
- For plots located at street corners, there may be a consideration to step out of the frontage line to highlight or define the street corner. This needs to be considered as part of the overall wayfinding strategy and spatial hierarchy of the area.



Fig. 456: Art Deco style new build along Putney High Street, with the frontage expressing itself to mark a strategic junction

### B.3.6 Detailing

- A study of the predominant architectural character of the buildings around the site must be carried out to ascertain the key design elements such as windows, doors, lintels, roof gable details, chimneys. The proposed design must incorporate/re-interpret these elements to blend in with the existing architectural character.
- The proposed design must respond to the vertical and horizontal rhythm and scale of fenestrations, recesses or projections along the street to maintain continuity of the street character.
- Where a predominant architectural style is absent, the design must set a precedent for human scale design, optimal functionality and high quality workmanship for future developments.



Fig. 455: Sensitive extensions to period terraced houses fronting onto the River Thames in Putney



Fig. 457: Quality materials and interesting details taking the clues from the sites historic uses and culture, along Wandsworth High Street

- Where large period homes are converted into flats particular care needs to be taken on details such as electricity cabinets, cabling, entrance buzzers and bin storage. When poorly considered these can be significantly detrimental to the whole street scene.



**Fig. 458:** The new development on Stapleton road, constructed on a heavily constrained site blends into its Victorian styled neighbours while providing a contemporary and flexible layout internally.  
Fig.459: © Solidspace

# Appendix C

## Methodology

### C.1 Overview

This appendix sets out the methodology this study has followed.

#### C.1.1 Summary

A flow chart summarising the methodology is provided on the following page ([Fig. 460](#)). The first stage of the methodology establishes a full understanding of the context and policy background, in national planning policy and design guidance, and in Wandsworth's current policies and evidence base studies.

The process of characterisation and evaluation is grounded in industry guidance set out by the Landscape Institute, Natural England and the GLA in dividing the borough into character areas, and subsequently describing and evaluating them, drawing out valued features and negative aspects for enhancement. Character areas and their key characteristics and qualities are verified on site and through stakeholder engagement.

In order to assess capacity for growth, the sensitivity of the character areas is assessed, by considering their value and susceptibility to change, using the Landscape Institute industry guidance for assessing sensitivity. This process establishes high sensitivity areas unlikely to have capacity for development; areas of medium and low sensitivity with the potential for targeted or larger scale growth, where development may provide a positive contribution. Simultaneously, the 'probability' of change is assessed, analysing the borough in terms of aspects such as public transport accessibility, land availability and planning policies. The sensitivity and probability are then overlaid to understand the potential development capacity of individual character areas.

We then test different types of scenarios, modelling hypothetical 'tall building developments' to understand how they might fit into the existing character and urban grain. The testing considers likely development potential, appropriate optimal massing/height ranges and the potential impacts they may have. A similar process is undertaken for smaller sites: for these we test types of development appropriate to the individual

context of each sample site.

The outcome of this process is design principles and recommendations indicating where in the borough tall building development is likely to be appropriate and opportunities for guiding and enhancing design qualities in these areas.

### C.2 Guidance

The method for undertaking the characterisation and capacity study follows guidance by the Landscape Institute, Natural England and the GLA. The key documents are listed below:

- London Plan, 2021);
- An Approach to landscape character assessment, Natural England, 2014;
- Character and context, Supplementary Planning Guidance, GLA, 2014;
- Guidelines for Landscape and Visual Impact Assessment (GLVIA3), Landscape Institute and IEMA, 2013;
- London View Management Framework SPG; and
- National design guide, MHCLG, 2019.

#### C.2.1 London Plan

At the time of writing, the latest version of the London Plan was published in 2021. The key policies which have guided the study are listed below.

- D1 London's form, character and capacity for growth;
- D2 Infrastructure requirements for sustainable densities;
- D3 Optimising site capacity through the design-led approach;
- D9 Tall buildings; and
- H2 Small sites.

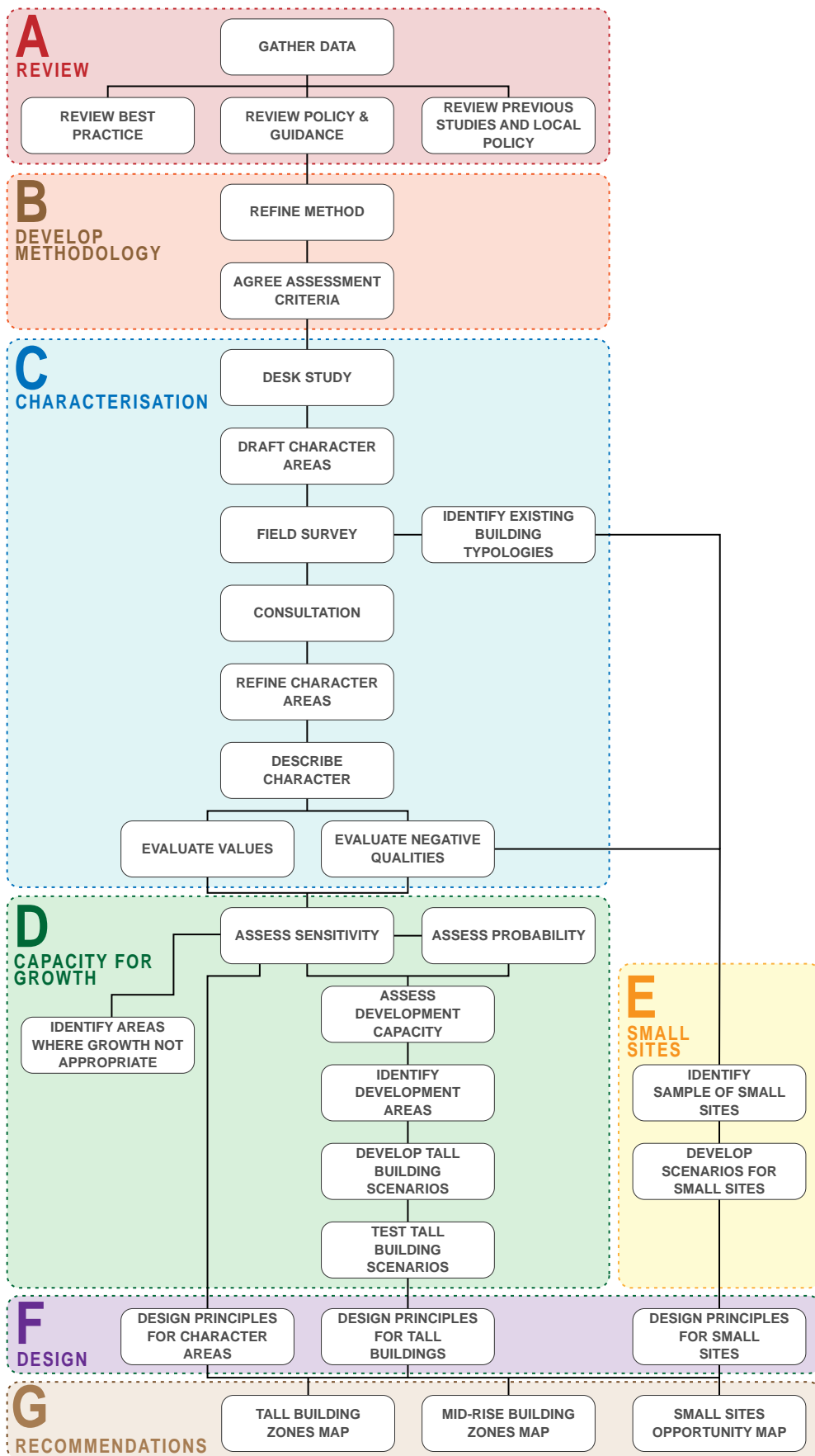


Fig. 460: Methodology overview

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## C.3 Review

The first stage of the study involves developing a full understanding of the context and policy background.

This includes a review of national and regional policy (the London Plan), national design guidance and London Plan Supplementary Planning Guidance (SPGs). It also includes reviewing information from the Council's Local Plan evidence base documents and their methodologies, enabling an understanding of what work has been undertaken to date, how it might inform the study and any potential gaps the study may need to fill.

### C.3.1 Documents

The key documents reviewed are listed in [Table 3](#) with a brief summary as to how they have informed the study. They include:

- Wandsworth Borough Council evidence base – key documents:
  - Tall buildings study;
  - Opportunity Area Planning Framework;
  - Core Strategy;
  - Conservation Area appraisals; and
  - Local Plan areas of future change or potential for change.
- Characterisation studies from neighbouring authorities:
  - London Borough of Merton
  - London Borough of Lambeth;
  - London Borough of Richmond.

Title/author/date	Overview	Relevant content	Analysis
<b>Townscape</b>			
<b>Stage One Urban Design Statement – Tall Buildings, WBC, 2009</b>	<p>Sets out a summary of the baseline documents used for the tall buildings study.</p> <p>Broad overview of locations for tall buildings.</p>	<p>Paragraph descriptions of the borough's five identified town centres (Balham and Tooting, Battersea and Clapham Junction, Putney, Roehampton and Wandsworth).</p> <p>Baseline documents referred to:</p> <ul style="list-style-type: none"> <li>• Development briefs for riverside sites (Various)</li> <li>• Wandsworth Unitary Development Plan, Adopted August 2003</li> <li>• Supplementary Planning Guidance on Important Local Views, November 2003</li> <li>• Guidelines for Thames Riverside development in Wandsworth 1990</li> <li>• Urban Design Framework for the Wandle Delta, March 1999</li> <li>• Clapham Junction Town Centre Conservation Area (CLTCCA) Urban Design Framework/Action Plan, January 1999</li> <li>• CJTCCA Town Centre Public Consultation Newsletter, May 1999</li> <li>• CJTCCA Character Appraisal and Management Strategy, 2008</li> <li>• Wandsworth Town Centre Conservation Area Character appraisal and Management Strategy, 2006</li> </ul>	<p>Most of the baseline documents are now out of date.</p> <p>The overview paragraphs of the five town centres will be useful as a starting point as they are largely historic evolution descriptions.</p> <p>Content for tall buildings locations is very broad and focuses on existing town centres.</p>
<b>Stage 2 Urban Design Study: Tall Buildings, WBC, 2001</b>	<p>Report identifying areas for tall buildings including maps.</p>	<p>Detailed analysis of areas that may be appropriate for tall buildings</p> <p>Areas identified as potential for tall buildings are within the town centres, Nine Elms and focal points of activity. Namely:</p> <p>Wandsworth Town Centre Clapham Junction Town Centre Putney Town Centre and Putney Wharf Focal Point Balham Town Centre Tooting Town Centre Nine Elms near Vauxhall Wandsworth Riverside Quarter Focal Point and Wandle Delta Proposed Focal Point Ransomes Dock Focal Point Battersea Power Station Proposed Focal Point</p> <p>Detailed urban characterisation (of each area). Some valuable information on valued features which will be useful for the character area study. Boundaries for each area are based on the 'town centre' designation from the Local Plan. Useful conclusions on tall building suitability but very high level and desk-based. Maps showing clear areas for tall building appropriateness: (inappropriate; sensitive; appropriate).</p>	<p>Study is a good starting point.</p> <p>Character areas: useful information for the 11 areas identified - to use in the character area descriptions.</p> <p>Tall buildings: Focal points: these areas (in addition to town centres) – are designated as locations where tall buildings are likely to be appropriate. Useful starting point for 'opportunity areas' – to be extended/expanded on.</p>
<b>Local views SPD, WBC, 2014</b>	<p>Report identifying 6 local views</p>	<p>View 1: Upstream from Putney Bridge View 2: Downstream from Battersea Bridge View 3: Downstream from Albert Bridge View 4: Battersea Power Station from Chelsea Bridge View 5: From Queenstown Road to Battersea Power Station View 6: Battersea Power Station from Battersea Park</p> <p>Each view contains a photo, map and description of the view. SPD requires management of the views along the lines of the LVMF</p>	<p>Character areas: record views in the relevant character area descriptions.</p> <p>GIS: The views are currently not available in GIS - to map.</p> <p>Viewsheds are not provided but there is some detail in the descriptions.</p>



Title/author/date	Overview	Relevant content	Analysis
<b>Town Centres SPD, WBC, 2015</b>	SPD providing further information on specific town centre uses	Public houses Highlights the community value and local architectural value of the borough's public houses - they represent important buildings/ focal points for areas and local communities. Maps all public houses.  Also contains a list and schedule of all schools.	Character areas: may be useful including information for specific pubs.
<b>Historic</b>			
<b>Historic environment SPD, WBC, 2016</b>	Historic environment, historic evolution and general timeline of the borough	Appendix 1 provides character area descriptions (see below). Useful overview of the character of the borough. Historic time line of the borough provided. Overlap with neighbouring boroughs: (para. 10.2) Setting of Westminster World Heritage Site Battersea/Battersea/ Square/Battersea Park/Nine Elms Lavender Hill - Queenstown Road/Clapham Junction/Clapham Common and between the Commons/Bellvue Road/St John's Hill Wandsworth/Wandsworth Common/Wandsworth Town/Clapham South/Balham Tooting Bec (Upper Tooting)/Streatham Park and Furzedown/Tooting (Lower Tooting) Earlsfield/Southfields/Wimbledon Parkside East Putney/Putney Town Centre/West Putney Roehampton/Putney Vale River Thames	Character areas: The descriptions in Appendix 1 will be very useful information for character areas. There are no boundaries drawn for these areas.  Borough-wide overview: useful information for the borough overview section. Historic time line
<b>Conservation area appraisals, WBC, various dates</b>	46 conservation areas. In 2 parts: character analysis and management statement	The CAAMS typically include: A map of the Conservation Area / character analysis / important views if there are any / heritage assets / architectural details.  The character analysis is made up of Spatial character: townscape / streetscape / views / greenspace and Architectural character: monuments & buildings / named special and or listed buildings.	Character areas: detailed information on the history, special qualities and positive and negative contributions of buildings.
<b>Nine Elms</b>			
<b>Vauxhall Nine Elms Battersea: Opportunity Area Planning Framework, GLA, 2012</b>	High level analysis, strategy and vision for the area	Tall buildings: sets out locations for tall buildings (some now built out).  Context information.	
<b>Area Spatial Strategy for Nine Elms, WBC, 2016</b>		See SSAD below. The London Plan identifies the area as part of Vauxhall/Nine Elms/ Battersea (VNEB) Opportunity Area within the Central Activities Zone (CAZ) - extending into Lambeth. It is now referred to as Nine Elms Vauxhall (NEV).	
<b>Battersea Design and Tech Quarter Economic Appraisal &amp; Design Framework, We Made That, 2020</b>	Strategic guidance for the Battersea Design Quarter	Urban appraisal of the study area including engagement with council/ local stakeholders. Aims for intensification of industrial uses. Proposes potential major improvements for transport – including pedestrian/ cycle links, realignment of Queenstown Road/ Silverthorne Road junction. Open spaces Key focal points	Character areas: Feed the findings and proposals in to the relevant character area.
<b>The Placebook: Battersea Power Station, Battersea Power Station Development Company and JTP, 2014</b>	A masterplan vision for the Battersea Power Station site		
<b>Nine Elms Placemaking Framework (Draft), AR Urbanism, 2019</b>	Placemaking Framework for Nine Elms	Includes detailed baseline section.	Character areas: detailed information to feed in to the character descriptions and design guidance.
<b>Roehampton</b>			
<b>Roehampton SPD, WBC, 2015</b>	Guidance for all future development in the area – focuses on the western side of the Alton estate.	Additional guidance on how policies should be implemented in the area. References the Alton area masterplan (2014) by Biffinger GVA and Studio Egret West (including supporting baseline report and masterplan report) – translating it into planning policy guidance.  Baseline urban design analysis – including positive and negative features. Vision – and strategic principles for future development. Defines 'intervention areas' including a 'local centre' and development sites.	Character areas: detailed information to feed in to the character descriptions and design guidance.  Some of the area has a recent planning application.
<b>Lombard Road/ York Road Riverside Focal Point SPD, WBC, 2015</b>	Area Spatial Strategy and guidance for the area to become a focal point of activity	Provides guidance for 16 sites within the area and how new development could be designed, including sites in the SSAD and six additional sites.  Contains detailed design principles, including some commentary on tall buildings.	Character areas: description (future baseline) and design guidance.  Tall buildings: review detail in relation to tall buildings, scenarios and principles.

Title/author/date	Overview	Relevant content	Analysis
<b>Lombard Rd/ York Rd Riverside Focal Point Area: guidance in relation to arts and culture provision, WBC, 2018</b>	Provides guidance and ideas on how to incorporate art and culture in the area	Links to SPD and other policy – may be some useful detail for design guidance.	Character areas: design guidance – detailed information.
<b>The Neighbourhood Area boundary map, Tooting Neighbourhood Forum, 2017</b>	Boundary map for Neighbourhood Plan area and characterisation study.	Baseline and public consultation results.	Consider using as a character area/ 'place' boundary
<b>Local Plan</b>			
<b>Core Strategy, WBC, 2016</b>	Core strategy - vision and spatial strategy	<p>Area specific policies for places:</p> <ul style="list-style-type: none"> <li>-PL 9 River Thames and the riverside (including Thames Policy Area, Putney Embankment special recreational character and function)</li> <li>-PL 10 the Wandle Valley</li> <li>-PL 11 Nine Elms and north east Battersea</li> <li>-PL 12 Central Wandsworth and the Wandle Delta</li> <li>-PL 13 Clapham Junction and the adjoining area (including Northcote Road area of special shopping character)</li> <li>-PL 14 East Putney and Upper Richmond Road</li> <li>-PL 15 Roehampton</li> </ul> <ul style="list-style-type: none"> <li>-PL 1 Attractive and distinctive neighbourhoods and regeneration initiatives</li> <li>-PL 4 Open space and the natural environment</li> <li>-PL 5 Provision of new homes</li> <li>-PL 6 Meeting the needs of the local economy</li> <li>-PL 8 Town and local centres</li> <li>-IS 1 Sustainable development</li> <li>-IS 3 - Good quality design and townscape (tall buildings locations; protection of views of Westminster WHS and local views SPD)</li> </ul>	Places: areas may be useful basis
<b>Site Specific Allocations Document (SSAD), WBC, 2016</b>	Sets out the main sites where development is anticipated and the Council's aspirations for these	Contains Area Spatial Strategies for the areas of greatest change: Nine Elms; Central Wandsworth and the Wandle Delta (Central Wandsworth, Land between Armoury Way and the railway, Wandle Delta); Clapham Junction; Putney (Putney Town Centre north, Putney Town Centre south (Upper Richmond Road)); Roehampton; Ransome's Dock.	Character areas
<b>Development Management Policies Document, WBC, 2016</b>	Provides further detail on policies	Further detail on key aspects to understand the Council's priorities for future development and management of the borough.	
<b>Employment and Industry Document, WBC, 2018</b>	Policy for employment and industry.	Further detail on key aspects to understand the Council's priorities for future development and management of employment and industry in the borough.	
<b>Open space study, Atkins, 2007</b>			
<b>Adjacent local authorities and regional</b>			
<b>Lambeth Local Distinctiveness Study, Lambeth Borough Council, 2012</b>	Baseline of Lambeth's local distinctiveness – primarily built form	Analysis on the basis of a sample of areas/sites. Borough has been broken down into simple townscape types: City centre/urban/suburban.	<p>Building types – inform building typology for consistency (as relevant)</p> <p>Character areas: Inform adjoining character areas where relevant</p> <p>Tall buildings scenarios assessment if they are in proximity to Lambeth</p>
<b>Lambeth tall building study, Lambeth Borough Council, 2014</b>	Identifies areas in the borough that are appropriate/ sensitive/ inappropriate for tall development	<p>Contains map of local views – note one in the south west of Lambeth with the viewshed extending to the Furzedown area of Wandsworth. Others in Nine Elms (picked up the opportunity area baseline analysis).</p> <p>Concludes that no areas of Lambeth are identified as appropriate for tall buildings.</p>	Tall buildings scenarios: Refer to local protected views/ other heritage information for sites on the border with Lambeth.

<b>Title/author/date</b>	<b>Overview</b>	<b>Relevant content</b>	<b>Analysis</b>
<b>Wandle Valley Conservation Area character assessment, Merton Borough Council, 2007</b>	Character assessment	History of the Wandle – useful detail Separate documents for detailed assessments of the sub-areas of the Wandle Valley area	Character areas – refer to for the character areas adjacent to Wandle Valley
<b>Merton Borough Character Study, Merton Borough Council, 2015</b>	Pre-consultation draft. Only some of the areas have been completed to date	The borough has been divided into five areas (Wimbledon, Colliers Wood/South Wimbledon and Mitcham border Wandsworth borough). The sub-areas have been subdivided into 36 local neighbourhoods.	Character areas: potentially use to inform adjacent areas although only some areas appear to be available online.
<b>London's Natural Signatures, 2011</b>	Regional characterisation (London)	Divides London into character areas known as 'natural signatures' and contains high level character information. 4 areas within Wandsworth: areas 13, 15, 16 and 17.	Relatively old document but provides a useful strategic starting point for broad characterisation.
<b>LVMF, GLA, 2012</b>		Description of the one designated view in Wandsworth.	

**Table 3 Key documents reviewed**

## C.3.2 Data

The data used in the study is listed in [Table 4](#).

Data name	Source	Data name	Source
1:25,000 Ordnance Survey map raster data	Provided by Wandsworth	Housing	DataShine Census
OS open data map base	Ordnance Survey	Local centres	Provided by Wandsworth
MasterMap OS	Provided by Wandsworth	TPO	Provided by Wandsworth
Local authority boundaries	data.gov.uk	Open space type	Provided by Wandsworth
County boundaries	data.gov.uk	Wandle Valley Regional Park	Provided by Wandsworth
BGS Geology data	British Geological Society	Green chain	Provided by Wandsworth
Topography	Defra Survey	Green infrastructure	Ordnance Survey
National Character Areas	data.gov.uk	Tube stations	TFL
National Cycle Network/ sustrans	Provided by Wandsworth	Rail station	Ordnance Survey-Zoomstack
PTAL	London Datastore	Road network (national, local, regional)	Ordnance Survey-Zoomstack
London cycle network	data.gov.uk	Railway line	Ordnance Survey-Zoomstack
EA flood zones	data.gov.uk	Emissions	London Datastore
Main watercourses	Ordnance Survey	Building heights	Emy Analytics
Local Nature Reserves (England)	data.gov.uk	Land in public sector ownership	Provided by Wandsworth
Local ecological designations e.g. SINCI/ SNCI	London Datastore	London Green Blue cover	London Datastore
National Habitat Networks (England)	data.gov.uk	Cultural Venues	London Datastore
Priority Habitat	data.gov.uk	Schools	London Datastore
Ancient woodland	data.gov.uk	Leisure centres	Provided by Wandsworth
Listed buildings	Historic England	Aerial map base	Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community
Local listings (if held)	Provided by Wandsworth	Archaeological Priority Areas	data.gov.uk
Scheduled Monuments	Historic England	Other Larger Protected Open Spaces	Provided by Wandsworth
Registered Parks and Gardens	Provided by Wandsworth	Area Spatial Strategy	Provided by Wandsworth
Conservation Areas	Provided by Wandsworth	Central Activities Zone	Provided by Wandsworth
Archaeology designations (if held?)	Provided by Wandsworth	Crossrail 2	Provided by Wandsworth
Historic mapping/ photos (if held)	Provided by Wandsworth	Economic Use Intensification Area	Provided by Wandsworth
MOL	Provided by Wandsworth	Focal Point	Provided by Wandsworth
LPA planning policy data and development areas	Provided by Wandsworth	Industrial Business Park	Provided by Wandsworth
GLA opportunity areas	London Datastore	Local Centres	Provided by Wandsworth
GLA London Plan SHLAA sites and accompanying spreadsheet (if this is held)	Provided by Wandsworth	Locally Significant Industrial Areas	Provided by Wandsworth
Green infrastructure/ open space sites and datasets	Provided by Wandsworth	Nine Elms Vauxhall Opportunity Area	Provided by Wandsworth
Any existing hierarchy of places e.g. town centre hierarchy	Provided by Wandsworth	Northcote Road Area of Special Shopping Character Poly	Provided by Wandsworth
Potential areas of future change	Provided by Wandsworth	Putney Embankment Policy Area	Provided by Wandsworth
Indices of multiple deprivation (IMD)	Provided by Wandsworth	Strategic Industrial Locations	Provided by Wandsworth
Population	DataShine Census	Town Centres	Provided by Wandsworth
Age	DataShine Census	Trees	London Datastore
Ethnicity	DataShine Census	Noise	London Datastore

**Table 4** GIS data reviewed

## C.4 Characterisation methodology

### C.4.1 Desk study

Desk study of existing published information and data is used to determine broad character areas for verification in the field and through consultation.

### C.4.2 Field survey

Field surveys were carried out in June-August 2020 and in June 2021. The field surveys focussed on obtaining perceptual aspects of character, verifying desk study and contributing to valued features/negative qualities. Site observations also captured features of local distinction and aspects of townscape quality and condition. Field survey information was captured in digital form and bespoke proformas: a template of the proforma is provided in [Appendix F](#).

### C.4.3 Consultation

Public consultation was carried out in September 2020 (refer to [Appendix G](#)). The consultation invited feedback on the places and place names, character area boundaries and descriptions, valued features and future strategies for the character areas. The feedback from the consultation was fed back into the study to refine character area boundaries, places, valued features, negative qualities and design guidance.

### C.4.4 Characterisation

Characterisation is the process of dividing the borough into character areas, and defining the boundaries of those areas based on a transparent process.

The characterisation hierarchy consists of three levels:

- broad characterisation of ‘places’;
- a more fine-grained division of ‘character areas’; and
- where relevant, ‘sub-areas’ may also be discussed to help describe particular parts of a character area.

#### ***Broad characterisation – ‘places’***

The broad characterisation stage divides the borough into high-level ‘places’. The purpose of this layer of categorisation is to reflect a ‘sense of place’ as well as identifying areas recognised as ‘places’ by local people. The following data sources are used as a basis for defining the places:

- ward boundaries;
- Local Plan including the core strategy and site specific allocations document (including area spatial

strategies);

- draft areas being identified by the council for the new Local Plan as the basis for potential place-based strategies;
- existing town centres; areas of regeneration including the opportunity area; and
- broad areas identified in published studies including the tall buildings study.

The list of places is as follows:

- a. Balham
- b. Battersea
- c. Earlsfield and Southfields
- d. Putney
- e. Roehampton
- f. Tooting
- g. Wandsworth Town and Common

#### ***Detailed characterisation – ‘character areas’***

This level of characterisation focusses on a finer grain of detail in order to draw out the distinctive qualities important to character. The character areas sit within each of the places. Defining character areas draws on a wide range of information including building types, conservation areas, urban grain, open spaces, social data and historic mapping.

It should be recognised that lines on maps are typically in reality zones of transition – they very rarely represent a sudden change in character.

The study presents each character area on an individual ‘profile’. The structure of the profiles are explained below in the order they appear. The first pages contain the character description, and the second two pages contain the character evaluation.

#### ***Key characteristics***

For each character area a broad summary of the key elements that contribute to character is provided, noting the relevant aspects of particular importance to each area. The character descriptions are summarised as a set of **key characteristics**, described as *the combinations of elements particularly important to the area's character; its physical, cultural, social influences, and how it is perceived and experienced.*

The descriptions incorporate relevant elements listed in Policy D1 of the new London Plan shown in [Fig.462](#).

**Policy D1 London's form, character and capacity for growth**

**Defining an area's character to understand its capacity for growth**

A Boroughs should undertake area assessments to define the characteristics, qualities and value of different places within the plan area to develop an understanding of different areas' capacity for growth. Area assessments should cover the elements listed below:

- 1) demographic make-up and socio-economic data (such as Indices of Multiple Deprivation, health and wellbeing indicators, population density, employment data, educational qualifications, crime statistics)
- 2) housing types and tenure
- 3) urban form and structure (for example townscape, block pattern, urban grain, extent of frontages, building heights and density)
- 4) existing and planned transport networks (particularly walking and cycling networks) and public transport connectivity
- 5) air quality and noise levels
- 6) open space networks, green infrastructure, and water bodies
- 7) historical evolution and heritage assets (including an assessment of their significance and contribution to local character)
- 8) topography and hydrology
- 9) land availability
- 10) existing and emerging Development Plan designations
- 11) land uses
- 12) views and landmarks

**Fig. 462:** Policy D1, London Plan 2021  
© GLA

The evaluation of character draws conclusions and recommendations for each character area, as follows.

**Valued features**

**Valued features** are described as *an overview of the qualities and characteristics likely to have relative value. A townscape may be valued by different stakeholders for a variety of reasons.*

This aims to draw out what is valued, and why, using information gathered through field survey and public consultation.

Definitions of value are guided by criteria in Guidelines for Landscape and Visual Impact Assessment (GLVIA) - see [Fig. 461](#).

**Box 5.1**

**Range of factors that can help in the identification of valued landscapes**

- **Landscape quality (condition):** A measure of the physical state of the landscape. It may include the extent to which typical character is represented in individual areas, the intactness of the landscape and the condition of individual elements.
- **Scenic quality:** The term used to describe landscapes that appeal primarily to the senses (primarily but not wholly the visual senses).
- **Rarity:** The presence of rare elements or features in the landscape or the presence of a rare Landscape Character Type.
- **Representativeness:** Whether the landscape contains a particular character and/or features or elements which are considered particularly important examples.
- **Conservation interests:** The presence of features of wildlife, earth science or archaeological or historical and cultural interest can add to the value of the landscape as well as having value in their own right.
- **Recreation value:** Evidence that the landscape is valued for recreational activity where experience of the landscape is important.
- **Perceptual aspects:** A landscape may be valued for its perceptual qualities, notably wildness and/or tranquillity.
- **Associations:** Some landscapes are associated with particular people, such as artists or writers, or events in history that contribute to perceptions of the natural beauty of the area.

Based on Swanwick and Land Use Consultants (2002)

**Fig. 461:** Range of factors that can help in the identification of valued landscapes, from Guidelines for Landscape and Visual Impact Assessment  
© 2013 Landscape Institute and Institute of Environmental Management & Assessment

The GLVIA criteria have been adapted and interpreted for the purposes of this study, as shown in [Table 5](#).

Value	Indicators of value	
	Valued features	Negative qualities
<b>Townscape condition</b> The physical state of the townscape and condition of individual elements (buildings, green space, public realm, streets, gardens etc.)	Well-managed Intact Characteristic elements in good condition.	Under-managed Poor state of repair Signs of decay Degraded Land use decline/change (e.g. front gardens paved over)
<b>Scenic quality</b> Particular scenic and aesthetic qualities. Special pattern of townscape elements that create high aesthetic quality or sense of place.  Views, visual unity. Balance, Proportion, Rhythm, Emphasis, Unity, Variety.	Harmonious Unified Dramatic features Visual contrasts Special pattern of landscape elements High aesthetic quality Important features in views Distinctive skyline Vertical Horizontal.  Views Panoramic/framed views Memorable views Distinctive views.	Discordant Incongruous elements Fragmented Conflicting elements Out of proportion.  Views: Visual intrusions
<b>Distinctiveness</b> Important examples of townscape features and characteristics that contribute to a strong sense of place and recognisable local distinctiveness. Representativeness. Typically recognisable of Wandsworth	Rare features/ characteristics in the townscape Coherent/ Strong townscape character – strong pattern of features Distinct built form, materials or landscape Important or recognisable features or characteristics Community influences Landmarks	Indistinct character Unremarkable Development unsympathetic to character.

Value	Indicators of value	
<b>Natural/ historic interest</b> Presence of features of wildlife, earth science or archaeological or historical and cultural interest.	Features of natural interest Wildlife Important for biodiversity – may be indicated by ecological designations.  Historic character – e.g. indicated through listed buildings, conservation areas, Scheduled Monuments Features of historic interest	Few features of natural/historic interest.
<b>Townscape function</b> Particular or special role of the area in the local context.	Special function as a setting to valued townscape features Visual backdrop Open gap Recreation value - where experience of the landscape is important – e.g. indicated through presence of outdoor visitor attractions/ country parks. Active street frontages	Little townscape function role.
<b>Perceptual aspects</b> Experiential qualities such as sense of tranquillity; sensory qualities (sound, smell, texture); perceptions of safety, pollution	Sense of 'wildness' Sense of 'tranquillity' Lack of intrusion Quiet Calm Colourful Texture Intimate Vast Enclosed Open Diverse Interesting Inspiring Exhilarating Vibrant Formal	Busy Cluttered Poor legibility Monotonous Awkward juxtapositions Safety Garish Noisy Lack of green
<b>Cultural associations</b> Some townscapes are associated with particular people, such as artists or writers, or events in history.	Art Literature Battlefield Film Music Myth/ Legend/ Folklore People Events.	Few cultural associations

**Table 5 Indicators of valued features and negative qualities**

### Negative qualities

**Negative qualities**, described as *qualities that do not contribute to the character of the area. They may indicate opportunity for enhancement in future planning and management.*

### Building types

This section lists the most prominent building types in the character area. The building types are set out in the building typology in [Appendix E](#), based on elements such as height, massing, layout and materials. It is not intended to be an exhaustive list but aims to capture the main patterns and overall types. The buildings types listed in the character profiles are only residential types apart from where a non-residential use is a major contributor to the character of the area (e.g. town centres).

### Sensitivity

This section contains an overview of the likely sensitivity of the character area considering its relative value and susceptibility to they types of changes likely to occur in the area. The method for the sensitivity assessment is described in more detail in section [C.5](#).

### Strategy

The strategy takes into consideration the key characteristics, valued features and negative qualities, and outlines a high level strategy to achieve a future desired state for the character of the area.

The character area strategies are grouped into four broad actions, as described below:

**Conserve** the character: the area has a strong existing character and elements are generally in good condition. Protect the existing character, characteristic elements and features. Enhance existing elements and features where appropriate.

**Restore** the character: the area has a strong character in places or a perceptible underlying character but which has deteriorated over time. The strategy is to restore the existing character to its 'essence' e.g. by improving maintenance of remaining characteristic features or restoring historic characteristic features that have been lost over time.

**Improve** the character: the existing area contains features which detract from the overall character, resulting in a fragmented character or one in which the essence of the character is not readily perceptible. The strategy is to protect existing features which are the essence of character; while adding new features which supplement and support the existing essential character of the area. e.g. new development in keeping with the essence of the place but creating additional height or density as appropriate, or new public realm features

to enhance the value of a space or street, such as tree planting.

**Transform** the character: the existing character is not readily perceptible, fragmented or very deteriorated. The strategy is therefore to remove detracting features and add new elements that fundamentally change the character of a place. New features should be in character with a former historic essence of place or be completely new but planned cohesively.

### *Character area design guidance*

Design guidance for each character sets out broad principles which are intended to help achieve the strategy. The design guidelines are intended as a high level overview of priorities, to inform more detailed strategies and policies. Depending on the nature of the character area, design principles may include:

- indicative height ranges where appropriate;
- massing;
- scale;
- features to be retained;
- relationship with streetscape, urban realm and open space;
- skyline;
- visual relationships and views; and
- opportunities for enhancement/mitigation.

### *A note on boundaries*

In addition to considering all of the evidence noted in the sections above, one of the key principles for defining boundaries for the study was avoiding splitting conservation areas, in order to make the document easy to use in combination with the character area appraisals, and because the conservation areas have undergone a significant degree of detailed evaluation. Therefore, where possible, conservation areas are contained within a character area rather than extending across multiple character areas. Notable exceptions to this are where parts of conservation areas extend into a town centre, or where there is a distinct change in character that would make this principle inappropriate.

It should be noted that boundaries are rarely clear lines on the ground, and represent a gradual transition. Therefore all boundaries should be treated as approximate lines of transition between areas of different character.

## C.5 Method for assessing sensitivity

The method for assessing sensitivity combines judgements on the relative value of the character area and its susceptibility to change.

'Sensitivity' is defined by Natural England's *Approach to landscape sensitivity assessment, 2019*. Essentially, it is a measure of the resilience, or robustness, of a townscape to withstand change arising from development, without undue negative effects on the area's existing character and its values, including changes to valued attributes and views.

It is a way of understanding the ability of each of the character areas to accommodate change.

The method for assessing sensitivity is informed by the Natural England guidance and adapted for the purposes of this study. **As this is a borough-wide study, the assessment of sensitivity is necessarily high level and it should be noted that sensitivity will vary on a site to site basis. Judgements on sensitivity are provided as a guide.**

It should be noted that 'high' sensitivity does not necessarily mean that no change or development can occur, but that any new development should be sensitively designed so as to not detract from the valued or susceptible qualities and characteristics. Likewise, a 'low' sensitivity should not be interpreted as any development can occur, but simply that the features and characteristics *may* mean that the area can accommodate change more easily.

**Sub areas** are defined, where relevant, to illustrate broad differences in sensitivity across a character area. The purpose of these areas is to draw out differences in sensitivity as a result of value and/or susceptibility within the area.

### C.5.1 Defining development parameters

The first stage of assessing sensitivity is defining the parameters of the change: i.e. 'sensitivity to *what*'. One of this study's key objectives is understanding potential capacity in relation to 'tall' buildings, and in respect of primarily residential uses, but also occasionally employment and commercial uses.

Therefore, the development parameters are assumed to be:

- either residential or mixed uses;
- employment and commercial uses if in the existing or planned context of the same uses;



- good quality of design and finish; and
- of a height up to 50% above the prevailing building height in the area. The 'prevailing height' is defined in this study as a general average height of existing buildings in the character area, in relation to number of storeys, as noted in the key characteristics. The general average height is estimated using GIS information (primarily building height data) and information gathered through site survey.

### C.5.2 Assessing value

The second stage of assessing sensitivity involves identifying the relative value of the character area or sub areas. Value is identified as high/ medium/ low based on criteria in [Table 6](#):

Value	Criteria
High	A high proportion of the valued features and are represented in the area, with few negative qualities.
Medium	Some valued features are represented in the area with some negative qualities.
Low	Few valued features are represented in the area with a high proportion of negative qualities.

**Table 6** Assessment of value

### C.5.3 Assessing susceptibility

The third stage of assessing sensitivity involves judging the relative susceptibility of the character area or sub areas. Susceptibility is identified as high/medium/low, informed by the factors below, and criteria in [Table 7](#).

- **Pattern of built form:** whether the proposed scale of development would integrate with the general pattern, or detract from it. Areas with consistent, regular street pattern and a fine urban grain are more likely to be susceptible to change than areas with a mixed or irregular pattern and coarser urban grain.
- **Scale and style of built form:** areas with a small scale of built form and coherent architectural style are more likely to be susceptible to change than areas with larger scale buildings and a mixture of massing, styles and forms.
- **Land use:** consistent residential land uses may be more susceptible to changes or use than mixed land uses.
- **Distinctiveness and condition:** townscapes with a strong and positive townscape character and sense of place, in good condition and with features worthy of conservation, will be more susceptible to change because of the potential impact on their legibility and upon the characteristic features or elements that may be difficult to replace.
- **Historic features and cultural heritage:** areas with

a strong historic character or designations such as conservation areas, listed buildings, registered parks and gardens are likely to be more susceptible to change.

- **Perceptual qualities:** areas with high scenic quality, a sense of calm, quiet and tranquillity are likely to be more susceptible to change as new development may detract from these qualities, causing loss or disturbance.
- **Skylines and focal points:** areas with distinctive features on the skyline which would be adversely affected may be more susceptible to change than areas with unmemorable skylines and/or landmarks.
- **Key strategic (LVMF) and designated local views:** areas with valued views are likely to be more highly susceptible to new development which may impact on those views

Value	Criteria
High	The characteristics of the area are very susceptible to change and it is unlikely to be able to accommodate development of the type anticipated without significant character change or adverse effects.
Medium	The characteristics of the area are susceptible to change although it may have some potential to accommodate the relevant type of development in some defined situations without significant character change or adverse effects.
Low	The characteristics of the area are robust or degraded and it is likely to be able to accommodate the relevant type of development in many situations without significant character change or adverse effects.

**Table 7** Assessment of susceptibility

### C.5.4 Judgements on overall sensitivity

The final stage of assessing sensitivity is combining judgements on value and susceptibility to result in an overall judgement on sensitivity. Judgements are either in relation to a whole character area, or for sub areas within a larger character area. A description of overall judgements on sensitivity is provided in [Table 8](#).

Value	Criteria
Very high	Growth is not likely to be appropriate. The area is very sensitive to all types of development because of its outstanding quality and local distinctiveness (e.g. grade I listed buildings, registered parks and gardens). Tall/ taller buildings are very unlikely to be appropriate in these areas.  Likely to be associated with the 'conserve' strategy.

Value	Criteria
High	<p>Extensive change is not likely to be appropriate although there may be individual sites that could accommodate new development if in character with the area. These areas are likely to be high quality residential areas that could accommodate developments of similar proportions/architecture. There may be localised areas within these where tall/taller buildings could be accommodated.</p> <p>Likely to be associated with the 'conserve' or 'restore' strategy.</p>
Medium	<p>Potential for targeted growth: areas where character is mixed with some valued components / features but other areas with the potential for further enhancement. These areas are likely to have specific locations where growth may be possible either without detracting from existing qualities or enhancing local scenes. Areas could receive buildings different in style and architecture if they enhance local character. There are likely to be localised areas where tall/taller buildings could be accommodated.</p> <p>Likely to be associated with the 'restore' or 'improve' strategy.</p>
Low	<p>Opportunity for growth/change: areas where character is fragmented and in need of enhancement, and where there are detractors that weaken sense of place. These areas are likely to be locations appropriate for character to change, and where new development can provide a positive contribution to the broad character area (and the borough as a whole), leading to potential transformation of character. These will include the Local Plan opportunity areas where these have not been built-out. Development should be well-planned. These areas are likely to be able to accommodate tall/taller buildings.</p> <p>Likely to be associated with the 'transform' strategy.</p>

**Table 8 Sensitivity descriptions**

## C.6 Assessing probability of change

'Probability of change', also referred to as 'suitability', looks at the context of the area and the consequent appropriateness of that area, for future growth.

The probability of change assessment considers the likelihood of areas coming forward for development.

It should be noted that indications of 'higher' probability is not to suggest that all sites within this areas are acceptable for development; rather, that as a whole the likelihood of change is higher. Likewise, an indication of 'lower probability' is not to suggest that no change will occur in these areas; however, as a whole the likelihood of change is lower.

Higher probability is indicated by:

- areas already designated for development (through an existing allocation or opportunity area status);
- high Public Transport Accessibility Level (PTAL) score;

- areas in proximity to a town centre or station.

Lower probability is indicated by:

- areas designated in their existing use (such as Strategic Industrial Location);
- open spaces;
- areas with a low PTAL score.

The criteria for probability of change are set out in [Table 9](#).

Probability Status	Criteria
Low Probability	<ul style="list-style-type: none"> <li>• Strategic Industrial Location</li> <li>• Locally Strategic Industrial Area</li> <li>• Open space and blue network</li> <li>• PTAL 0-2 (unless other criteria applies)</li> </ul>
Medium Probability	<ul style="list-style-type: none"> <li>• PTAL 3-4</li> <li>• Within a conservation area (unless included in a low or very high probability due to other criteria)</li> <li>• <i>(Unless included in low probability, high probability or high probability plus due to other criteria)</i></li> </ul>
High Probability	<ul style="list-style-type: none"> <li>• PTAL 5-6</li> <li>• Inclusion on the Brownfield Land Register</li> <li>• Within a local centre</li> <li>• <i>(Unless included in low probability or high probability plus due to other criteria)</i></li> </ul>
Very high Probability	<ul style="list-style-type: none"> <li>• Known areas with emerging masterplans or major planning applications</li> <li>• Opportunity area</li> <li>• Existing site allocation</li> <li>• <i>(Unless included in low probability due to other criteria)</i></li> </ul>

**Table 9 Probability of change descriptions**

On a site-by-site basis, there will be a number of factors that influence probability of change, including: existing use; quality, fitness-for-purpose and vacancy; site size and configuration; and ownership.

## C.7 Assessing development capacity

Sensitivity and probability of change are considered together to understand the development capacity of each character area.

The matrix at [Table 10](#) illustrates the interrelationship between probability and sensitivity and how they are used together to understand the development capacity.

Character areas have been categorised by both (high, medium, and low) sensitivity to change, and (high, medium, and low) probability of change.

		Probability			
		Very high	High	Medium	Low
Sensitivity	High				
	Medium				
	Low				

**Table 10 Sensitivity and probability matrix**

Broad sites with potential opportunities for clustered tall buildings are then identified, considering the following characteristics:

- proximity to major transport hubs;
- proximity to existing tall building clusters; and
- areas where there is large regeneration potential.

Suitability for tall buildings across the borough is then mapped showing an appropriate height range in storeys and metres above ground level for each individual zone.

Suitability for mid-rise buildings has also been mapped (5-6 storey buildings) with the appropriate height defined and mapped for each individual zone.

### *Small sites*

Opportunities for small sites are also assessed on the basis of:

- open space and blue network, strategic industrial locations and locally strategic industrial areas limiting opportunities for any small site development;
- conservation areas and high sensitivity character areas restricting small sites developments to those that respond positively to the sensitive character of the townscape and heritage assets;
- areas further than 800m from a town centre boundary or rail, underground or overground station with a PTAL rating of 0-2 restricting the scale and/or density of small sites developments;
- the remainder of the area having potential for small sites development in keeping with the character of the townscape (note, these all sit within the emerging London Plan Small Sites Policy H2).

These are mapped on a small sites opportunity map.

## C.8 Tall buildings analysis method

The tall buildings analysis identifies and tests a sample of sites likely to be appropriate for taller buildings.

Nine sites were provided by the London Borough of Wandsworth. For each of the sites a high-level hypothetical development scenario was modelled, in consultation with the Council. It should be noted that the massings and layouts shown for the scenarios are general, and should not be considered as site specific masterplan proposals.

Other tall building zones were assessed with reference to consented masterplans and/or existing and consented tall buildings.

The scenarios are developed and tested as outlined below.

### **C.8.1 Draft scenarios based on good urban design principles**

High density scenarios are considered initially, with a 'target' density defined for each site, which is informed by the site's probability rating, PTAL rating and the SRQ (sustainable residential quality) matrix.

The principles considered for developing the massing scenarios are listed below.

- The context for each site is analysed at a high level, and assumptions are defined in agreement with the Council.
- Existing masterplans or site proposals are used as a basis where they already exist and are in the public domain.
- Dispersal of massing is based on its appropriateness within the wider context, including townscape, and the outcomes of the characterisation study, taking into account heights, plot sizes, proximity to adjoining buildings and uses, etc.
- Building heights are determined and adjusted to have a positive and sensitive relationship with adjoining buildings. This includes stepping down towards lower height elements, and in relation to the position of the development along a street scene.
- Buildings are distributed and spaced in line with good urban design/architectural practice to minimise overshadowing and overlooking properties.
- Land use is predominantly residential with an exception of non-residential uses prescribed to the ground floors or tower podiums generally, and in locations where deemed appropriate in terms of best practice urban design, such as providing active frontage, or delivering non-residential land use as

part of increased density.

### C.8.2 Review and test the scenarios

Each development scenario is reviewed in relation to its context, and a high-level townscape and visual assessment undertaken.

Firstly, the model for each scenario is incorporated into Vu.City software to understand how it relates to its existing and future context. Views are rendered from viewpoints in the local and wider context to illustrate how the scenarios sit, including in strategic vistas or local street scenes.

Secondly, using 3D GIS software, a zone of theoretical visibility (ZTV) is generated for each scenario, taking into account topographic changes and existing intervening built form, to establish its potential visibility. The ZTV indicates which character areas may be affected by tall development in these locations.

Thirdly, each scenario is then assessed for potential effects on existing townscape character and visual amenity. The assessment is based on the evaluative information set out in the characterisation part of the study, following guidance in Guidelines for Landscape and Visual Impact Assessment (GLVIA3), Landscape Institute and IEMA, 2013. This considers how the development may positively or negatively affect character or setting through addition of new features or removing/obscuring existing features.

The principal steps for the assessment are outlined below:

- Townscape assessments focus on the character area the scenario is located within and any adjacent character areas likely to be substantially affected on the basis of the ZTV. The assessment then focused on the potential impacts to valued features as set out within the characterisation study. In particular, this includes focus on sensitive heritage assets where the historic environment is a particularly important characteristic of an area.
- Where developments would be visible beyond the borough boundary a commentary against the character of neighbouring borough areas is provided, including potential impacts of any proposed riverside development on the character of boroughs on the northern bank of the Thames.
- A high-level visual assessment, if scenarios would be visible in any strategic views (LVMF or designated local views) or locally important views identified within the characterisation study. This focuses on how the development may alter the skyline

when viewed from specific locations, alongside understanding what it may obscure and form a backdrop to. The assessment is supported by images generated from Vu.City.

- An overall conclusion is then drawn as to the acceptability of the scenario as illustrated, or whether some changes to its massing should be noted.

### C.8.3 Update massing

Where the high density scenario is found to be potentially inappropriate in a townscape a further mid-density scenario will be developed and presented in this evidence base. This mid-density scenario is then re-tested as described above to understand its appropriateness, with relevant conclusions drawn.

Where high density scenarios are found inappropriate, a brief note of this testing is included in the scenario pages ([Appendix A](#)) with the medium density scenario reported in full.

### C.8.4 Assumptions for development

The development scenarios are set out as follows:

- site areas reported in hectares (ha);
- densities reported as dwellings per hectare (dph);
- non-residential floorspace reported as GEA in sqm;
- residential floorspace reported as GEA;
- typical residential unit size is assumed to be 100 sqm GEA and will be applied to the overall GEA to assume total unit numbers.

The scenarios are based on the following assumptions:

- The block dispersal does not take into account the requirements for private open space or car parking provision.
- Contributions to public open space are assumed to be made off-site or as per the proposed masterplan.
- The massing development does not include viability assessment.
- The massing does not include any sunlight, daylight analysis.
- The impact of the increased density on existing social infrastructure and transport network needs to be assessed if the developments take place.
- No allowance has been made for provision of additional social infrastructure on sites.
- No allowance has been made for provision of utilities or service infrastructure on sites.

# Appendix D

## Policy and design review

This appendix outlines the reviews undertaken of policy relevant to the study at the national, London and borough-wide scale. It also includes a review of policies in similar boroughs alongside a design review of ten recent tall building developments within Wandsworth.

### D.1 National policy

#### D.1.1 National Planning Policy Framework

Chapter 12 of the National Planning Policy Framework (NPPF) (2019) covers design, reflecting that high quality buildings and places are key to what planning should seek to achieve. It states that plans should:

*‘...set out a clear design vision and expectations, so that applicants have as much curtailed as possible about what is likely to be acceptable. Design policies should be developed with local communities so they reflect local aspirations, and are grounded in an understanding and evaluation of each area’s defining characteristics.’* (Paragraph 125)

Particularly relevant to this study, the NPPF states that planning policies should ensure that developments are sympathetic to local character and history, including the surrounding built environment and landscape setting – but also not preventing or discouraging appropriate innovation and change (including increasing densities). Development should establish or maintain a strong sense of place, and optimise the potential of sites.

The NPPF also includes a number of policies around making effective use of land in meeting the need for homes and other uses – including building at appropriate densities and avoiding low density development where there is an existing/anticipated shortfall in land. It is important that strategic policies contained within local plans set out a clear strategy on meeting housing and employment needs. The NPPF gives support to upward extensions above existing residential and commercial properties for new homes, where this is consistent with the prevailing height and form of neighbouring properties and overall street scene and is well-designed.

This Urban Design Assessment therefore has a role to play in balancing (potentially competing) demands between optimising the use of sites to deliver required growth, and ensuring that well-designed places that reflect existing character are delivered.

#### D.1.2 Planning Practice Guidance

Planning Practice Guidance (PPG) provides regularly updated guidance in support of the NPPF. There are a number of elements of the PPG which are relevant to this work:

- It encourages the use of the National Design Guide (see below) in both plan making and in decision-making on individual applications (Reference ID: 26-001-20191001).
- It states that planning policy can embed design outcomes through: a plan’s vision, objectives and strategic policies; non-strategic policies (e.g. in providing a clear indication of the types of development that will be allowed in the area); and supplementary planning documents (Reference ID: 26-002-20191001 / 26-003-20191001).
- The use of local design guides, masterplans and design codes should be considered (Reference ID: 26-004-20191001 onwards).
- A range of considerations should be taken into account in establishing appropriate densities, including characterisation studies and design strategies, including consideration of urban form, historic character, typologies, etc. (Reference ID: 66-004-20190722).

#### D.1.3 National Design Guide

Good design is set out in the National Design Guide under the following ten characteristics: context; identity; built form; movement; nature; public spaces; uses; homes and buildings; resources; and lifespan.

Particularly important to this study are the following points summarised in [Table 11](#):

Characteristic	Well-designed places are:
<b>Context</b> (enhances the surroundings)	<ul style="list-style-type: none"> <li>Based on a sound understanding of the features of the site and the surrounding context, using baseline studies as a starting point for design;</li> <li>integrated into their surroundings so they relate well to them;</li> <li>influenced by and influence their context positively; and</li> <li>responsive to local history, culture and heritage.</li> </ul>
<b>Identity</b> (attractive and distinctive)	<ul style="list-style-type: none"> <li>Have a positive and coherent identity that everyone can identify with, including residents and local communities, so contributing towards health and well-being, inclusion and cohesion;</li> <li>have a character that suits the context, its history, how we live today and how we are likely to live in the future; and</li> <li>are visually attractive, to delight their occupants and other users.</li> </ul>
<b>Built form</b> (a coherent pattern of development)	<ul style="list-style-type: none"> <li>compact forms of development that are walkable, contributing positively to well-being and placemaking;</li> <li>accessible local public transport, services and facilities, to ensure sustainable development;</li> <li>recognisable streets and other spaces with their edges defined by buildings, making it easy for anyone to find their way around, and promoting safety and accessibility; and</li> <li>memorable features or groupings of buildings, spaces, uses or activities that create a sense of place, promoting inclusion and cohesion.</li> </ul>

**Table 11 National Design Guide: relevant extracts**

## D.1.4 Historic England

The study has also been informed by Historic England Advice Note 4: Tall Buildings (<https://historicengland.org.uk/images-books/publications/tall-buildings-advice-note-4/>) which guides sustainable and successful tall building design and planning within historic environments. Note that this document is due to be updated in 2021.

## D.2 London-wide policy

### D.2.1 London Plan

The London Plan was adopted in March 2021 and is the new Spatial Development Strategy for Greater London. It sets out a framework for how London will develop over the next 20-25 years and the Mayor's vision for Good Growth.

Chapter 3 of the London Plan includes policies on design, character and capacity for growth. A summary of the policies and supporting text is provided in [Table 12](#).

Policy	Part	Summary
D1 London's form, character and capacity for growth	A	Boroughs should undertake area assessments to define the characteristics, qualities and value of different places within the plan area to develop an understanding of different areas' capacity for growth. This should include: <ul style="list-style-type: none"> <li>demographic make-up and socio-economic data</li> <li>housing types and tenure</li> <li>urban form and structure</li> <li>existing and planned transport networks</li> <li>air quality and noise levels</li> <li>open space networks, green infrastructure and water bodies</li> <li>historical evolution and heritage assets (including an assessment of their significance and contribution to local character)</li> <li>typology and hydrology</li> <li>land availability</li> <li>existing and emerging plan designations</li> <li>land uses</li> <li>views and landmarks</li> </ul>
	B	Boroughs should plan to meet growth requirements by using the findings of area assessments to identify suitable locations for growth (and the scale of that growth), and follow a design-led approach to establish optimised site capacities for allocations. Boroughs are encouraged to set out acceptable heights, scale, massing etc.
	3.1.3	It is important to understand how places are perceived, experienced and valued – a wide range of people should be engaged in the area assessment (depending on the scope and purpose of the work).
	3.1.7	Respecting character and accommodating change are not mutually exclusive; instead, an appropriate balance should be struck. Opportunities for change and transformation, through new building forms and typologies, should be informed by an understanding of character.
D2 Infrastructure requirements for sustainable densities	A	Density should consider and be linked to the provision of future planned levels of infrastructure, and be proportionate to connectivity and accessibility (by walking, cycling and public transport).
	B	Higher density developments should generally be promoted in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling, in accordance with Policy D2 Infrastructure requirements for sustainable densities. Where these locations have existing areas of high density buildings, expansion of the areas should be positively considered by Boroughs where appropriate.
	C	In other areas, incremental densification should be actively encouraged by Boroughs to achieve a change in densities in the most appropriate way. This should be interpreted in the context of Policy H2 Small sites.
	D (11)	Development should respond to the existing character of a place by identifying the special and valued features and characteristics that are unique to the locality.

Policy	Part	Summary
D9 Tall buildings	A	Development plans should define what is considered a tall building for specific localities – which will vary between and within different parts of London.
	B 1)	Boroughs should determine if there are locations where tall buildings may be an appropriate form of development, which should include engagement with neighbouring boroughs that may be affected.
	B 2)	Locations and appropriate tall building heights should be identified on maps in development plans.
	B 3)	Tall buildings should only be developed in locations that are identified in development plans.
	C	Development proposals should address the following impacts (note, this is not the full list in Policy D9): <ul style="list-style-type: none"> <li>• long-range, mid-range and immediate views of buildings;</li> <li>• whether part of a group or stand-alone, tall buildings should</li> <li>• reinforce the spatial hierarchy of the local and wider context and aid legibility and wayfinding;</li> <li>• significance of London’s heritage assets and settings;</li> <li>• protection and enhancement of the open quality of the River Thames;</li> <li>• wind, daylight, sunlight penetration and temperature conditions around the building(s) and neighbourhood, and any air quality and noise impacts; and</li> <li>• cumulative impacts from multiple proposals.</li> </ul>
3.9.2	Boroughs should determine and identify locations where tall buildings may be an appropriate form of development based on the areas identified for growth as part of Policy D1 and by undertaking a sieving exercise assessing potential visual and cumulative impacts to consider whether there are locations where tall buildings could have a role in contributing to the emerging character and vision for a place. In these locations the maximum height that could be acceptable should be determined and these locations and heights should be identified on maps in Development Plans.	
3.9.3	Boroughs should define what is a ‘tall building’ for specific localities. In large areas of extensive change, such as Opportunity Areas, the threshold for what constitutes a tall building should relate to the evolving (not just the existing) context. Policy D9 applies to tall buildings as defined by the borough; where there is no local definition, the policy applies to buildings over 6 storeys or 18 metres measured from ground to the floor level of the uppermost storey.	
H2 Small Sites	A	Boroughs should proactively support well-designed homes on small sites (below 0.25ha).
	B 1)	Boroughs should recognise in Development Plans that local character evolves over time and will need to change in appropriate locations to accommodate additional housing on small sites.

Table 12 London Plan 2021 policy extracts

## D.2.2 Supplementary Planning Guidance

### Housing SPG

The Housing SPG was published in 2016 under the previous Mayor of London, providing guidance on how

housing-related policies in the London Plan should be implemented. It should be noted that this SPG was prepared under the previous London Plan and some aspects have been superseded or are no longer relevant (e.g. they relate to the Sustainable Residential Quality (SRQ) matrix).

Section 1.3 states that, while the best use should be made of development opportunities, proper account must be taken of the range of factors which have to be addressed in order to ‘optimise’ rather than ‘maximise’ development – including ensuring good design and taking into account local context and character.

### Shaping Neighbourhoods: Character and Context SPG

The Shaping Neighbourhoods SPG was published in 2014 under the previous Mayor of London sets out an approach to understanding character and context so that it can be considered in the planning and design process. Again, it is worth noting that the SPG was prepared under a previous London Plan.

The SPG notes that character is made up of physical, cultural and perceptual and experiential elements. The SPG sets out a process for understanding character and context, set out below. It should be noted that there is no requirement for the study to follow this methodology.

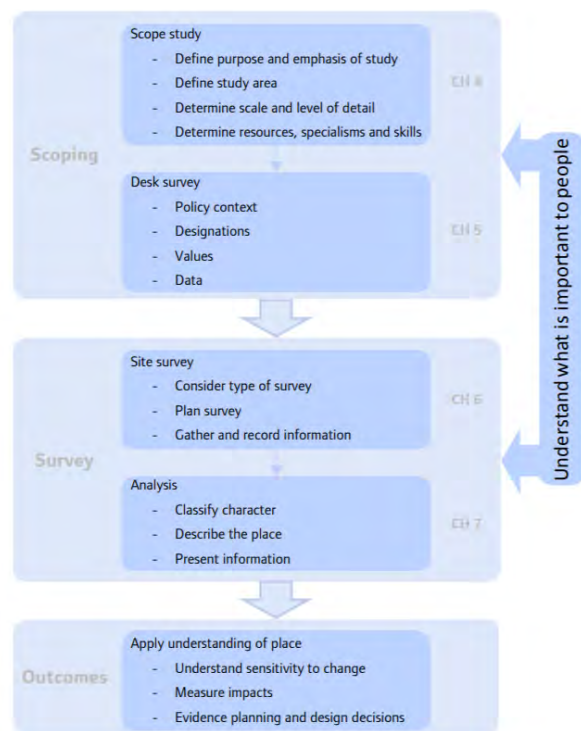


Fig. 463: Shaping Neighbourhoods: Character and Context SPG

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Once scoping and surveying has been undertaken, it suggests that character should be classified (including defining areas of distinct character and defining boundaries), describing each place with reference to particular characteristics and what is important and valued by the community and users, and presented using maps etc.

### *London View Management Framework SPG*

The London View Management Framework SPG was published in and sets out the approach to designating, protecting and managing 27 views of London and some of its major landmarks. These are the views which will be considered as part of the tall buildings assessment element of the review.

### *Industrial Intensification and Co-location Through Plan-led and Masterplan Approaches Practice Note*

London Plan Policy E7 supports the intensification of industrial uses within Strategic Industrial Locations (SIL) and Locally Significant Industrial Sites (LSIS) to make better use of land and to strengthen their role in supporting growth in London's economy and population. This note, published in November 2019, sets out good practice principles for plan-led or masterplan approaches to industrial intensification and co-location. It should be noted that Policy E7 is one of the policies falling under the Secretary of State's Direction, with the removal of the requirement for 'no net loss'.

Whilst the note does not primarily relate to urban design or character, it does reference the importance of demonstrating that any intensification is well integrated and lead to Good Growth.

### *Local Policy Context*

The London Borough of Wandsworth's current adopted local development plan consists of the following documents:

- Core Strategy (adopted March 2016)
- Development Management Policies Document (adopted March 2016)
- Site Specific Allocations Document (adopted March 2016)
- Policies Map (adopted March 2016 and subsequently superseded by a 2018 version)
- London Plan

A summary of the relevant policies included in the current local development plan is included in the comparison of approaches in [Table 13](#) and [Table 14](#).

Policy	Part	Summary
<b>PL 1 Attractive and distinctive neighbourhoods and regeneration initiatives</b>	a	The local distinctiveness of the various neighbourhoods which together make up the borough of Wandsworth will be promoted, building on their cherished existing character and attractiveness, including conserving and enhancing the borough's heritage assets and their settings.
<b>IS 3 Good quality design and townscape</b>	a	The Council will protect and reinforce the existing varied character and heritage of the borough
	c	With the exception of the major development sites within Nine Elms, the scale and density of development should make the most effective use of land and buildings.
	d	Tall buildings, (those which are substantially taller than the prevailing height of neighbouring buildings and/or which significantly change the skyline) may be appropriate in the borough's town centres, Nine Elms near Vauxhall or Putney Wharf, Wandsworth Riverside Quarter, Wandle Delta, Ransomes Dock, Battersea Power Station and Lombard Road/York Road Riverside focal points of activity. Some locations within these areas will be sensitive to, or inappropriate for, tall buildings. Applications for tall buildings will need to justify themselves in terms of the benefits they may bring for regeneration, townscape and public realm and be of high architectural quality, respect local context and the historic environment. Tall buildings are likely to be inappropriate in other areas.
	e	Views of the Westminster World Heritage Site will be protected in accordance with the London Plan 2015 and the London View Management Framework.
	f	The Council's position on the protection of Important Local Views is set out in the Local Views Supplementary Planning Document.

**Table 13 Core Strategy**

Policy	Part	Summary
<b>DMS 1 General development principles - Sustainable urban design and the quality of the environment</b>	a	Use a design-led approach to optimise the potential of sites, so that the layout and arrangement of buildings ensure a high level of physical integration with their surroundings.
	b	Ensure that the scale, massing and appearance of the development provides a high quality, sustainable design and layout, that contributes positively to local spatial character.
	c	Are sympathetic to local landscape characteristics.
<b>DMS 4 Tall buildings</b>	a	Locations where tall buildings may be appropriate: town centres, focal points of activity and Nine Elms near Vauxhall. Locations where tall buildings are likely to be inappropriate: Thames Policy Area (9+ storeys), Nine Elms (excluding Battersea Power Station and near Vauxhall) (11+ storeys), and the remainder of the borough (5+ storeys).



Policy	Part	Summary
	b	<p>Applications for tall buildings should (note, this is not the full list):</p> <ul style="list-style-type: none"> <li>• Describe how the proposal contributes to social inclusion, environmental health and economic vitality.</li> <li>• Demonstrate innovative and sustainable approaches to transport issues.</li> <li>• Address any climatic effects on its surroundings.</li> <li>• Show, through a detailed design analysis, that the proposal will have an acceptable visual impact on surrounding areas.</li> <li>• Assess the impact on the existing historic environment.</li> <li>• Demonstrate how the proposal successfully sits within the existing townscape and landform, and positively contributes to streetscape.</li> </ul>

**Table 14** Development management policies

### *Comparison of approaches used in recently adopted Local Plans*

A comparison of policy approaches used in other London boroughs is presented in [Table 15](#). These boroughs have all adopted policies in the last three years and share similarities (in terms of geography, development pressure etc.) with Wandsworth – although there are notable differences too. It should be noted that these Local Plans are not necessarily consistent with the emerging London Plan given the timing.

Whilst these are not necessarily ‘best practice’, there are a number of useful conclusions that can be drawn:

- The policy approach to design and character differs. Most of the case studies include an overarching policy which covers these elements; however, some also embed additional policy requirements in other policies – e.g. area-specific policies. The detail and criteria used also varies.
- Given design and character can ‘spill’ across multiple policies, this has the potential of causing confusion.
- Character areas, or the difference in character across the boroughs, are not always mapped – and where they are, they are not referred to in specific policies etc. (A notable exception is Richmond, through Village Planning Guidance SPDs rather than the local plan itself.
- None of the case studies included policies specifically on the design of small sites.
- The definition of a tall building is locally-defined, and in some cases is sub-categorised by area or type to allow for more locally-sensitive outcomes. Areas where tall buildings are likely to be appropriate (or inappropriate) are usually defined.

	Hammersmith and Fulham	Sutton	Richmond upon Thames	Tower Hamlets	Wandsworth
<b>Date of adoption</b>	February 2018	February 2018	July 2018 (re-adopted in March 2020 following High Court review)	January 2020	March 2016
<b>Approach to urban design and character</b>	Includes a policy on build environment, requiring development to respect and enhance townscape context and heritage – borough-wide rather than location-specific. Includes a policy on housing quality and density which states that development should respect the local setting and context – again, this is borough-wide.	Includes a comprehensive overarching criteria-based policy on character and design. Policies on specific areas (e.g. Sutton Town Centre) also have guidance on character, design and taller buildings.	Local Plan Strategic Vision includes a section on protecting local character, including the special and distinctive characteristics of the borough's villages and the historic environment. Strategic objectives and spatial strategy also reflect this. Includes a policy on local character and design quality, requiring proposals to have a through understanding of the site and how it relates to existing context including character. Individual Village Planning Guidance SPDs have been or are being prepared, identifying key features and characteristics of the village areas valued by local communities. Includes a detailed criteria-based policy on building heights. Includes a policy on open land on townscape importance. Some of the policies for each site allocation include guidance on design, townscape etc.	Includes a criteria based policy on design which requires consideration of character.	Vision references distinctive neighbourhoods, reflected in a high level policy in the Core Strategy. Higher densities promoted within centres, particularly Wandsworth, Clapham Junction and East Putney. Includes a policy on townscape which covers urban design, character and tall buildings. Development Management Policies Document includes a general development principles policy which references local landscape and character.
<b>Mapped character areas</b>	No	Yes – based on both density, era (pre-1915, inter-war, post-war and recent), and estates and cottage garden estates. However, these maps sit in the supporting text and do not have specific policies attached to them.	Yes (through Village Planning Guidance SPDs).	24 individual 'places' mapped (with fuzzy boundaries), but details of individual characters not set out.	No
<b>Definition of 'tall building'</b>	'Significantly higher than the general prevailing height of the surrounding townscape'	Includes three categories: <ul style="list-style-type: none"> <li>• Mid-rise ('tall in the context of relatively low-rise development. In absolute terms, they are in the region of 4 to 6 storeys (12 to 18m).')</li> <li>• Tall ('significantly taller than the mean height of surrounding development and will have a range of 7 to 10 storeys (21 to 30m).')</li> <li>• Very tall ('excessively taller than the surrounding built form and will be from 11 storeys upwards.')</li> </ul>	'Taller' buildings are defined as those being significantly taller than the neighbouring buildings, but less than 18 metres in height (below six storeys); a 'tall' building is defined as a building of 18 metres in height or higher.'	None	'Those which are substantially taller than the prevailing height of neighbouring buildings and/or which significantly change the skyline.' Number of storeys for particular locations set out in the Development Management Policies Local Plan (where tall buildings are likely to be inappropriate).

	Hammersmith and Fulham	Sutton	Richmond upon Thames	Tower Hamlets	Wandsworth
<b>Approach to tall buildings</b>	Includes a policy on tall buildings which states they will generally be resisted apart from within four defined areas. Criteria-based policy used to guide proposals in these areas.	Includes a policy on taller buildings both in terms of areas (via a policies map) and design criteria. Areas are broken down into the three categories of taller buildings set out above.	Supporting text states that tall buildings should be generally clustered close to Richmond and Twickenham stations, with 'taller' buildings elsewhere in these centres and other locations. Guidance is quite granular – e.g. names specific streets where tall buildings are unlikely to be appropriate. Required townscape appraisal / visual assessment and design justification for taller / bulkier developments.	Includes a criteria-based policy on tall buildings. Designates Tall Building Clusters with principles set for each one; proposals outside these areas must meet further criteria to be acceptable	Supporting text reflects that tall buildings can create attractive landmarks, act as a catalyst for regeneration, and be an effective use of land, but should be sited in appropriate locations and be acceptable in design terms. Areas of search for tall buildings defined in Core Strategy, though it is stated that some sites will still be sensitive. Tall Building Policy Areas defined in the Development Management Policies Document, and area-specific policies on tall buildings included in the Site Specific Allocations Document (making reference to the Development Management Policies Document).
<b>Approach to urban design on small sites</b>	None	None	None	None	None
<b>Other relevant details</b>	Much development focussed into four Regeneration Areas and other strategic sites (plus Old Oak Development Corporation) – strategic policies for each area/site include limited details on design including urban grain, townscape etc.				Includes specific policies on Nine Elms and North east Battersea, Wandsworth Town and Wandle Delta, Clapham Junction etc., which reflects the different approach in relation to higher densities etc. on a high level – further detail provided in the separate Area Spatial Strategy for Nine Elms and upcoming masterplan for Wandsworth Town.

**Table 15 Borough-wide planning policy comparison**

### D.3 Assessment of design quality

This section provides a review of the design quality of ten recent developments in the borough

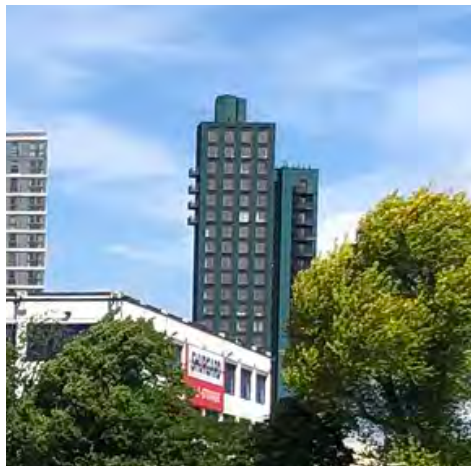
The assessment considers aspects such as appropriateness of height and massing, materials, façade treatments, overall quality of detailing and public realm provision. It is a subjective assessment undertaken by the same team completing the characterisation aspects of the study.

Please note that the assessment of design quality is subjective and based upon an independent review of each individual application by the Arup team, considering the information submitted in support of the application. This assessment offers a high level discussion of the principles of each individual scheme and do not seek to challenge or conflict with any planning decision in relation to these applications.

### D.3.1 11 Mapleton Crescent



**Fig. 464:** 11 Mapleton Crescent location plan  
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**Fig. 465:** 11 Mapleton Crescent scheme from King George's Park

<b>Application reference</b>	2015/5777 2015/0662 (amendments)
<b>Number of units</b>	89 residential (including 53 affordable)
<b>Site Area</b>	0.09 ha or 900m <sup>2</sup>
<b>Density (dph)</b>	988 dph
<b>Use</b>	Residential
<b>Height (max)</b>	89.5m
<b>Number of storeys</b>	Part 27-storey, part 24-storey
<b>Private open space</b>	Private and communal amenity space
<b>Public open space/ public realm</b>	Provision of a river walk to the south side of the River Wandle and improvements to bridge and pedestrian footway
<b>Has the scheme been built?</b>	Yes
<b>Character area</b>	Wandsworth Town and Riverside

The site is located at the edge of Wandsworth town centre but set back from a main road. Architectural quality is good in terms of the materials, façades and layout of the building; It is well orientated to consider views to the river and away from the substation to the south, both in plan and section. The plot is small and adjacent features are small scale (Mapleton Crescent, the River Wandle and adjacent buildings) and therefore the relative height of the building sits uncomfortably in its context. There may be issues of overshadowing to surrounding buildings and potential future development plots. The new pedestrian walkway provides access along the Wandle, which is a strong benefit. There are some active frontages along the street and river, but could be improved along Mapleton Crescent with more communal ground floor uses.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
YES				N/A			

**Table 16** 11 Mapleton Crescent design quality assessment overview

#### Conclusion

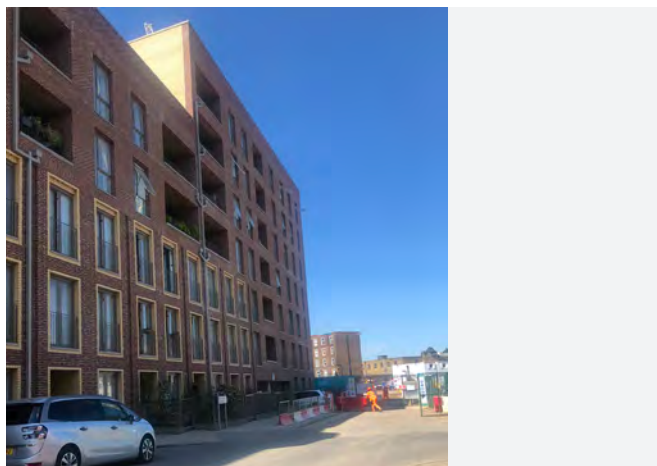
Although of good architectural design quality, the height of the building is inappropriate for its location on the edge of the town centre and away from a main road or key destination, which confuses legibility. The plot is small in relation to the height of the building and the relationship with the adjacent buildings and narrow street section results in a shaded, uncomfortable street environment which may feel unsafe. Its height in proportion to the River Wandle does not maximise the value of the environment and sense of place that could have been achieved around the watercourse,

although provision of a new walkway along it is positive. Although there are tall buildings in the wider area, the overall height of this building is excessive for this specific plot. The planning framework did not limit the increase in height beyond the original proposed scheme; indeed, the improved quality of the public realm and new housing were considered to be material considerations which justified the building exceeding limits set in planning policy of the time. The built scheme does generally reflect the planning application (it is assumed that the roof garden has been implemented, although unclear from Google Earth).

### D.3.2 Peabody Estate St Johns Hill



**Fig. 466:** Peabody Estate St Johns Hill location plan  
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**Fig. 467:** Peabody Estate St Johns Hill from Comlyn Road

<b>Application reference</b>	2012/1258
<b>Number of units</b>	527 resi
<b>Site Area</b>	2.27 ha or 22,700m <sup>2</sup>
<b>Density (dph)</b>	232 dph (767 habitable rooms per hectare)
<b>Use</b>	Mixed-use (residential and commercial)
<b>Height (max)</b>	12 storeys at 40m high
<b>Number of storeys</b>	Ranging from 4 to 12 storeys
<b>Private open space</b>	Private and communal amenity space
<b>Public open space/ public realm</b>	Public space and new pedestrian and cycle paths provided together with commercial offerings
<b>Has the scheme been built?</b>	Phase 1 (153 homes) complete, Phase 2 underway and due to be completed in 2021, Phase 3 due to be completed in 2023
<b>Character area</b>	Clapham Junction Town Centre

The site is in a site allocation and the spatial strategy area of Clapham Junction, on the edge of the town centre. Good quality materials which respond well to the nearby brick Victorian terraces. It addresses the main road and railway well, stepping up in height to this edge, but there is a more negative juxtaposition with the adjacent 3 storey, more intricately detailed period terraces (in conservation area). It is quite monolithic - there could be more variation (e.g. in the roofline) of the largest blocks to break up the massing and reduce perceived bulk. Good north-south permeability through the site - a new public route through to Wandsworth Common with tree planting, good legibility, and high quality public open space in the centre. Shops, retail and a community hub on the ground floor creates active frontages. Provides good natural surveillance overlooking Wandsworth Common.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
YES							

**Table 17** Peabody Estate St Johns Hill design quality assessment overview

#### Conclusion

This is a good example of a medium size development that increases density while responding to its surrounding character. The completed scheme (so far) is broadly consistent with the planning application. Overall the scheme is well-designed and integrates high quality open spaces. It improves connectivity to the wider area, with new pedestrian routes connecting the new public open space and Wandsworth Common. It responds well to its railway/road context, but is less successful in its response to the adjacent period terraces - the massing appears rather bulky and bland in relation

to some of the existing buildings and this could have been broken up more. The quality of materials is good.

As the proposals were of a particular height they triggered assessment against a criteria-based policy, which assessed the impact of the tall buildings. This approach was successful in ensuring heights were acceptable. The application was also subject to a Design Review Panel.

### D.3.3 Chadwick Hall, student accommodation



Fig. 468: Chadwick Hall location plan

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Fig. 469: Chadwick Hall scheme image

© Henley Halebrown Rorrison architects (Design & Access Statement, 2013)

<b>Application reference</b>	2013/1857
<b>Number of units</b>	205 resi
<b>Site Area</b>	1.028 ha 10,228m <sup>2</sup>
<b>Density (dph)</b>	199 dph
<b>Use</b>	Residential (student)
<b>Height (max)</b>	15.3m
<b>Number of storeys</b>	Between 3-5
<b>Private open space</b>	Landscaped communal amenity space
<b>Public open space/ public realm</b>	Pedestrian access to open space
<b>Has the scheme been built?</b>	Yes
<b>Character area</b>	Roehampton Residential Estates

The scheme is in a site allocation in the Roehampton spatial strategy area. Materials, height, massing and appearance respond well to its historic context including listed buildings and Alton Conservation Area. However, the new boundary wall to the east presents a poor interface with the public footpath on Roehampton Lane and there is a lack of active frontage. The frontage creates an uncomfortable area between the building and its boundary with a lack of active uses. There is a public footpath from Roehampton Lane to the open space to the west but it could be better signposted and its design could be improved with more active uses/ common areas/permeability in the facade alongside on the ground floor. The scheme also includes works include the restoration of the lower lawn and landscaping with the removal of 26 trees and planting of 19.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
YES							

Table 18 Chadwick Hall design quality assessment overview

#### Conclusion

The built scheme does generally reflect the planning application. It responds well to the site internally, however, its street interface could be improved by accommodating active uses and a more permeable frontage.

The distribution on the common areas within the layout could be improved to contribute to the safety and vibrancy of the new pedestrian access.

Much of the consideration at planning determination stage focussed on the impact on Downshire House and its wider landscaped settings, and required screening and some height reductions. It may have been beneficial to require improvements to the building more generally including its (lack of) active frontage. The policy around building heights was sufficiently flexible to allow reflection that the five storey north west block did not cause additional harm due to the topography of the site.

### D.3.4 One Putney



**Fig. 470:** One Putney location plan  
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**Fig. 471:** One Putney scheme photo

<b>Application reference</b>	2015/2601 and 2017/0560
<b>Number of units</b>	15 resi
<b>Site Area</b>	0.2 ha
<b>Density (dph)</b>	75 dph
<b>Use</b>	Mixed-use (retail and residential)
<b>Height (max)</b>	17.09m
<b>Number of storeys</b>	2-5 storeys
<b>Private open space</b>	Communal roof terrace
<b>Public open space/ public realm</b>	No
<b>Has the scheme been built?</b>	Yes
<b>Character area</b>	Putney Town Centre

The site is in Putney town centre, on the high street. It includes retail space at basement and ground floor, and new residential (15 flats) above, with balconies and a communal roof terrace above. Materials, height and massing distribution respond well to the context, stepping down towards the housing to the east. Fits well with the surrounding buildings and character of the high street, including Oxford Rd Conservation Area adjacent to the east, with the floors aligning well with existing building lines, despite being different in style and appearance. Its distinctive architecture helps to enhance legibility of the high street corner plot, responding to what was on the site previously, reflecting some of its history. The back of the development and its interface with Incorporation of residential windows along Montserrat Rd provides natural surveillance to the access road and improves its safety.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
YES							

**Table 19** One Putney design quality assessment overview

#### Conclusion

This is a good example of a town centre redevelopment providing distinctive response to its corner plot context and therefore good legibility, and also improving the relationship with surrounding residential areas to its rear. The building re-established the predominant building line broken by the existing building at second floor level, as well as re-establishing a building of more appropriate scale on this corner plot. It therefore makes a positive contribution to the local character of Putney High Street.

As the proposals were of a particular height they triggered assessment against a criteria-based policy, which assessed the impact of the tall buildings. This approach was successful in ensuring heights were acceptable - particularly in relation to a previously-refused proposal for the site which was considerably higher and was considered by an Inspector to have an 'uncomfortable relationship' with adjoining properties.

The completed scheme is broadly similar to the application although it is not clear from Google Earth whether green roofs have been incorporated.



### D.3.5 56-66 Gwynne Road



**Fig. 472:** 56-66 Gwynne Road location plan  
© Crown copyright and database right 2021



**Fig. 473:** 56-66 Gwynne Road scheme image  
© Farthings Investments Ltd, (Planning, Design and Access Statement, 2014)

<b>Application reference</b>	2014/5357
<b>Number of units</b>	33 resi
<b>Site Area</b>	0.04 ha or 400m2
<b>Density (dph)</b>	825 dph
<b>Use</b>	Mixed-use (commercial/retail/residential)
<b>Height (max)</b>	52.5m
<b>Number of storeys</b>	14 storeys plus basement
<b>Private open space</b>	Communal roof garden
<b>Public open space/ public realm</b>	Financial contribution to Harroway Gardens opposite, including footpaths, street furniture
<b>Has the scheme been built?</b>	Yes
<b>Character area</b>	Battersea Residential

The site lies within a Local Plan focal point. The scheme includes redevelopment of the site involving the demolition of the existing 2-storey commercial building and replacement with a new 14-storey building to provide a mixed-use site. The colourful and distinctive facade creates a memorable landmark in the street and the materials are appropriate to its varied surroundings. The internal layout maximises views to both north and south. Although taller than the surrounding buildings, it doesn't negatively impact the streetscape and introduces a new landmark for the adjacent locally listed park. The presence of existing trees in the surrounding streets integrates the building well into its context in local views. The commercial ground floor uses activate and enliven the public realm and improve feeling of safety around the adjacent Harroway Gardens.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
YES							

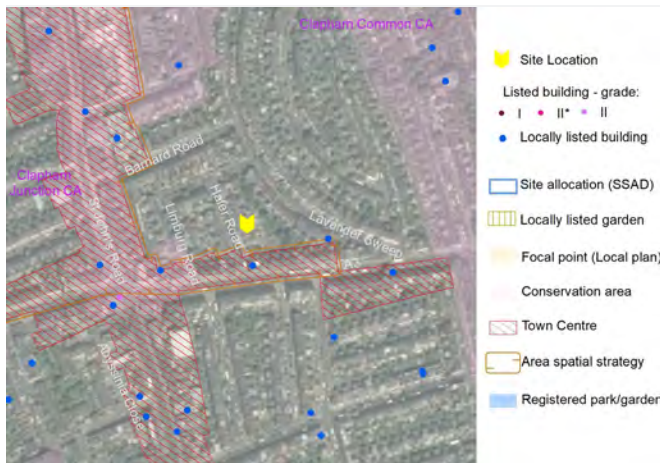
**Table 20** 56-66 Gwynne Road design quality assessment overview

#### Conclusion

This is a good example of an infill tower development in an area of change, creating a new landmark for an existing park. In determining the application, the impact on character (and in particular, the relationship between the building height and the gardens opposite) was considered. There was an acknowledgement that the height was, at least in part, justified due to the high quality of the design, and that detailed planning conditions would be required to ensure this design quality.

The built scheme does generally reflect the planning application.

### D.3.6 4-8 Hafer Road



**Fig. 474:** 4-8 Hafer Road location plan  
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**Fig. 475:** 4-8 Hafer Road scheme photo

<b>Application reference</b>	2013/4946
<b>Number of units</b>	16 residential
<b>Site Area</b>	0.077 ha (770 sqm)
<b>Density (dph)</b>	208 dph (779 habitable rooms per ha.)
<b>Use</b>	Residential
<b>Height (max)</b>	12.2m
<b>Number of storeys</b>	4 storeys (plus basement level)
<b>Private open space</b>	Private amenity space including gardens
<b>Public open space/ public realm</b>	No
<b>Has the scheme been built?</b>	Yes
<b>Character area</b>	Clapham Common and Residential

The street has a homogeneous character of Victorian terraces with bay windows. It is on the edge of the town centre, and visible from the main road (Battersea Rise). The new building reflects the building line of the adjacent buildings (re-creating it from what was on the site previously) which is a positive feature. Materials are similar in colour and appearance to adjacent buildings although still of distinctive contemporary character. Projected windows and balconies on the facade continue the rhythm and pattern of the street. Overall height exceeds the existing building line by about half a storey but the variation in heights along the skyline reduces the perceived massing. It incorporates garden amenity space for all the dwellings. Good natural surveillance from bay windows and windows to street. Frontage is good (no parking in front gardens) and creates homogeneous street frontage.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
NO				N/A	N/A		

**Table 21** 4-8 Hafer Road design quality assessment overview

#### Conclusion

This is a good example of an infill development on a typical Victorian terraced street and good response to context. The division into 4 bays maintains the key characteristics of a terraced house. It is clear that the Borough's Conservation and Design team were involved through pre-application engagement and that this led to an application in keeping with local character.

The completed scheme is very similar to what was proposed (the only difference being that the proposed

street trees don't appear to have been planted). It is however, beneficial that one tree has been retained. The new building doubles the number of units on the site and therefore is a good example of intensification appropriate to its context.

### D.3.7 Battersea Exchange



Fig. 476: Battersea Exchange location plan

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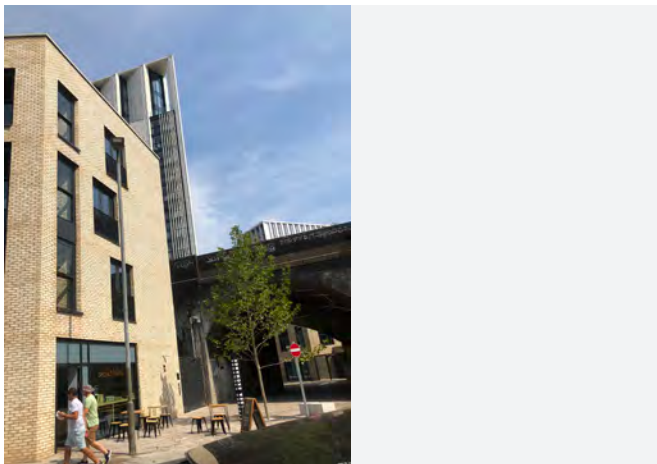


Fig. 477: Battersea Exchange scheme photo

<b>Application reference</b>	2014/4665
<b>Number of units</b>	290 resi, 3,676m <sup>2</sup> of flexible retail, office and community use floorspace
<b>Site Area</b>	1.83 ha
<b>Density (dph)</b>	257 units/ha (764 habitable rooms per hectare)
<b>Use</b>	Mixed-use
<b>Height (max)</b>	60.3m
<b>Number of storeys</b>	1-18 storeys
<b>Private open space</b>	Communal gardens and private gardens, private balconies
<b>Public open space/ public realm</b>	New public square extending through the railway arch, and pedestrian walkways including new pedestrian access from Patcham Terrace into Queenstown Road Train Station. Street trees, feature trees and play space included.
<b>Has the scheme been built?</b>	Yes
<b>Character area</b>	Nine Elms Mixed Use

This is an allocated site, next to a station, and within the Nine Elms regeneration area. The light coloured materials do not respond well to the existing character and context of the area (including historic character) and make the development stand out from its surroundings. The location of the towers next to the railway line responds well to context but the tower near the station is similar in appearance and therefore confuses legibility. Its height may overwhelm the adjacent public space and feel cramped in between viaducts and Battersea Park Rd. The refurbished railway arches and new pedestrian route between the stations provides a strong benefit, however the open spaces on the site are too small, particularly near the station, and in proportion to the scale of the buildings.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
YES							

Table 22 Battersea Exchange design quality assessment overview

### Conclusion

The overall shape and form of the completed scheme is similar to the planning application, although the finish and detailing appears poorer quality (as seen from Google street view). For example, the building at the corner of Meath street appears white rather than brick and jars with its context including the listed buildings.

The light colour and lack of texture in the façades means the development stands out from its context, which contains some high quality buildings that contribute positively to character. The public open

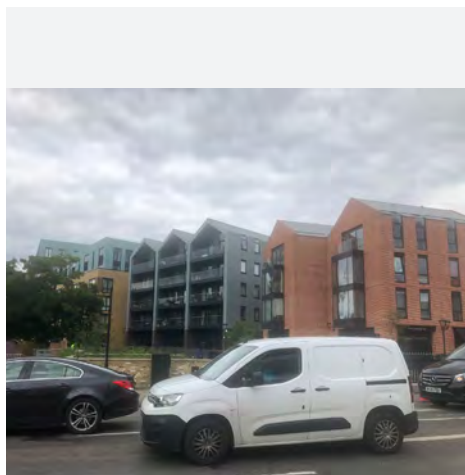
spaces - particularly the central public space needs to be larger in order to accommodate trees, seating and cafe uses. Where a scheme incorporates taller buildings these can aid legibility but in this case the towers in different parts of the site may confuse this. Details of materials were conditioned but it is not clear whether sufficient design policies were in place.

Building heights were carefully considered through the application's Environmental Statement, the Design Review Panel and the committee report, with the Panel noting the front tower played a 'marker role' opposite Battersea Park Station.

### D.3.8 Ram Quarter



**Fig. 478:** Ram Quarter location plan  
 Fig. 479: © Crown copyright and database right 2021



**Fig. 480:** Ram Quarter scheme photo

<b>Application reference</b>	2012/5286
<b>Number of units</b>	661 resi
<b>Site Area</b>	1.83 ha or 18,300m <sup>2</sup>
<b>Density (dph)</b>	361 dph
<b>Use</b>	Mixed-use (retail, residential, small scale brewery, museum, gym)
<b>Height (max)</b>	The 36 storey tower at max 115.7m
<b>Number of storeys</b>	2-12 storeys and 36 storeys
<b>Private open space</b>	Some private gardens
<b>Public open space/ public realm</b>	Public realm with retail offerings and a river walkway
<b>Has the scheme been built?</b>	Phase 1 complete (338 resi)
<b>Character area</b>	Wandsworth Town and Riverside

The scheme is in a site allocation/area spatial strategy. High quality architecture in terms of materials, arrangement and positive frontage to the river. However, the tower is generic/commercial in appearance and has a visual impact. The tower also adversely affects the setting of the conservation area and listed buildings (e.g. Church Row); however, the listed buildings on the brewery site have been sensitively restored and creatively re-used. The opening up of the River Wandle is a major benefit, however the interface with public realm on the edges of the site could be better. There is a section of poor frontage along Ram Street which lacks activity and could feel unsafe. Legibility within the site (e.g. alignment of routes with bridges and level changes) and connectivity with the town centre and the station appears poor, with routes not connecting as well as they could.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
YES							

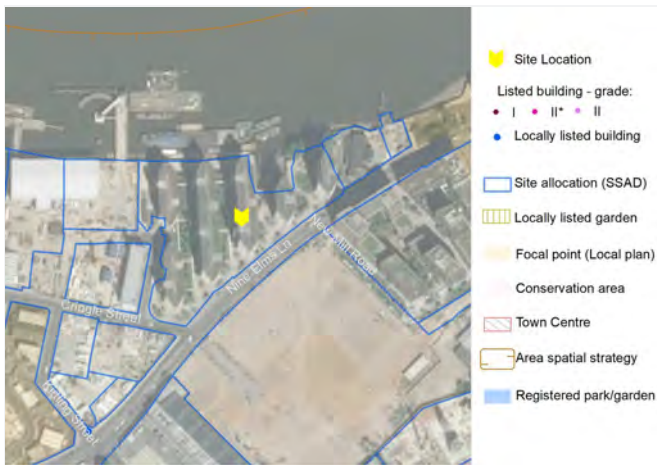
**Table 23** Ram Quarter design quality assessment overview

### Conclusion

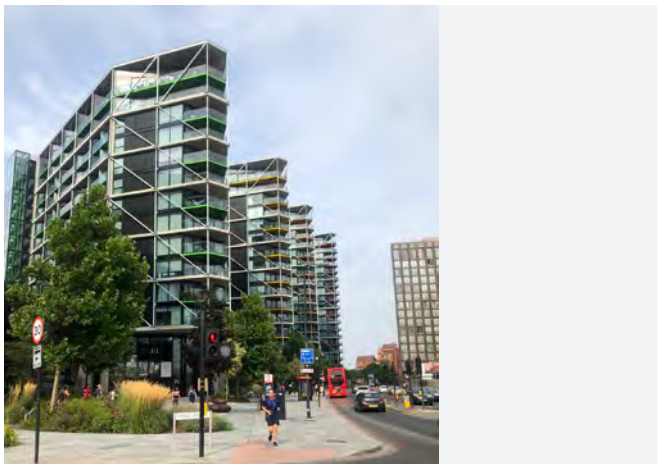
The scheme is a good example of using historical elements to add values and create character within a new mixed-use development. Some elements including the frontage to Wandsworth High Street, create a strong sense of place. The overall architectural layout provides interesting materials, rooflines and variations in massing. However, space along the river could be more generous, as could vistas/legibility across the site. Furthermore, the height and design of the upper parts of the tower respond poorly to context and would

be improved if the tower incorporated more variety/ rhythm/texture and was lower in height, thus creating a better graduation to adjacent buildings. This could also create a smoother transition to the listed building and therefore results in a better respond to the conservation area.

### D.3.9 Riverlight, Nine Elms



**Fig. 481:** Riverlight, Nine Elms location plan  
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**Fig. 482:** Riverlight, Nine Elms scheme photo

<b>Application reference</b>	2011/3748
<b>Number of units</b>	806 resi
<b>Site Area</b>	1.95 ha
<b>Density (dph)</b>	413 dph (1,234 habitable rooms per hectare)
<b>Use</b>	Mixed-use
<b>Height (max)</b>	62m
<b>Number of storeys</b>	12-20 storeys (plus 2 basement levels)
<b>Private open space</b>	No
<b>Public open space/ public realm</b>	Public open space comprising a landscaped riverside walk
<b>Has the scheme been built?</b>	Yes
<b>Character area</b>	Nine Elms Mixed Use

The site is within the Nine Elms Opportunity Area. Architectural design is of high quality, creating a distinctive landmark in wider views and from Nine Elms Lane. The height of the buildings steps down towards Battersea Power Station, which responds well to the setting of the power station. However, the tallest towers have an uncomfortable frontage to the river and the proportion of open space around them is too small, particularly along the river. More could have been achieved with the public realm along the river front to create a comfortable environment. Nevertheless, active ground floor uses with cafés etc. create a safe environment. The north-south spaces between the buildings create good sight-lines through the scheme from Nine Elms Lane to the river. However, more could have been achieved with the landscape design to create usable spaces for sitting as well as walking through.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
YES				N/A			

**Table 24** Riverlight, Nine Elms design quality assessment overview

#### Conclusion

This scheme provides a good precedent for architectural design quality to be achieved in towers in areas of urban character that are identified for growth.

It aims to increase permeability to the river front by introducing four fingers of open spaces between the towers. However, the massing layout could be improved to create a more consistent frontage along the river front walk and consequently create a better interface to the river.

The treatment of the public realm to the main road is positive, as is legibility through the scheme from the main road to the river. However, the address to the river could have been improved by more generous public realm area and creation of a more comfortable pedestrian environment.

The development's location within an area of change helps to justify its height, although the Design Review Panel also notes that the buildings have an acceptable visual impact in relation to the historic environment and complement surrounding emerging land uses.

### D.3.10 Salesian College



Fig. 483: Salesian College location plan

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Fig. 484: Salesian College scheme image

© Linden Homes (Design and Access Statement, 2014)

<b>Application reference</b>	2014/0910
<b>Number of units</b>	49 resi
<b>Site Area</b>	0.195 ha
<b>Density (dph)</b>	251 dph
<b>Use</b>	Residential
<b>Height (max)</b>	Approx. 15.5m
<b>Number of storeys</b>	Up to 5 storeys
<b>Private open space</b>	Communal and private amenity space, including balconies, roof terraces and a courtyard
<b>Public open space/ public realm</b>	Public realm improvement works, including reducing car parking spaces to create a pedestrian friendly landscape with new trees and street furniture
<b>Has the scheme been built?</b>	Yes
<b>Character area</b>	Battersea Residential

High quality architectural scheme, using similar materials to the adjacent locally listed building, but of different colour, creating a clear difference but presenting a positive relationship with the existing building. Vertical windows reflect existing vertical windows. Good response to the character and heritage of the adjacent buildings, including appropriate proportions, good relationship between old and new, and positive boundary treatment. The public realm improvements to Hyde Lane are positive, including tree planting and shared surface. The south-facing shared courtyard is a positive feature. Windows and balconies create strong natural surveillance to the street.

Area of change	Architectural quality	Urban design quality	Landscape	Heritage assets	Legibility and movement	Street frontages	Safety and security
NO							

Table 25 Salesian College design quality assessment overview

#### Conclusion

This is a positive example of responding to heritage context and positively enhancing the public realm. The proportions and massing enhance views in the surrounding area and the scheme positively enhances local character. The scheme is therefore a good example of accommodating a relatively high density.

The committee report acknowledged that the proposal made a significant contribution to the character of the area - in terms of its retention of the facade, the massing of the building, and the choice of materials.

The built scheme accurately reflects the planning application.

# Appendix E

## Building typology

### E.1 Introduction

This building typology highlights the predominant existing building types in Wandsworth borough.

The building types have been categorised for the purposes of this Urban Design Study, to add detail to the character area descriptions, and as a reference baseline in considering future potential development types. The typology is characterised by elements such as height, massing, layout and materials, and is intended to provide an overview of the main types of

buildings that exist in Wandsworth. These are cross-referenced into each character area profile, which lists the main building types occurring in that area. It is not intended to be an exhaustive list but aims to capture the main patterns and overall types. The buildings types listed in the character profiles are only residential types apart from where a non-residential use is a major contributor to the character of the area.

An overview of the building typology is provided in [Fig. 485](#) and a brief summary for each is provided on the following pages.

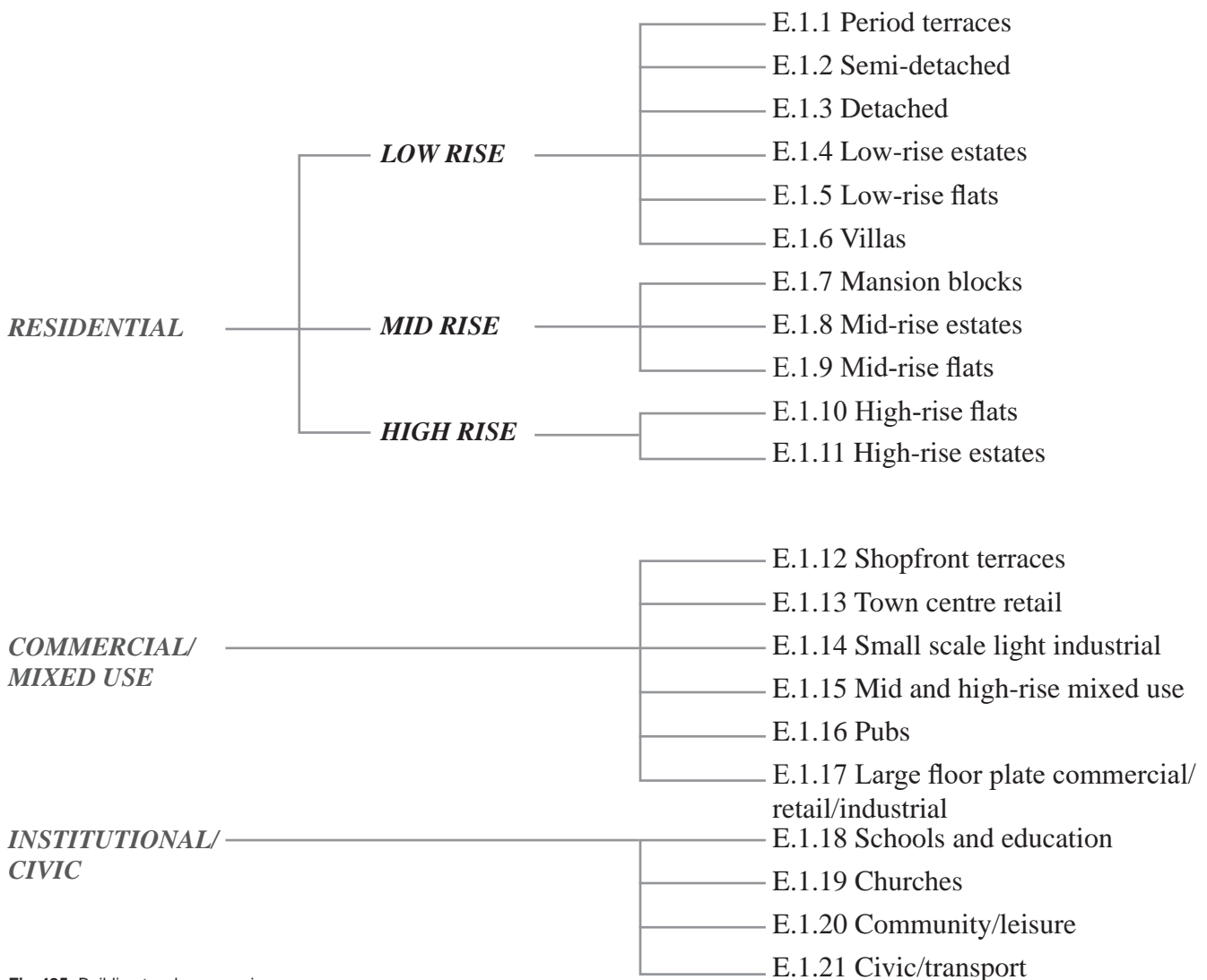


Fig. 485: Building typology overview

### E.1.1 Period terraces



Fig. 486: E.1.1 Period terraces on St Anne's Hill, Earlsfield

- 2-3 storeys;
- strong sense of enclosure to street;
- front and rear gardens typically although some front the street;
- repetition of plan, built form and architecture provides strong sense of rhythm;
- usually Victorian/Edwardian;
- front boundary walls;
- often intricate architectural detailing;
- instances of infill, particularly on war-damaged sites: where designed sensitively these blend seamlessly into the streetscape.

### E.1.2 Semi-detached



Fig. 487: E.1.2 Semi-detached house in Putney

- 2-3 storeys;
- front and rear gardens typically, some with parking in front gardens;
- front boundary walls

### E.1.3 Detached



Fig. 488: E.1.3 Detached house in Putney

- 2-3 storeys;
- front and rear gardens typically, some with parking in front gardens;
- front boundary walls, often with vegetation and trees
- often interspersed with semi-detached;
- suburban quality;
- usually in big plots and set back from the road.



### E.1.4 Low-rise estates



Fig. 489: Low-rise estates in Earlsfield

- 2-3 storeys;
- often mixed with terraced, semi-detached, maisonette, mid-rise estate and high-rise estates in post-war estate developments;
- wide streets with a spacious/suburban feel;
- small front gardens;
- often council-built in the post-war era but developer-led from the 1980s onwards;
- post-war estates often built rapidly to replace bomb-damaged sites often with poor quality, cheap materials for quick construction;
- set back from roads, layouts often have poor legibility and movement via warrens of access roads.

### E.1.5 Low-rise flats



Fig. 490: Low-rise flats house in Tooting

- typically 3-4 storeys;
- includes council-built and private developer-built;
- single front entrance which fronts on to the street;
- inter-war and post-war council blocks have good proportions and address to the street.

### E.1.6 Villas



Fig. 491: Villa in Balham

- 3-4 storeys;
- single front door with grand entrance;
- front garden behind boundary walls/gates/railings;
- often Georgian or Victorian.

### E.1.7 Mansion blocks



Fig. 492: Mansion blocks fronting Battersea Park

- typically around 5 storeys;
- imposing brick blocks of flats;
- arranged around private courtyards with landscaped perimeter spaces;
- high quality materials;
- plain form but with some detailing around windows, doors and balconies;
- usually single height across the development but occasionally include gradual steps;
- Massing broken up by recesses, bays and architectural detailing.

### E.1.8 Mid-rise estates



Fig. 493: Mid-rise estate block in Alton East, Roehampton

- flats of 4-8 storeys;
- either post-war or modern blocks;
- layout similar to low-rise estates, with curtailed movement and access and lack of legibility;
- set in landscaped surroundings, usually amenity grassland. Often communal public spaces do not fulfil their optimal functions;
- car parking;
- confusion of public and private spaces.

### E.1.9 Mid-rise flats



Fig. 494: Mid-rise flats in Wandsworth

- flats of 4-8 storeys;
- frontage to the street;
- usually modern;
- variety of architectural styles and materials.

### E.1.10 High-rise flats



Fig. 495: High-rise flats in Nine Elms

- over 8 storeys;
- similar to mid-rise flats but taller;
- often front the River Thames or large scale public spaces including parks;
- Include modern tower blocks.

### E.1.11 High-rise estates



Fig. 496: High-rise estates in Battersea

- flats over 8 storeys;
- either inter-war or post-war;
- usually council-built;
- Surrounded by open space;
- often tower blocks.

### E.1.12 Shopfront terraces



Fig. 497: Shopfront terraces in Balham

- 2-4 storeys;
- usually period terraces;
- retail ground floor uses, often with office or residential flats above.

### E.1.13 Town centre retail



Fig. 498: Town centre retail in a historic landmark building, Clapham Junction

- retail in town centre locations;
- buildings may be historic or modern;
- department stores or individual large shops.

### E.1.14 Small scale light industrial



Fig. 499: Mid-rise estate block in Alton East, Roehampton

- Low-rise;
- Uses such as garages, car repair shops, workshops;
- Often with hard standing in front given over to parking.

### E.1.15 Mid and high-rise mixed use



Fig. 500: High-rise mixed use development in Nine Elms

- over 4 storeys;
- often cafés/restaurants/shops at ground floor level with residential flats above;
- includes hotels;
- often includes areas of public realm.

### E.1.16 Pubs



Fig. 501: The Bedford pub, Balham

- often historic buildings;
- memorable and familiar landmarks;
- often occupy key locations on junctions.

### E.1.17 Large floor plate commercial/retail/industrial



Fig. 502: Large floor plate sub station in Wandle Valley

- large massing, with simple forms and low-cost materials;
- usually simple rooflines;
- uses include shopping centres, large supermarkets, industrial units and garages.

### E.1.18 Schools and education



Fig. 503: Historic board school, Earlsfield

- includes distinctive historic schools which are often landmarks/listed buildings;
- also includes modern, large and extensive schools;
- includes universities and colleges.

### E.1.19 Churches



Fig. 504: St Mary's Church, Battersea

- often distinctive landmarks;
- usually historic but also includes modern churches and other religious institutions.

### E.1.20 Community/leisure



Fig. 505: Boat house on Putney riverside

- includes a variety of uses, but usually have community functions or are destinations e.g. leisure centres, libraries, theatres, cinemas;
- includes more niche leisure functions such as boat houses.

### E.1.21 Civic/transport



Fig. 506: Wandsworth town hall

- includes a variety of uses but usually have civic functions e.g. hospitals, prisons, town halls;
- often landmarks or large buildings;
- also includes stations and associated developments.

# Appendix F

## Site survey pro-formas

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The pro-forma used during the site surveys is provided on the following pages.

<b>Character area</b>	
Date	

### Valued features and qualities

Value	Key words <i>*italics indicate primarily desk-based judgements</i>	Notes
<b>Townscape condition</b> <i>The physical state of the townscape and condition of individual elements (Buildings, green space, public realm, streets, gardens etc.)</i>	Well-managed Intact Characteristic elements in good condition.	
<b>Scenic quality</b> <i>Particular scenic and aesthetic qualities. Special pattern of townscape elements that create high aesthetic quality or sense of place.</i>  <i>Views, visual unity. Balance, Proportion, Rhythm, Emphasis, Unity, Variety.</i>	Harmonious Unified Dramatic features Visual contrasts Special pattern of landscape elements High aesthetic quality Important features in views Distinctive skyline Vertical Horizontal.  Views Panoramic/framed views Memorable views Distinctive views.	
<b>Distinctiveness</b> <i>Important examples of townscape features and characteristics that contribute to a strong sense of place and recognisable local distinctiveness. Representativeness. Typically recognisable of Wandsworth</i>	<i>Rare features/characteristics in the townscape</i> Coherent/ Strong townscape character – strong pattern of features Distinct built form, materials or landscape Important or recognisable features or characteristics Community influences Landmarks	
<b>Natural/ historic interest</b> <i>Presence of features of wildlife, earth science or archaeological or historical and cultural interest.</i>	Features of natural interest Wildlife Important for biodiversity – <i>may be indicated by ecological designations.</i>  Historic character – <i>e.g. indicated through listed buildings, conservation areas, Scheduled Monuments</i> Features of historic interest	
<b>Townscape function</b> <i>Particular or special role of the area in the local context.</i>	Special function as a setting to valued townscape features Visual backdrop Open gap Recreation value - where experience of the landscape is important – <i>e.g. indicated through presence of outdoor visitor attractions/country parks.</i> Active street frontages	
<b>Perceptual aspects</b> <i>Experiential qualities such as sense of tranquillity; sensory qualities (sound, smell, texture); perceptions of safety, pollution</i>	Sense of ‘wildness’ Sense of ‘tranquillity’ Lack of intrusion Quiet Calm Colourful Texture Intimate Vast Enclosed Open Diverse Interesting Inspiring Exhilarating Vibrant Formal	
<b>Cultural associations</b>	<i>Art</i>	



<p><i>Some townscapes are associated with particular people, such as artists or writers, or events in history.</i></p>	<p><i>Literature Battlefield Film Music Myth/ Legend/ Folklore People Events.</i></p>	
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## Negative qualities

<p><b>Townscape condition</b> <i>The physical state of the townscape and condition of individual elements</i></p> <p><i>(Buildings, green space, public realm, front gardens etc.)</i></p>	<p>Under-managed Poor state of repair Signs of decay Degraded Land use decline/change (incremental garden development, front gardens paved over)</p>	
<p><b>Scenic quality</b> <i>Particular scenic and aesthetic qualities. Special pattern of townscape elements that</i></p> <p><i>Views, visual unity. Balance, Proportion, Rhythm, Emphasis, Unity, Variety.</i></p>	<p>Discordant Incongruous elements Fragmented Conflicting elements Out of proportion.</p> <p>Views: Visual intrusions</p>	
<p><b>Distinctiveness</b> <i>Townscape features and characteristics that contribute to a strong sense of place and recognisable local distinctiveness. Representativeness. Typically recognisable of Wandsworth</i></p>	<p>Indistinct character Unremarkable Development unsympathetic to character.</p>	
<p><b>Perceptual aspects</b> <i>Experiential qualities such as sense of tranquillity; sensory qualities (sound, smell, texture); perceptions of safety, pollution</i></p>	<p>Busy Cluttered Poor legibility Monotonous Awkward juxtapositions Safety Garish Noisy Lack of green</p>	

## Design guidance

**Building types (draft for the purposes of the site visit)**

<b>Towers</b>	
<b>Modern flats</b>	
<b>Villa blocks (villa/detached/semi-detached/mansion)</b>	
<b>Modern terrace</b>	
<b>Period terrace</b>	
<b>Cottage style</b>	
<b>Council estate</b>	
<b>Modern estate</b>	
<b>Period terraces with shops</b>	
<b>Modern large retail boxes</b>	
<b>Signature landmarks</b>	

**General notes**

# Appendix G

## Public consultation

### G.1 Introduction

A four week online consultation was held in September 2020, inviting comments on the draft character areas and on what local people valued about their areas.

In total, 697 responses were received – of which 93% (646 respondents) lived in Wandsworth, 3% (21 respondents) worked or studied in Wandsworth, and 4% (30 respondents) visit Wandsworth. The questions asked are provided below and some analysis of the results are provided on the following pages.

1. **Do you feel the boundary of your selected character area is correct?**
2. **How would you rate your selected area on each of the following attributes?**
  - Attractiveness
  - Valuing heritage
  - Tranquil/calm places
  - Green and open space
  - Vibrant/lively places
  - Shops/restaurants/food
  - Culture and entertainment
  - Community spirit
3. **Are there any specific features or places in your selected area that you particularly enjoy?**
4. **What do you love about this feature or place?**
  - Attractive/scenic/special view
  - Historic landmark or feature
  - Familiar place
  - Secluded/tranquil feeling
  - Green and open space
  - Buildings/features
  - Greenery/trees/landscape
  - Memories/nostalgia
  - Sense of history
  - Vibrant/lively
  - Sense of escape
  - Other (please specify below)

5. **Which FIVE of the following are most important to you about the future of your area?**

- Preserving historic features
- Better public open spaces
- Improving architecture
- Better sense of community
- Better pavements and squares
- Better cleanliness and maintenance
- Better range of youth facilities
- Safer streets
- Better provision of shops/restaurants
- Better provision of culture/destinations
- Better provision of community facilities
- More wildlife / habitats
- Reduced traffic
- Improved layout for walking
- Improved cycle lanes
- Improved public transport
- Better housing provision
- Other (please state below)

The consultation findings were incorporated into the final version of the Urban Design Study, principally through:

- Amending the boundaries of some of the character areas where people felt they did not accurately reflect where they lived. This included incorporating the Clapham Junction and Clapham Common area into the Battersea 'place'. Note that this means the coded references to character areas are different in the following analysis compared to the final areas in the main report. [Fig. 507](#) in [Section G.3](#) summarises the changes made to the character areas.
- Incorporating additional valued features into many of the character areas within [Section 3](#).
- Noting additional negative qualities into some of the character areas within [Section 3](#).
- Providing some additional character area design guidance within [Section 3](#) based on observations from the local community.

## G.2 Analysis of responses

### 2. How would you rate your selected area on each of the following attributes?



For Balham Town Centre, respondents generally valued its vibrancy as well as its shops, restaurants and food more highly than some of its other features. Features such as heritage, attractiveness, culture and entertainment scored lower, although in general most features scored relatively consistently.



For Balham Residential, respondents scored all features relatively highly on average with the exception of tranquillity, culture and entertainment; which respondents considered to be weaker aspects of the area. Similarly to Balham Town Centre – shops, restaurants and food are considered particular assets to this area.

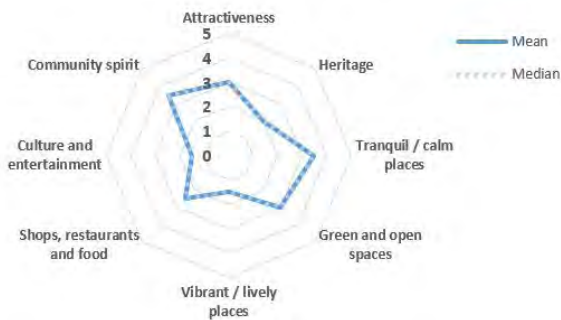


Features within Battersea Residential were generally scored consistently, although green and open spaces are particularly valued. Community spirit and attractiveness also scored relatively high. Although scoring is generally consistent – heritage, culture and entertainment scored marginally lower than other features.



In general, respondents scored all features relatively consistently, although particularly highly for attractiveness; green and open spaces; and shops, restaurants and food. Respondents considered that this area wasn't particularly strong in terms of heritage, tranquillity and community spirit, despite scoring generally consistently well.

### B3 Nine Elms Mixed Use



Nine Elms Mixed Use scored relatively poorly and inconsistently for most features. Despite an overall poor scoring, respondents considered that this area had relatively good green and open spaces as well as being tranquil and calm. Scores were particularly low for heritage and vibrancy; and this area was scored worst for culture and entertainment.

### B4 Battersea Park



Battersea Park was scored highly for most features with attractiveness, tranquillity as well as green and open spaces scoring highest. Vibrancy, culture and entertainment scored marginally lower for this area.

### C1 Clapham Junction Town Centre



Respondents scored vibrancy and shops, restaurants and food very highly for Clapham Junction Town Centre. Green and open spaces as well as culture and entertainment were scored moderately. However all of the remaining features were scored lower by respondents on average. Tranquillity was scored lowest out of all of the features.

### C2 Clapham Common and Residential



Respondents scored all features for Clapham Common and Residential consistently highly. Green and open spaces as well as attractiveness and shops, restaurants and food were considered as particular assets.

### D1 Earlsfield Residential



Scores for Earlsfield Residential were not generally consistent across the different features. Respondents considered green and open spaces as well as shops, restaurants and food as particular assets to the area whilst heritage, culture and entertainment were considered as weaknesses in this area.

### D2 Southfields Residential



Scores for Southfields Residential were generally high across all aspects, with the exception of culture and entertainment which was scored generally fairly low on average. Despite this, community spirit is considered as particularly strong here by respondents as well as green and open spaces.

### D3 Wandle Valley

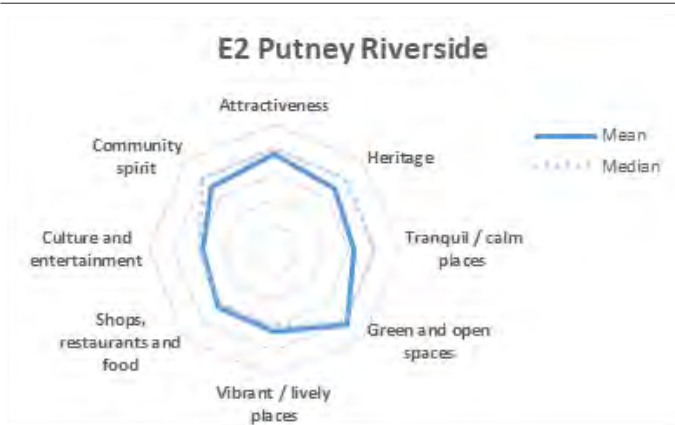


The Wandle Valley was considered as particularly strong for community spirit and green and open spaces; although many other aspects including tranquillity, attractive and shops, restaurants and food were considered relatively valuable as well. Respondents considered this area was particularly poor for culture and entertainment, however.

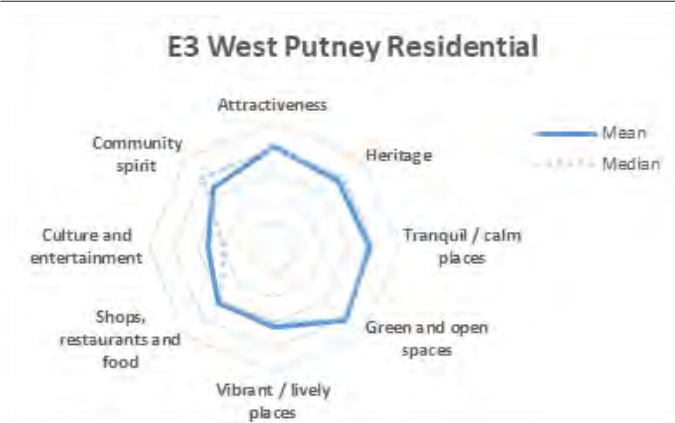
### E1 Putney Town Centre



Putney Town Centre was scored relatively low for all features, being scored most highly for shops, restaurants and food. Respondents considered the worst features in this area were heritage and tranquillity.



Putney Riverside was scored generally fairly highly by respondents for all aspects, although particularly highly for open and green spaces. The features that respondents considered less adequate in this area were shops, restaurants and food as well as culture and entertainment, although these features are not consistent with the character of this area.



West Putney Residential was scored consistently for most features and in general, very highly. Respondents considered its particular assets included green and open spaces as well as attractiveness and tranquillity. Culture and entertainment was scored lowest in this area.



East Putney Residential was scored less favourably by respondents than West Putney residential area, although the profile of scores remained largely consistent with West Putney. Shops, restaurants and food interestingly scored more highly for this residential area than for West Putney.



Scores for features at Wimbledon Parkside Estate were not consistent against all features, with culture and entertainment being considered particularly poor by respondents. Despite this, the area was considered to have good green and open spaces as well as being a tranquil and attractive area.

### E6 Putney Heath and Vale



Respondents considered Putney Heath and Vale as a particularly good area for attractiveness and green and open spaces. Aside from these assets and the tranquillity of the area, all other features were scored relatively poorly by residents, with community spirit being considered as the least positive aspect of this area.

### F1 Roehampton Parkland Estate



Roehampton Parkland Estate was scored generally poorly across all aspects. The worst scored features were shops, restaurants and food. Despite this, respondents considered the heritage, attractiveness and green and open spaces as particular assets to this area.

### F2 Roehampton Residential Estate



Features of the Roehampton Residential Estate were not scored consistently by respondents and showed stark positive views on some aspects mirrored by stark negative views on other aspects. In particular, green and open spaces were considered to be significantly positive features of this area, whilst tranquillity was also regarded highly. In contrast, shops, restaurants and food scored most poorly, although this reflects the character of the area. In comparison to other residential areas, community spirit also scored low.

### G1 Tooting Town Centre



Tooting Town Centre was considered highly by respondents for vibrancy, shops, restaurants and food as well as culture and entertainment; owing to the nature of the area. It was considered poor by respondents for green and open spaces as well as tranquillity and attractiveness.



### G2 Tooting Residential



The score profile for Tooting Residential largely reflected the score profile for Tooting Town Centre, although heritage and vibrancy were considered slightly poorer here.

### G3 Tooting Common



Respondents regarded Tooting Common on average highly for all features – placing particular merit on attractiveness and green and open spaces. Across all other features, respondents considered all relatively high with the exception of culture and entertainment, which was scored marginally lower than the other features.

### G4 Furzedown Residential



Furzedown Residential was regarded highly by respondents across all features, particularly community spirit, attractiveness and green and open spaces. This area scored marginally lower for culture and entertainment than all other features.

### H1 Wandsworth Town and Riverside

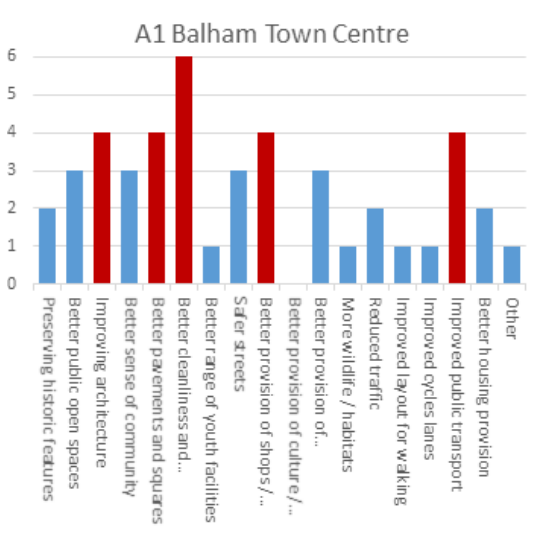


Wandsworth Town and Riverside was considered particularly strong by residents for shops, restaurants and food as well as vibrancy. Respondents scored all other aspects moderately, although green and open spaces was scored lower than all other features.

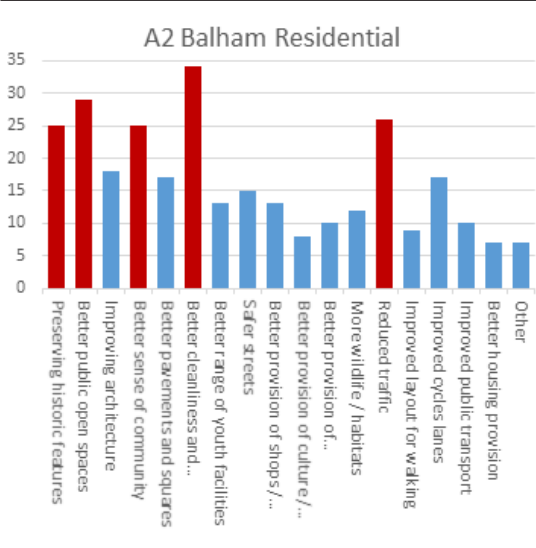


Wandsworth Common and Residential was regarded highly on average by respondents, especially for green and open spaces. Attractiveness and tranquillity were also considered strong here. Although all aspects were scored relatively highly, culture and entertainment was regarded by respondents as the weakest aspects of this area.

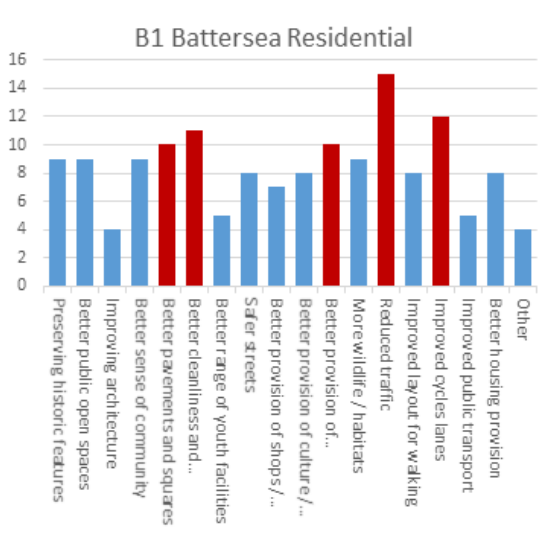
### 5. Which FIVE of the following are most important to you about the future of your area?



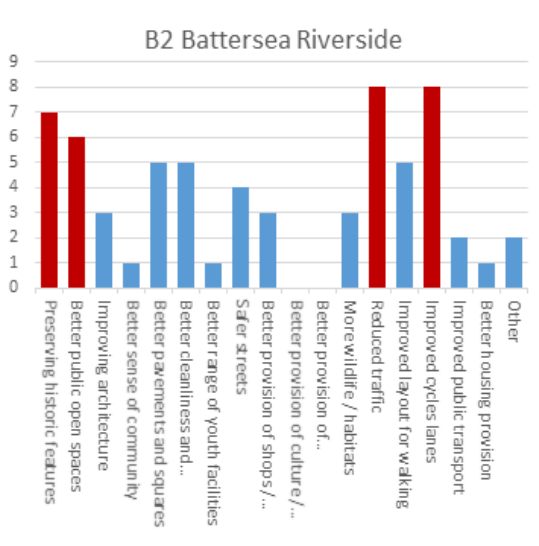
For Balham Town Centre, respondents unanimously considered that better cleanliness and maintenance was highly important to improve the area in the future. Following this, improving architecture; better pavements and squares; better provision of shops and restaurants; and improved public transport were also considered priorities. Interestingly, provision of culture and destinations was not considered by any respondents as in need of improvement.



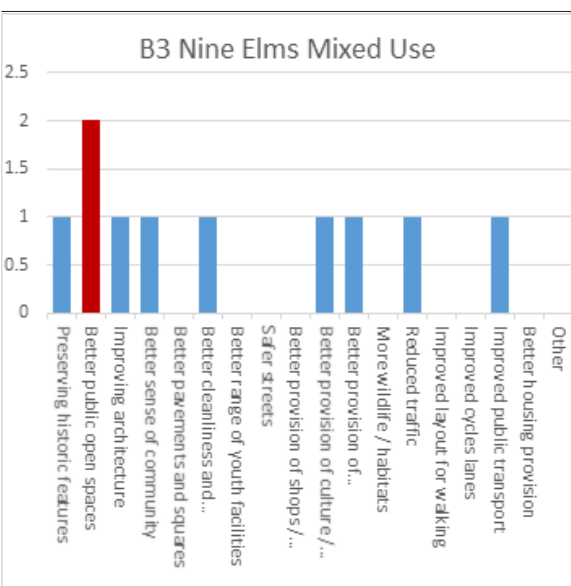
For Balham Residential there were a number of priorities considered with similarly high levels of importance. Again, better cleanliness and maintenance was considered as the most important priority, but this was closely followed by reduced traffic; better sense of community; better open spaces; and better historic preservation. For this area, there were no interventions that respondents didn't consider to be a priority of some sort.



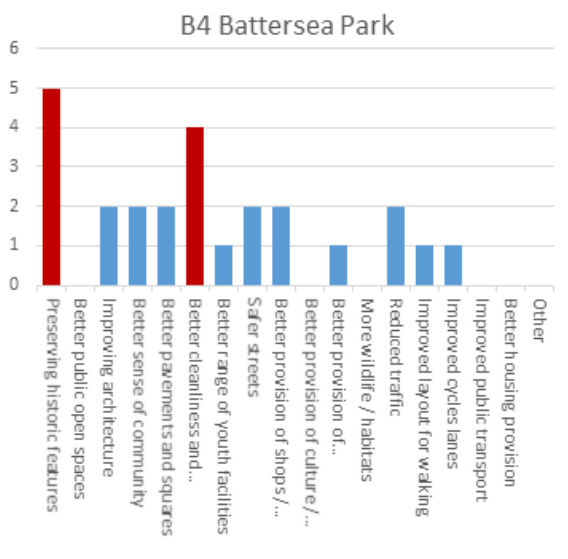
For Battersea Residential, reduced traffic was considered by respondents as the main priority, followed by and complimentary to, improved cycle lanes. Better pavements and squares; better cleanliness and maintenance; and better provision of community facilities. Interestingly, for this area, improvement of architecture was seen as the lowest priority for the future.



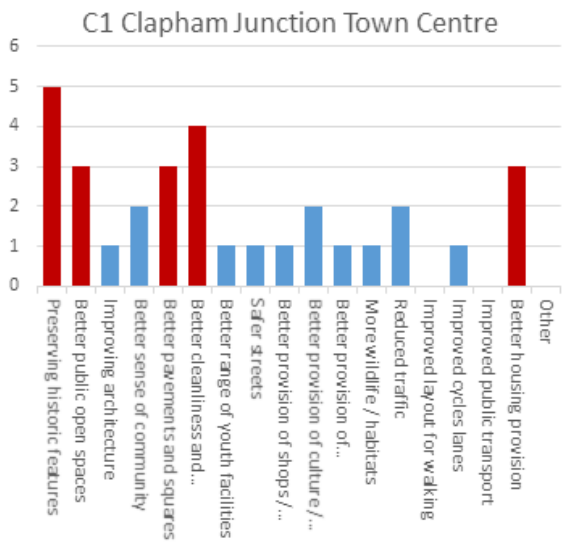
For Battersea Riverside, respondents considered two equally important key priorities for the area which again, similarly to Battersea Residential area were reduced traffic and improved cycle lanes. Other priorities include preserving historic features and better public open spaces. For this area, no respondents considered better provision of culture and destinations or better provision of community facilities as a particular priority.



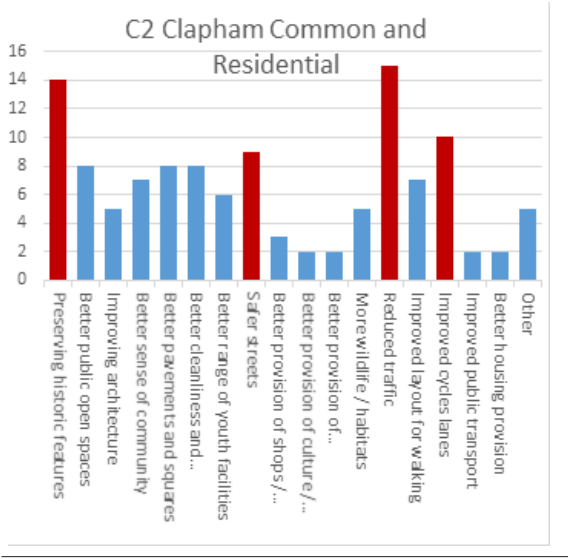
For Nine Elms Mixed Use, respondents considered a number of priorities as not relevant to this area and these included better pavements and squares; better range of youth facilities; safer streets; and improved cycle lanes, among others. The priorities that were identified as relevant such as reduced traffic, improved architecture, improved public transport etc. were all considered as equal priorities. Better public open spaces however were considered a key priority, above all others.



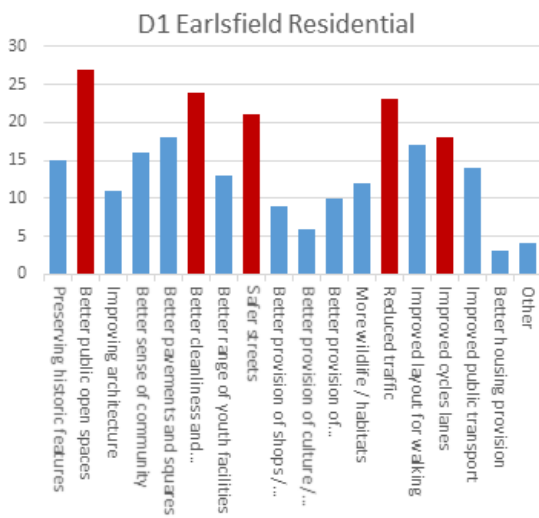
For Battersea Park, there were two main priorities identified and these were preserving historic features and better cleanliness and maintenance. All other priorities were scored relatively consistently and included better sense of community, reduced traffic, safer streets etc. Some improvements such as better provision of culture and destinations and more wildlife and habitats were not considered a priority for this area by any respondents.



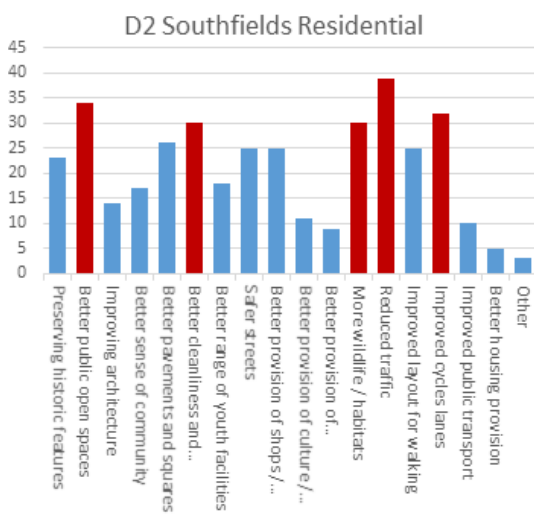
A number of priorities were identified by respondents for Clapham Junction Town Centre, with preserving historic features considered the most important priority. Other priorities included better public open spaces; better pavements and squares; better housing provision; and better cleanliness and maintenance were also considered important by respondents. For this area, better public transport and improved layout of walking were not considered priorities by any respondents, suggesting the existing state of these is considered adequate.



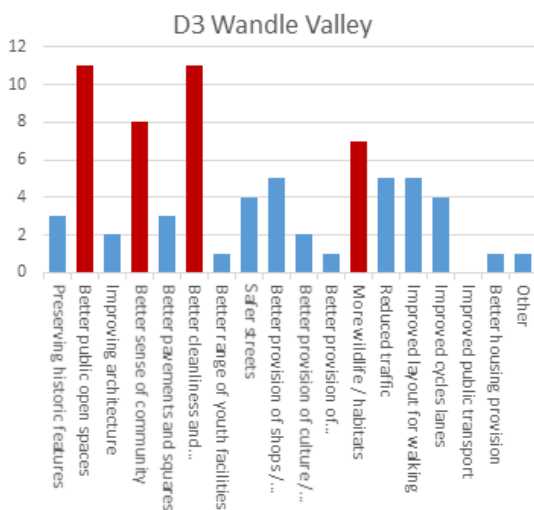
For Clapham Common and Residential there were two clear main priorities, and these are preserving historic features and reducing traffic. Safer streets and improved cycle lanes were also considered priorities for this area. Most other improvements were considered of equal priority for the area, although less so for better provision of culture and destinations, community facilities, housing and improved public transport.



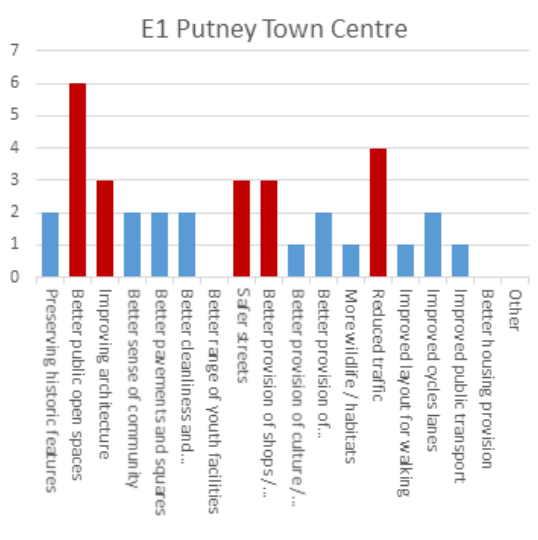
In Earlsfield Residential, respondents considered better public open spaces; better cleanliness and maintenance; safer streets; reduced traffic; and improved cycle lanes as the main priorities. Aside from these, a number of other priorities were considered as relatively important. Some improvements such as better housing provision and better provision of culture and destination were considered a priority to some degree, but to a much lesser level than all of the other improvements.



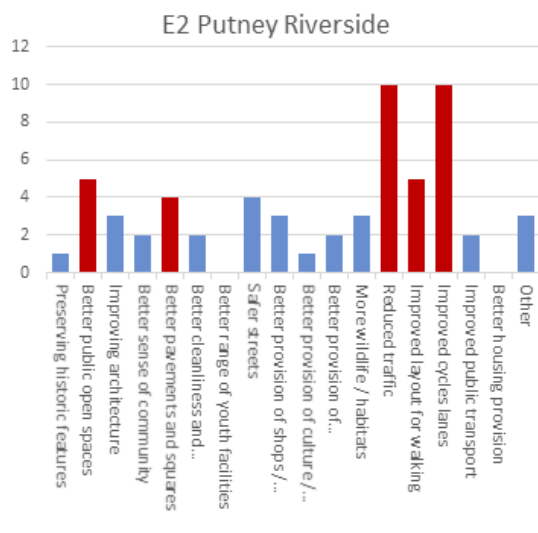
In Southfields Residential, reduced traffic, improved cycle lanes, better wildlife and open spaces as well as better cleanliness were considered the key priorities by respondents. Reduced traffic was considered the most important improvement. Better housing provision and better provision of community facilities were not regarded as priorities for the area by many respondents.



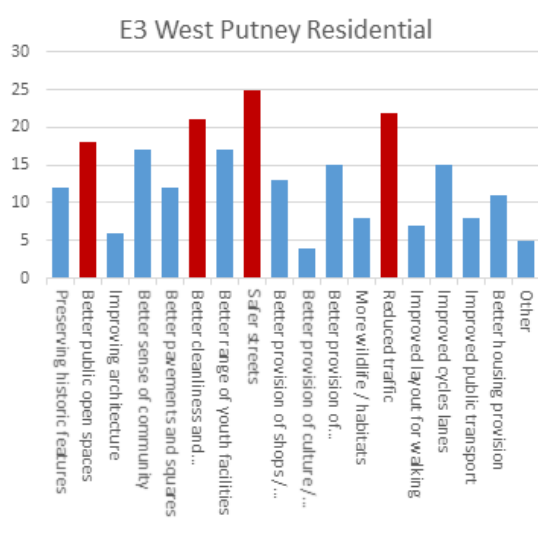
At Wandle Valley, respondents considered a number of key priorities but considered other potential improvements as of much lesser relevance. Better public open spaces; better sense of community; better cleanliness and maintenance; and more wildlife were considered the five top priorities. Interestingly here, improved public transport was not considered a priority by any respondents.



At Putney Town Centre, one key priority was considered important above all other priorities and this was better public open spaces. As well as this, reduced traffic; better provision of shops and restaurants; safer streets; and improved architecture were also considered as relevant by respondents. Better range of youth facilities was not considered a priority by any respondents here, suggesting that existing provision is sufficient.

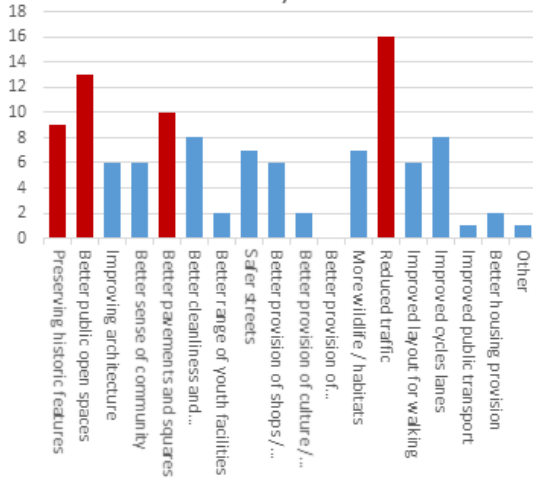


Putney Riverside was considered by respondents to require improvement for reduced traffic and improved cycle lanes as the key priorities. Improved layout for walking; better pavements and squares; and better public spaces were also considered as priorities here, suggesting that public realm improvements are needed. Respondents considered better public transport and better sense of community as low priorities here, suggesting that the current situation is sufficient.



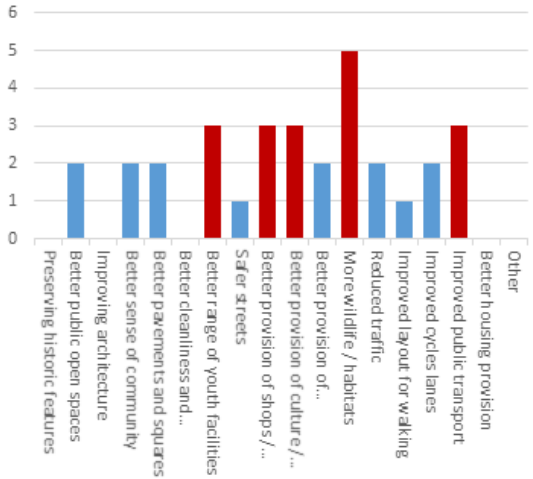
For West Putney Residential, all priorities were considered relatively high, with the exception of improved layout for walking; better provision of culture and destinations; and improving architecture. In particular, safer streets was considered the main priority by respondents, with reduced traffic; better cleanliness and maintenance; and better public open spaces, closely following.

E4 East Putney Residential



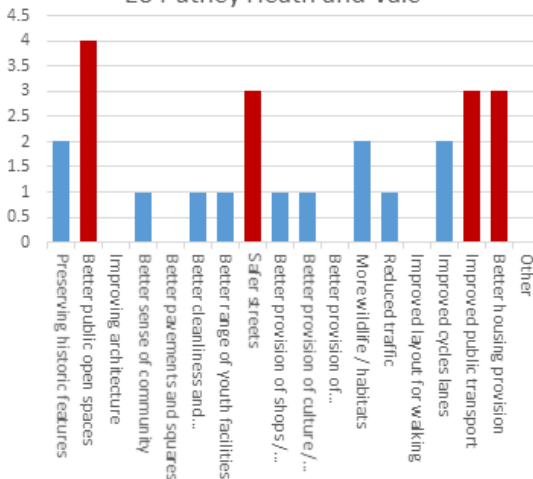
The priorities for East Putney Residential differed from those of West Putney Residential with the exception of better public open spaces and reduced traffic which were equally considered as key priorities. For this area, respondents also considered preserving historic features and better pavements and squares as important. Better provision of community facilities was not considered a priority here by any respondents and improved public transport was only considered important by a minority.

E5 Wimbledon Parkside Estate

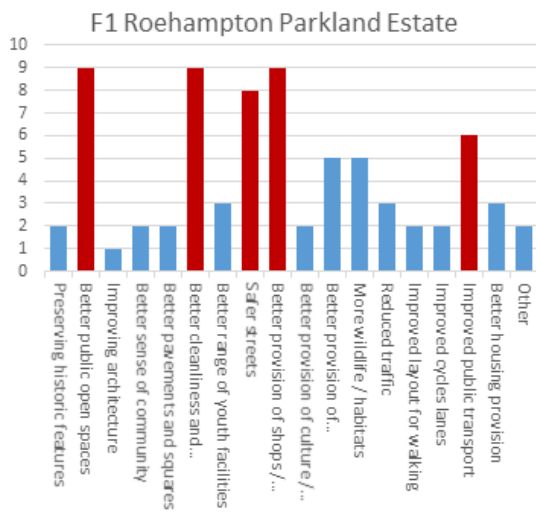


The key priority that respondents thought was important for Wimbledon Parkside Estate was more wildlife and habitats. Despite this better public open space was not regarded as high a priority. Better range of youth facilities; better provision of shops and restaurants; better provision of culture and destinations; and improved public transport were all considered as important for the future of the area. Better cleanliness and maintenance was not considered important by an respondents for this area, suggesting it is not a key issue.

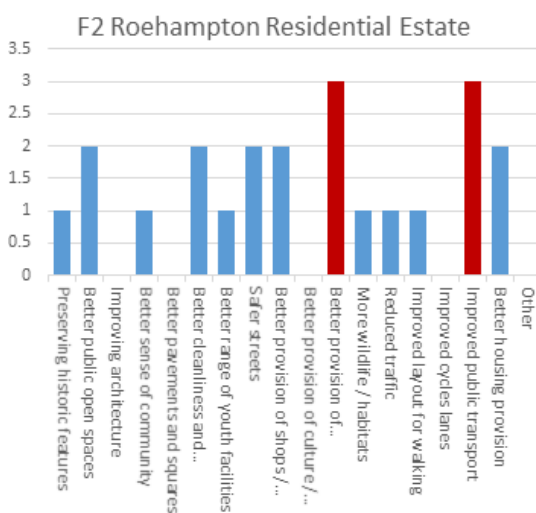
E6 Putney Heath and Vale



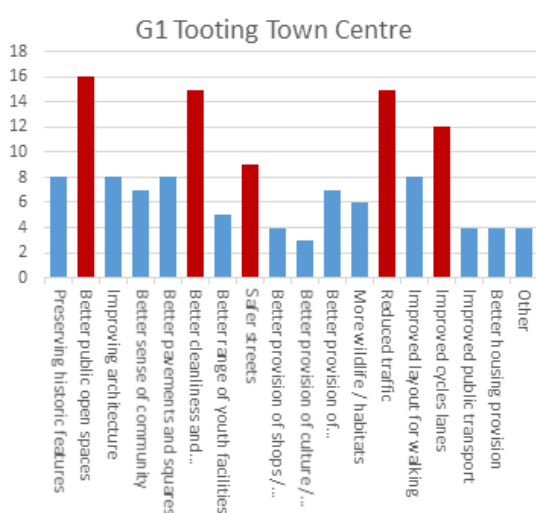
For Putney Heath and Vale, respondents considered the main priority to be better public open spaces. This was followed by safer streets, improved public transport and better housing provision which were regarded with equal importance by respondents. For this area, features such as improved architecture; better pavements and squares; and improved layout for walking were not considered priorities by any respondents.



Respondents considered a number of key priorities for Roehampton Parkland Estate which included better public open spaces; better cleanliness and maintenance; safer streets; and better provision of shops and restaurants. Improved public transport was also considered a priority but to a lesser degree. All other improvements were considered by respondents as of much lesser priority for the area, except better provision of community facilities and more wildlife which were regarded as moderately important.

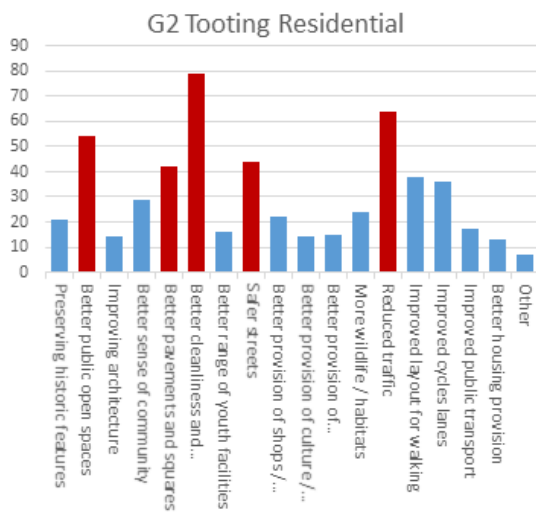


The priorities for Roehampton Residential Estate did not mirror those for Roehampton Parkland Estate, except from the fact that improved public transport was considered a key priority. The other key priority for this area was considered to be better provision of community facilities. A number of other priorities were considered of equal importance behind the two key priorities and these included some such as better public open spaces; better housing provision; better provision of shops and restaurants; and safer streets. Some improvements were not regarded as a priority by any respondents and this included improved cycle lanes, improved architecture and better pavements and squares.

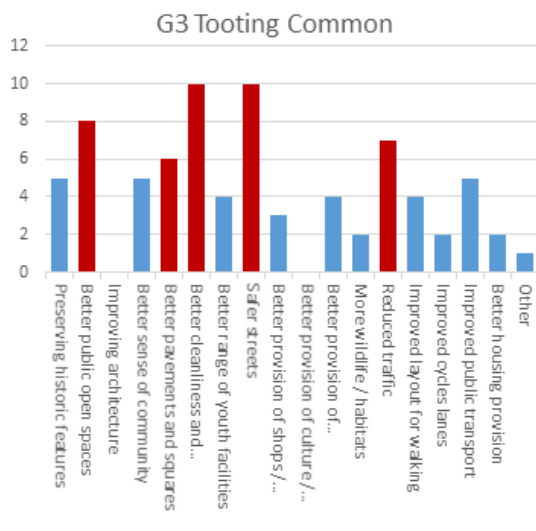


Respondents considered Tooting Town Centre to have a number of key priorities including better public open spaces; better cleanliness and maintenance; reduced traffic; and improved cycle lanes. Safer streets was also regarded as an important aspiration for this area. The remaining improvements were scored relatively equally by respondents, with least importance placed on better provision of entertainment and destinations; suggesting the area adequately provides for this.

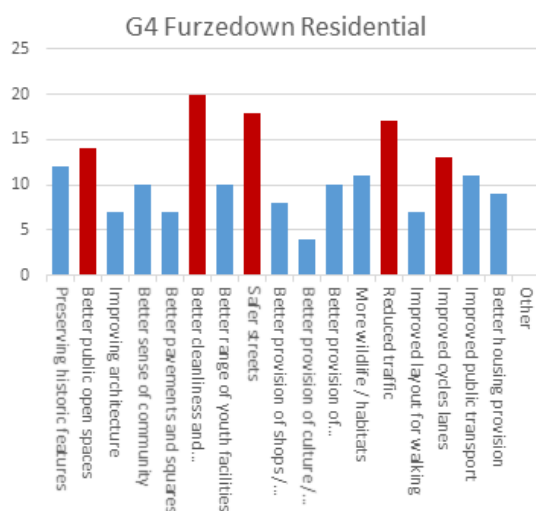




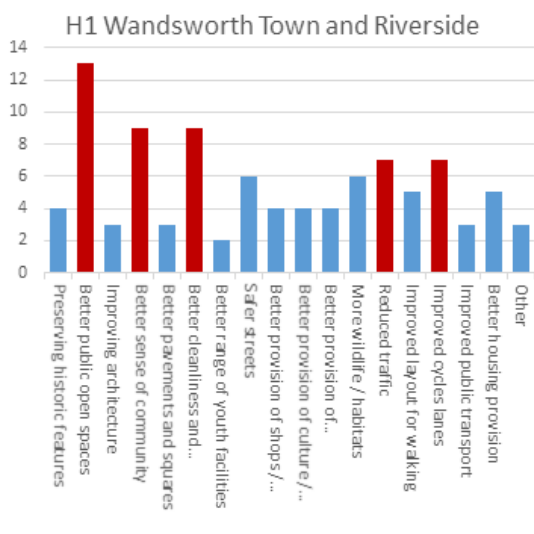
Tooting Residential was considered by respondents to require better cleanliness and maintenance; better public open spaces; and reduced traffic as the highest priority. Better pavements and squares as well as safer streets were also regarded as particularly important. Similarly to Tooting Town Centre, better provision of culture and destinations was considered a priority by the least number of residents.



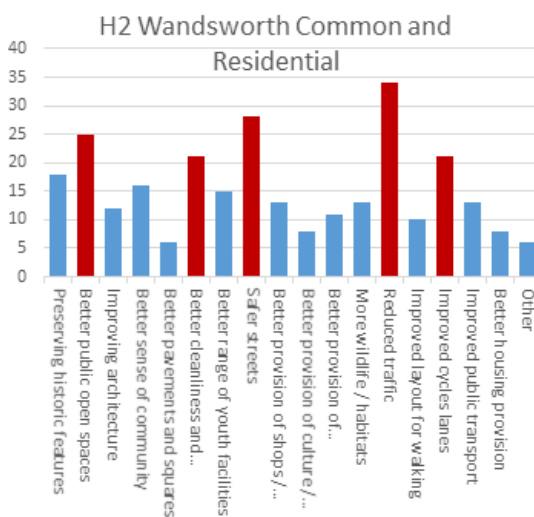
The most highly regarded priorities for Tooting Common were better cleanliness and maintenance as well as safer places. Other key priorities were better public open spaces; better pavements and squares; and reduced traffic. No respondents considered improving architecture or better provision of culture and destinations as a priority for this area, suggesting the existing state of these two features is sufficient.



For Furzedown Residential, the key priorities were considered to be better cleanliness and maintenance; safer streets, reduced traffic; improved cycle lanes; and better public open spaces. This suggests public realm is a key priority for improvement in this area. All improvements were considered by respondents to have relative importance, although better provision of culture and destinations was regarded as the least important priority.



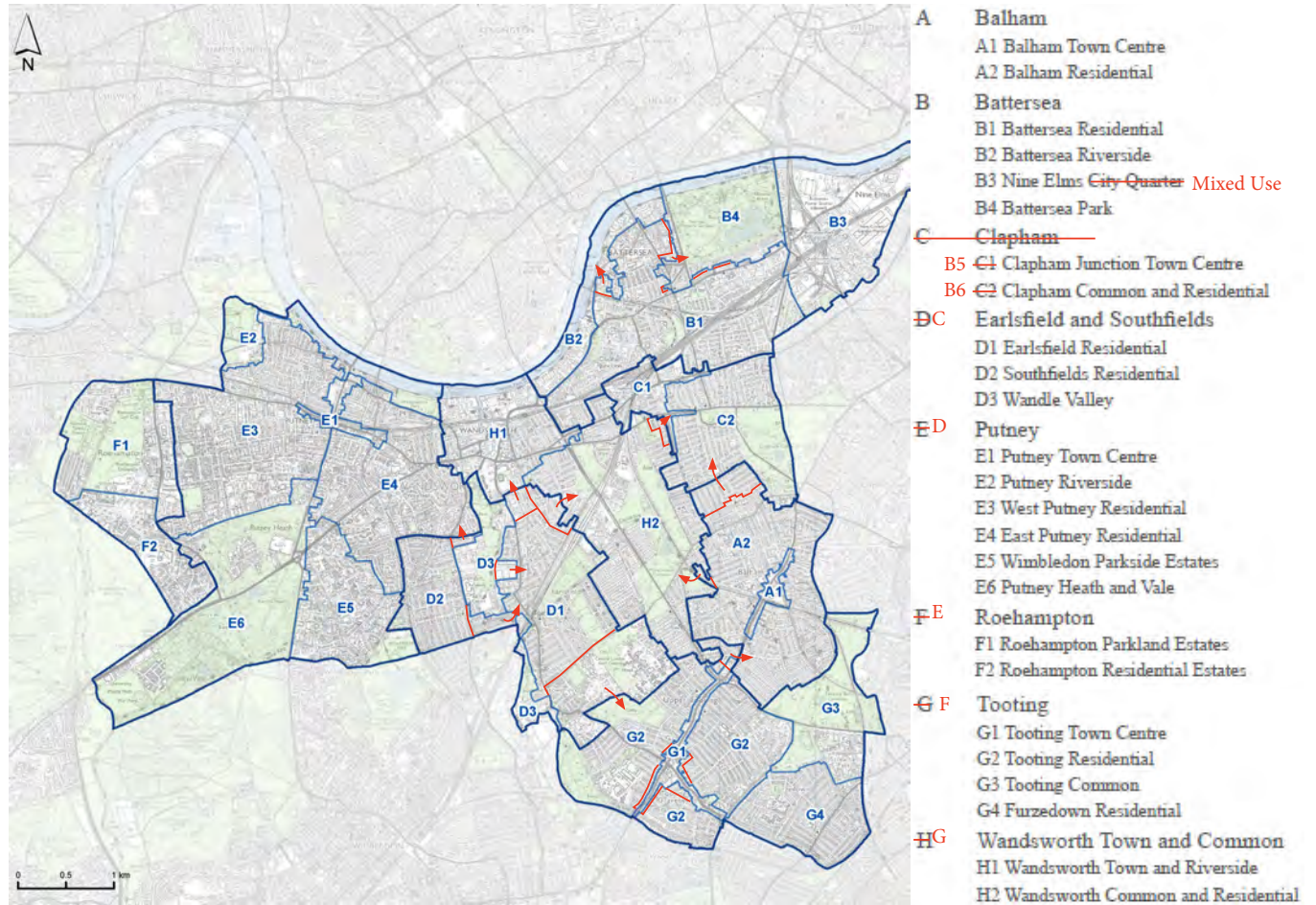
For Wandsworth Town and Riverside, the most important priority regarded by respondents was better public open spaces. Following this, better sense of community and cleanliness were considered of equal importance for the future; and reduced traffic and improved cycle lanes of equal but slightly lesser importance than the preceding priorities. For this area, all priorities were regarded as generally consistently important.



For Wandsworth Common and Residential, there was no consistency within consideration of importance of improvements. For this area, respondents considered reduced traffic was the most important priority, followed by improved cycle lanes; safer streets; better cleanliness and maintenance; and better public open spaces. Better pavements and squares along with better provision of culture and destinations were considered the least important priorities by respondents.

### G.3 Changes to character areas

[Fig. 507](#) below summarises the changes made to the character areas following the public consultation. Note that the 'Places' and character areas were renumbered after the consultation as shown.



**Fig. 507:** Overview of changes to character area boundaries and naming following public consultation

# Appendix H

## Density methodology

### H.1 Introduction

This Urban Design Study has informed how the Council has approached density calculations by providing a design-led approach. The density calculations have been used as part of the Council's methodology in identifying its estimate housing capacity for the Local Plan. This is set out in detail within the Council's HELAA update report 2022. In line with the GLA guidance [Good Quality Homes for all Londoners](#), the density calculations use the character assessment work and also the scenarios developed for tall building sites to provide a set of design-led assumptions.

The methodology for identifying and assessing sites is described in the [Wandsworth Housing and Economic Land Availability Assessment](#) (January 2021). This section provides an overview of the methodology used to establish approximate suitable density figures (dwellings per hectare (dph)) for each of the identified sites. It also describes how figures for ground floor floorspace and an estimate for capacity in terms of numbers of dwellings.

### H.2 Methodology

#### *Site status*

The Council have provided a list of sites for which to undertake the density estimates for. These sites fall into eight categories as follows:

- **Under construction:** The density for sites under construction has been derived from the actual developments being built and do not use numbers estimated by this Urban Design Study.
- **Planning;** The density for these sites has used the actual application if the application decision date is 3 years or less from 15 October 2021. Applications older than this are considered to not necessarily align with current approaches to design and height and therefore density estimates are derived from this Urban Design Study.
- **Subject to legal agreement:** The density for these sites has been derived from the actual applications

rather than those estimated by this Urban Design Study.

- **Pending or at appeal:** Density estimates derived from this Urban Design Study.
- **Identified sites:** Density estimates derived from this Urban Design Study.
- **Strategic Housing Land Availability Assessment (SHLAA) potential sites:** Density estimates derived from this Urban Design Study.
- **Site allocations:** Density estimates derived from this Urban Design Study unless there is a planning application on the site already.
- **SHLAA low potential sites:** Density estimates derived from this Urban Design Study.

Where scenarios were developed specifically for the Urban Design Study (presented in [Appendix A](#)) then the density figures were taken directly from these and do not follow the process described below. The scenarios were developed specifically with the character and sensitivity of the area in mind and were assessed accordingly.

#### *Establishing suitable building typologies*

The sites taken forward for assessment (highlighted in green in the list above) were assessed to establish which Urban Design Study character area they were within. Each character area has a series of building typologies typical of the area with reference to the GLA guidance [Good Quality Homes for all Londoners](#), as follows:

- Type B: House
- Type C: Cluster
- Type D: Terrace
- Type E: Linear blocks
- Type F: Villa block
- Type G: Tower

The site locations have also been analysed to understand whether they sit within a mid-rise or tall building zone as defined by this study. This established the following principles:

- All sites within a mid-rise building zone had building

types E and F applied.

- All sites within a tall building zone with a maximum height of 9 storeys also had building types E and F applied.
- All sites within a tall building zone with a maximum height of 10 storeys or more has building type G applied.
- For sites outside of mid-rise and tall building zones, the building types depended on the character of the area, the strategy for the area (Conserve, Restore, Improve, Transform) and also the PTAL rating. Higher PTAL ratings had building types D and E applied, while lower PTAL ratings also has building types B and C applied if relevant to the character of the area.

Density figures for each building type B to G, and for each PTAL rating (0-1, 2-3 and 4-6) were established with reference to density figures for existing developments, the density of scenarios prepared for this Urban Design Study and through analysis of neighbouring and similar authorities where relevant.

The development of the character areas, building typologies and the mid-rise & tall building zones took into account the sensitivity, probability of change and development capacity described in [Section 4](#) of this study. They also consider a series of different constraints including Local Plan designations, flood zones, open spaces, habitat designations, heritage designations and protected views. These were all re-reviewed for each site as part of the overall analysis and cross-checking of suitable density estimates.

### ***Establishing suitable building heights***

Where sites sit within a mid-rise building zone, the height for that zone (either 5 or 6 storeys) has been established as a suitable development height for the borough.

Where sites sit within a tall building zone (which have height ranges) then the height is determined by analysing the nearby maximum building heights (within a 100m diameter). The suitable height is established by taking the average of the tall building height zone

range unless this exceeds the maximum nearby building height - in which case this figure is used.

Where sites sit outside of either of these zones, then the building height depends on the sensitivity of the character area. Area with a low sensitivity area are calculated as being able to generally accommodate buildings up to 4 storeys (the maximum allowable outside of mid-rise and tall building zones). Where sensitivity is medium or high then the building height matches the average existing building height within a 100m diameter from the site.

The density figures and suitable building height figures are then used together to establish a suitable density in dwellings per hectare.

### ***Ground floor active uses***

A number of the sites fall within areas where ground floor commercial/retail uses would be expected and encouraged. The density estimate established above was further reviewed to remove the element of non-residential uses and establish a revised figure for suitable density in dwellings per hectare.

This process was completed for all sites located within either a Town Centre or Local Centre location.

The outcome of this process was an estimate of the capacity of each of the sites in terms of numbers of dwellings.

# Appendix I

## Views and vistas of interest

### I.1 Introduction

Views and vistas of interest were identified during site visits and borough data analysis.

These views and vistas are marked on maps in Section 3. The full list of views and vistas is presented in this appendix including locations on the map and photography from site visits or Street View imagery depending on available data.

## I.2 Views and vistas list

### I.2.1 A2 Balham Residential



Fig. 508: Alderbrook Rd looking south, Map



Fig. 509: Alderbrook Rd looking south, Photo

## I.2.2 B1 Battersea Residential



Fig.512: Wonders Rd looking north, Map



Fig.514: Wonders Rd looking north, Photo  
© Google



Fig.513: Queenstown Rd looking north east, Map



Fig.515: Queenstown Rd looking north east, Photo



Fig.510: Acanthus Rd looking north, Map



Fig.511: Acanthus Rd looking north, Photo  
© Google



### I.2.3 B2 Battersea Riverside

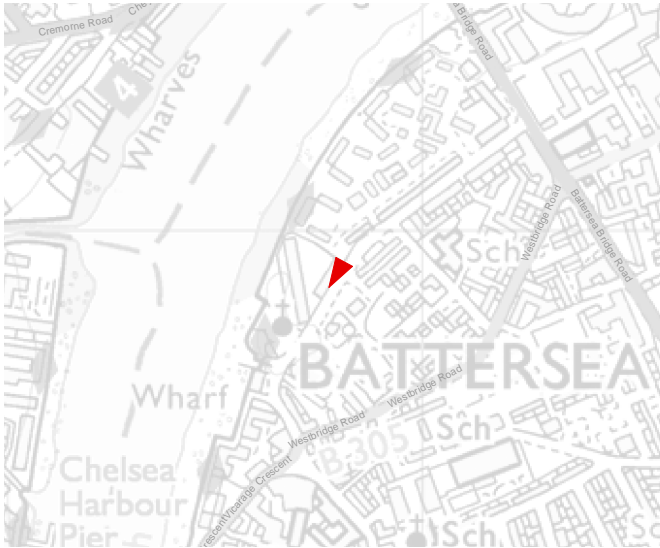


Fig.516: Battersea Church Rd looking south west, Map



Fig.519: Battersea Church Rd looking south west, Photo © Google



Fig.517: Montevetro Management Building public realm looking north west, Map



Fig.520: Montevetro Management Building public realm looking north west, Photo



Fig.518: Battersea Church Rd looking north, Map



Fig.521: Battersea Church Rd looking north, Photo © Google



Fig. 522: Vicarage Crescent looking north, Map



Fig. 524: Vicarage Crescent looking north, Photo  
© Google



Fig. 523: Vicarage Crescent looking south, Map



Fig. 525: Vicarage Crescent looking south, Photo  
© Google

## I.2.4 B3 Nine Elms Mixed Use



Fig. 526: View from railway bridge looking east Map

## I.2.5 B4 Battersea Park



Fig.527: Battersea Park Evolution London looking east, Map



Fig.530: Battersea Park Evolution London looking east, Photo  
© Google

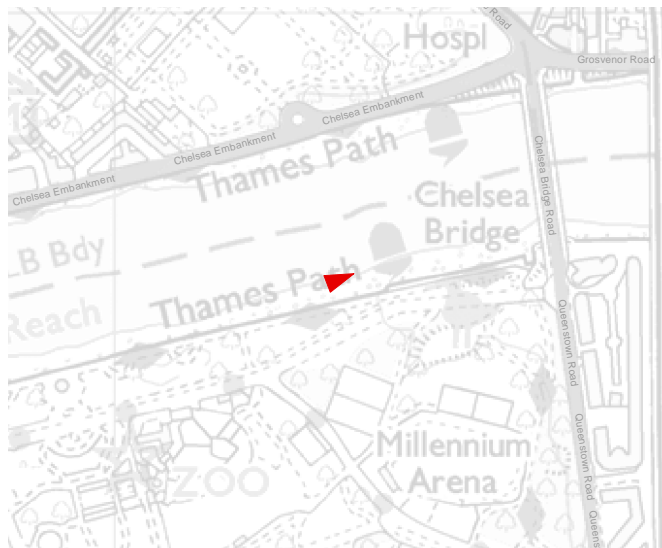


Fig.528: Carriage Drive North, looking east, Map



Fig.531: Carriage Drive North, looking east, Photo  
© Google

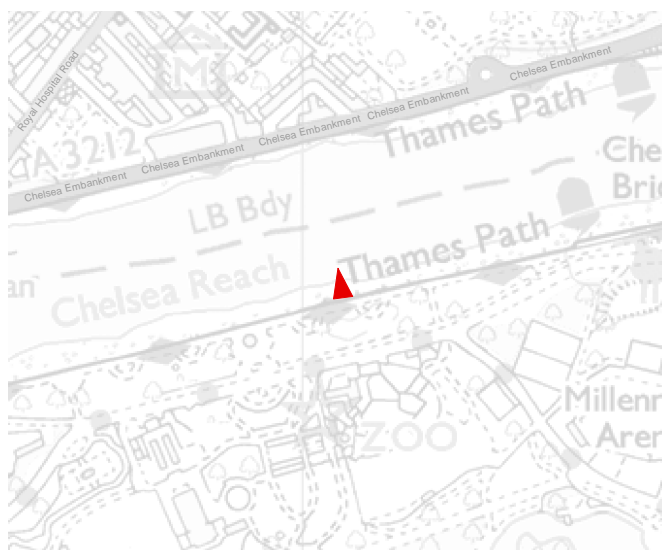


Fig.529: Carriage Drive North, looking north, Map



Fig.532: Carriage Drive North, looking north, Photo

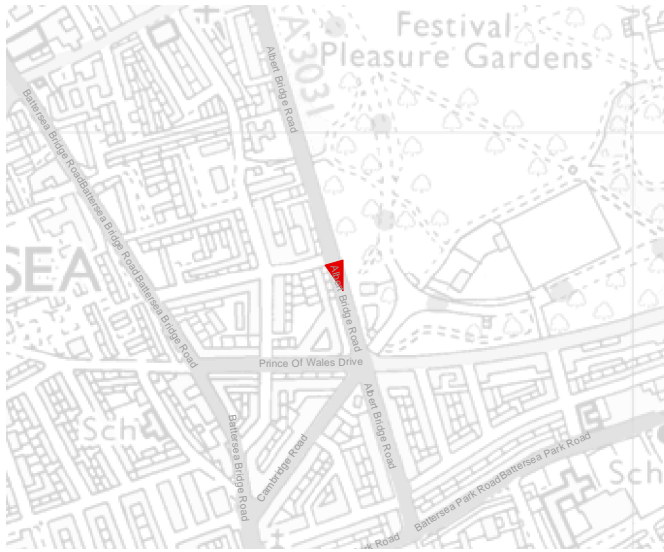


Fig. 533: Albert Bridge Rd looking south, Map



Fig. 534: Albert Bridge Rd looking south, Photo  
© Google

## I.2.6 B5 Clapham Junction Town Centre



Fig.535: Strathblaine Rd looking north east Map



Fig.538: Strathblaine Rd looking north east Photo  
© Google

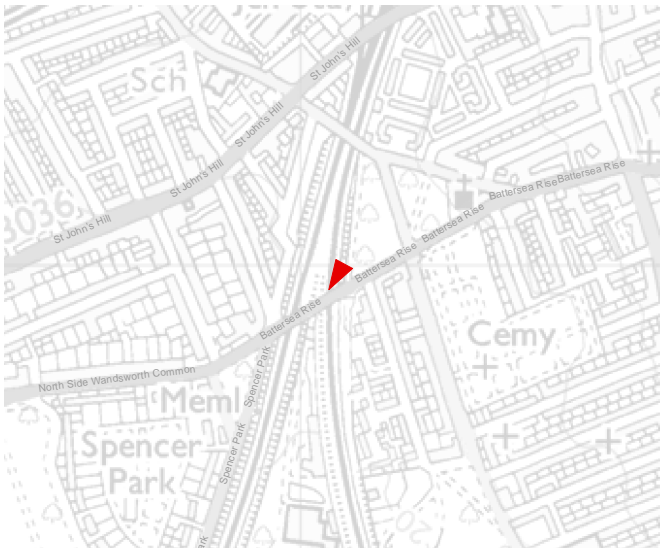


Fig.536: Battersea Rise/Chivalry Rd looking south west Map



Fig.539: Battersea Rise/Chivalry Rd looking south west Photo  
© Google

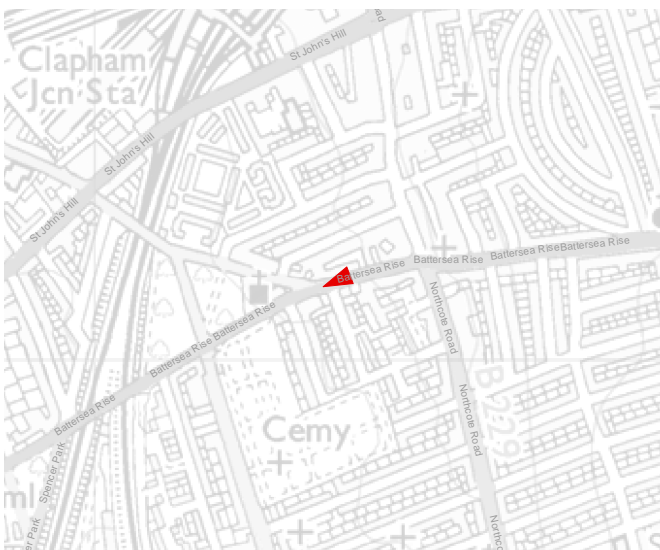


Fig.537: Battersea Rise Rd looking west Map



Fig.540: Battersea Rise Rd looking west Photo  
© Google

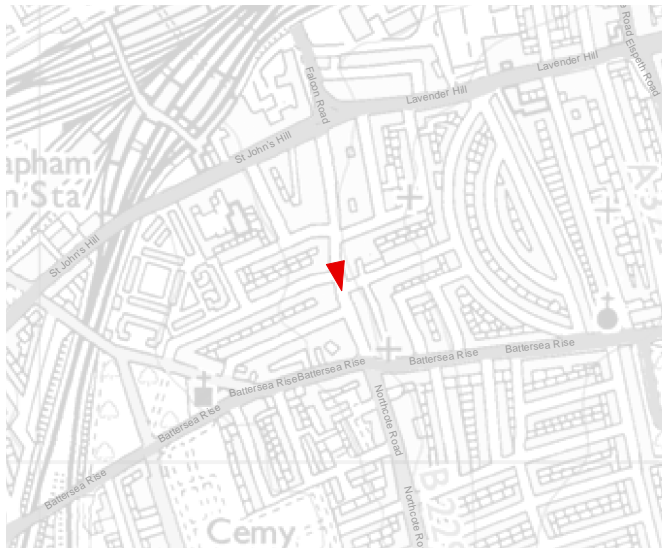


Fig.541: Falcon Rd looking south Map



Fig.544: Falcon Rd looking south Photo  
© Google



Fig.542: St John's Hill looking east Map



Fig.545: St John's Hill looking east Photo  
© Google

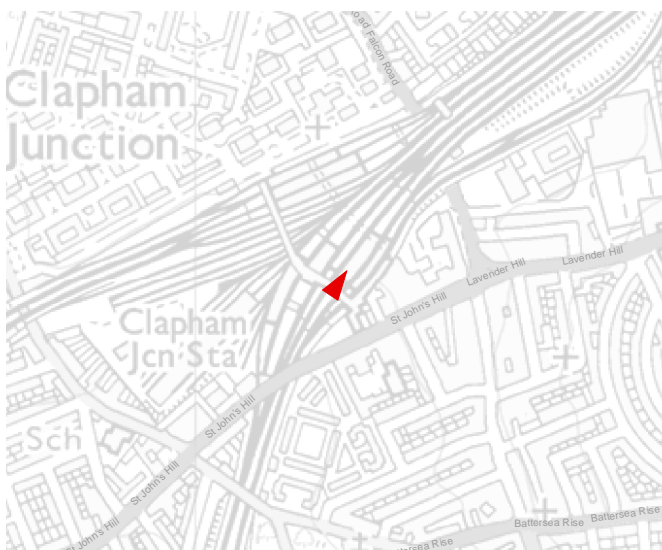


Fig.543: Clapham Junction looking north east Map

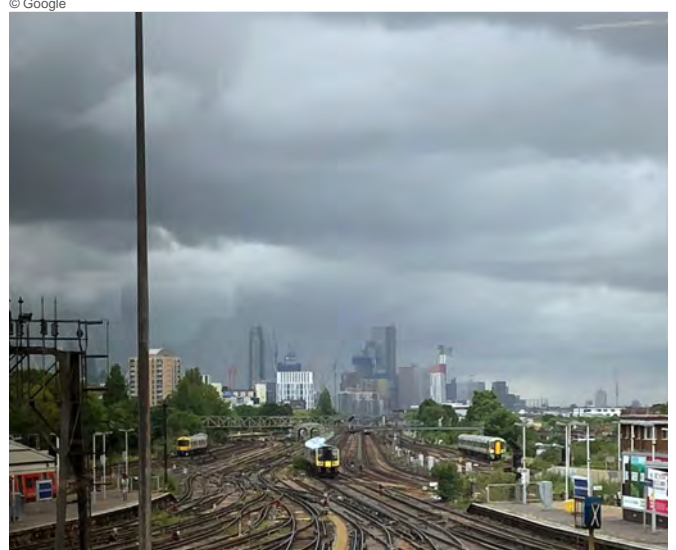


Fig.546: Clapham Junction looking north east Photo



Fig. 547: Railway view from the train looking south east Map



## I.2.7 B6 Clapham Common and Residential



Fig. 548: Broomwood Rd looking north east Map



Fig. 549: Broomwood Rd looking north east Photo  
© Google





Fig.556: Swaby Rd looking north west Map



Fig.559: Swaby Rd looking north west Photo  
© Google



Fig.557: Garratt Lane looking north Map



Fig.560: Garratt Lane looking north Photo  
© Google



Fig.558: Openview Rd looking north west Map



Fig.561: Openview Rd looking north west Photo  
© Google

## I.2.9 C2 Southfields Residential



Fig. 562: Replingham Rd looking west Map



Fig. 563: Replingham Rd looking west Photo  
© Google

## I.2.10 C3 Wandle Valley



Fig.564: Outside Nuffield Health looking south Map



Fig.567: Outside Nuffield Health looking south Photo  
© Google



Fig.565: Outside Nuffield Health looking north Map



Fig.568: Outside Nuffield Health looking north Photo



Fig.566: Kimber Rd looking south Map



Fig.569: Kimber Rd looking south Photo  
© Google



Fig.570: Kimber Rd looking north Map



Fig.572: Kimber Rd looking north Photo

© Google



Fig.571: King George's Park looking west Map



Fig.573: King George's Park looking west Photo

© Google



Fig.574: King George's Park looking south Map

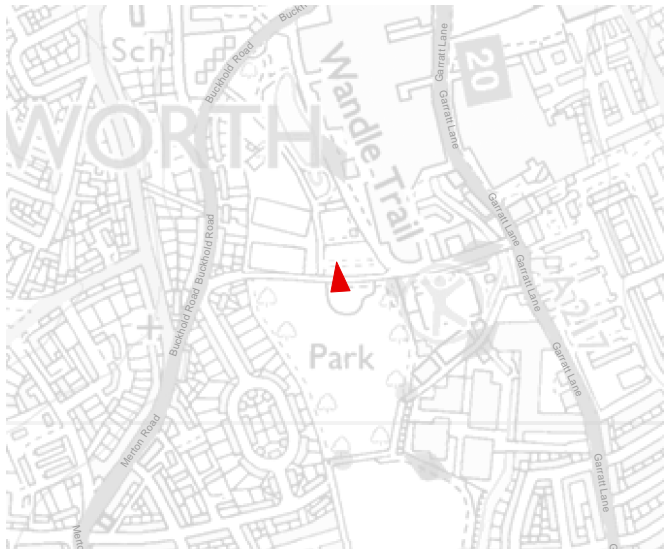


Fig. 575: Mapleton Road looking north Map

## I.2.11 D1 Putney Town Centre



Fig. 576: Putney High Street looking north Map



Fig. 577: Putney High Street looking north Photo  
© Google



## I.2.12 D2 Putney Riverside



Fig.578: Lower Richmond Road looking north Map



Fig.581: Lower Richmond Road looking north Photo



Fig.579: Putney Embankment CA looking south-east Map



Fig.582: Putney Embankment CA looking south-east Photo  
© Google



Fig.580: Putney Embankment Rowing Club looking south east Map

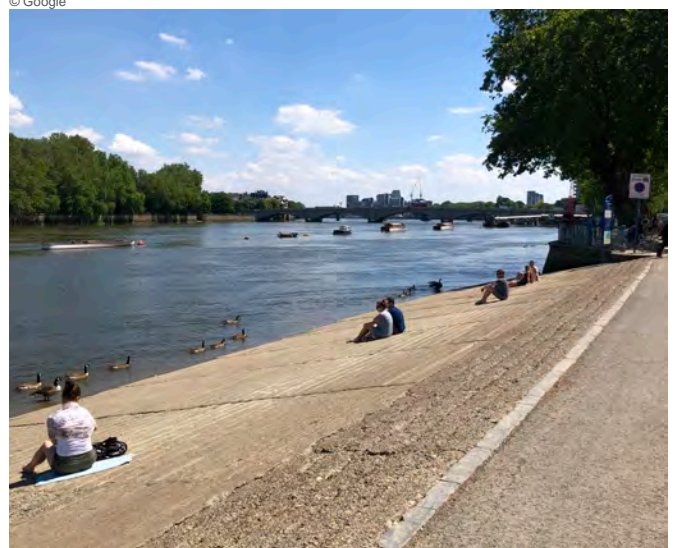


Fig.583: Putney Embankment Rowing Club looking south east Photo



Fig.584: Putney Emankment Winchester House looking east Map



Fig.585: Putney Emankment Winchester House looking east Photo  
© Google



Fig.586: Putney Emankment looking north Map



Fig.588: Putney Emankment looking north Photo

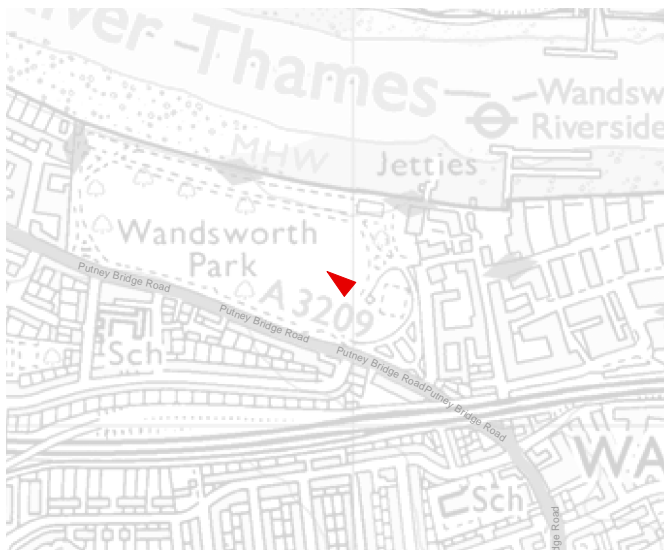


Fig.587: Wandsworth Park looking north east Map



Fig.589: Wandsworth Park looking north east Photo  
© Google

## I.2.13 D3 West Putney Residential



Fig.592: Weiss Road facing north Map



Fig.594: Weiss Road facing north Photo  
© Google



Fig.593: Thames Place facing north Map



Fig.595: Thames Place facing north Photo  
© Google



Fig.590: Egliston Road looking north Map



Fig.591: Egliston Road looking north Photo  
© Google



Fig.596: Dryburgh Road looking west Map



Fig.597: Dryburgh Road looking west Photo  
© Google



Fig.598: Hotham road looking east Map



Fig.599: Hotham road looking east Photo  
© Google



Fig.600: Charlwood Road looking north Map



Fig.601: Charlwood Road looking north Photo  
© Google



**Fig. 603:** Burston Road looking south Map



**Fig. 606:** Burston Road looking south Photo  
© Google



**Fig. 604:** Ravenna Road looking south Map



**Fig. 607:** Ravenna Road looking south Photo  
© Google



**Fig. 602:** St John's Avenue looking east Map



**Fig. 605:** St John's Avenue looking east Photo  
© Google



**Fig. 609:** Dover House road looking west Map



**Fig.611:** Dover House road looking west Photo  
© Google



**Fig. 608:** Coppice drive looking north Map



**Fig.610:** Coppice drive looking north Photo  
© Google

## I.2.14 D4 East Putney Residential



Fig.612: Keswick Road looking south Map



Fig.615: Keswick Road looking south Photo  
© Google

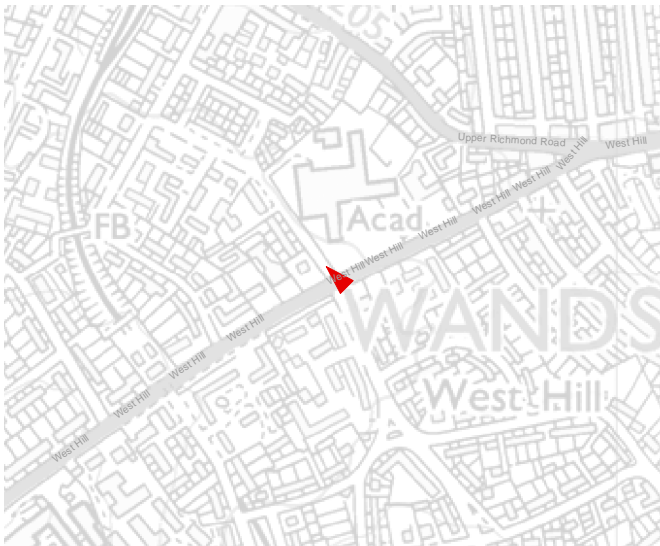


Fig.613: Portinscale Road looking north Map



Fig.616: Portinscale Road looking north Photo  
© Google



Fig.614: Upper Richmond Road looking west Map



Fig.617: Upper Richmond Road looking west Photo  
© Google



Fig.618: Upper Richmond Road looking east Map



Fig.621: Upper Richmond Road looking east Photo  
© Google



Fig.619: Melrose Road looking east Map



Fig.622: Melrose Road looking east Photo  
© Google

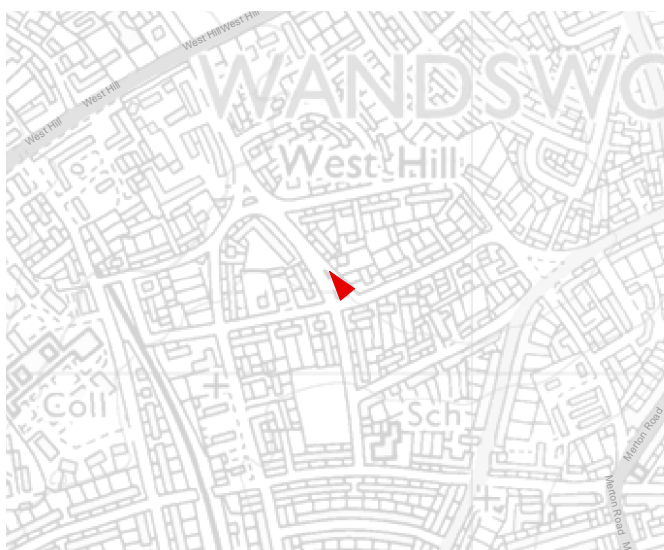


Fig.620: Viewfield Road looking north Map



Fig.623: Viewfield Road looking north Photo  
© Google





Fig. 624: Melrose Road looking west Map



Fig. 627: Melrose Road looking west Photo

© Google



Fig. 625: Holbush Road looking north east Map



Fig. 628: Holbush Road looking north east Photo

© Google



Fig. 626: Putney Heath lane looking east Map



Fig. 629: Putney Heath lane looking east Photo



Fig. 630: Putney Heath Lane looking west Map



Fig. 631: Putney Heath Lane looking west Photo

## I.2.15 D5 Wimbledon Parkside Estates



Fig. 632: Wimbledon Athletics Track looking south Map



Fig. 633: Wimbledon Athletics Track looking south Photo  
© Google

## I.2.16 D6 Putney Heath and Vale



Fig. 634: Putney Hill looking south east Map



Fig. 637: Putney Hill looking south east Photo  
© Google



Fig. 635: Roehampton Ln looking east Map



Fig. 638: Roehampton Ln looking east Photo  
© Google



Fig. 636: Roehampton Ln looking north Map



Fig. 639: Roehampton Ln looking north Photo  
© Google



Fig. 640: Alton Road looking north east Map



Fig. 643: Alton Road looking north east Photo

© Google



Fig. 641: Roehampton Ln towards Roehampton High St Map



Fig. 644: Roehampton Ln towards Roehampton High St Photo

© Google



Fig. 642: Rodway Road looking south Map



Fig. 645: Rodway Road looking south Photo

© Google



Fig. 646: Roehampton High St looking west Map



Fig. 647: Roehampton High St looking west Photo  
© Google

## I.2.17 F1 Tooting Town Centre



Fig. 648: Mitcham Road facing south east Map



Fig. 649: Mitcham Road facing south east Photo

## I.2.18 F2 Tooting Residential

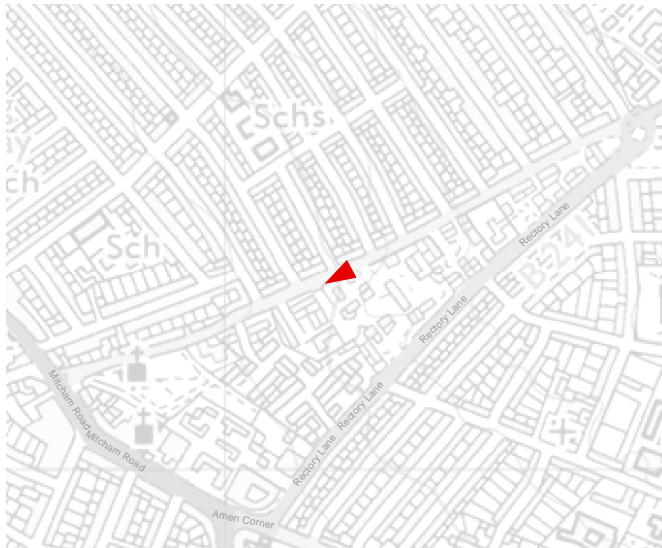


Fig. 650: Church Lane looking south west Map



Fig. 651: Church Lane looking south west Photo  
© Google

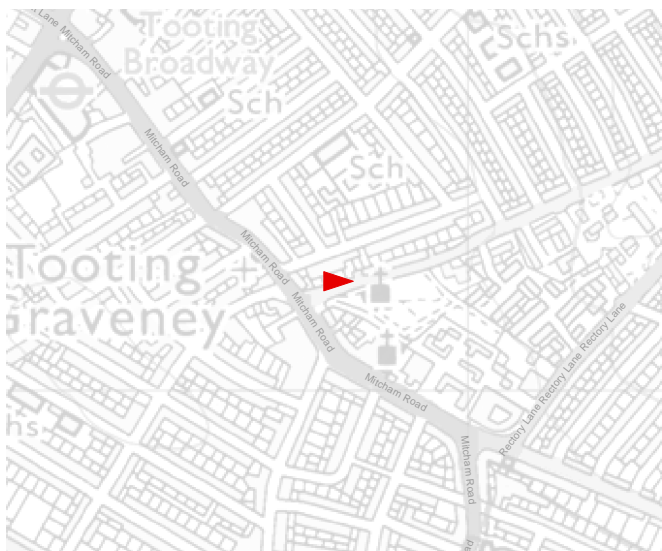


Fig. 652: Church Lane looking east Map



Fig. 654: Church Lane looking east Photo



Fig. 653: Fishponds Road looking south west Map



Fig. 655: Fishponds Road looking south west Photo  
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## I.2.19 F3 Tooting Common



Fig. 656: Tooting Bec Road looking south Map



Fig. 657: Aldrington Road looking north Map



Fig. 659: Aldrington Road looking north Photo

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Fig. 658: West Drive looking north Map



Fig. 660: West Drive looking north Photo

© Google



Fig.661: Bedford Hill in Tooting Common looking north west, Map



Fig.662: Bedford Hill in Tooting Common looking north west, Photo  
© Google

## I.2.20 F4 Furzedown Residential



Fig. 663: Furzedown Drive looking north Map



Fig. 664: Furzedown looking north Photo  
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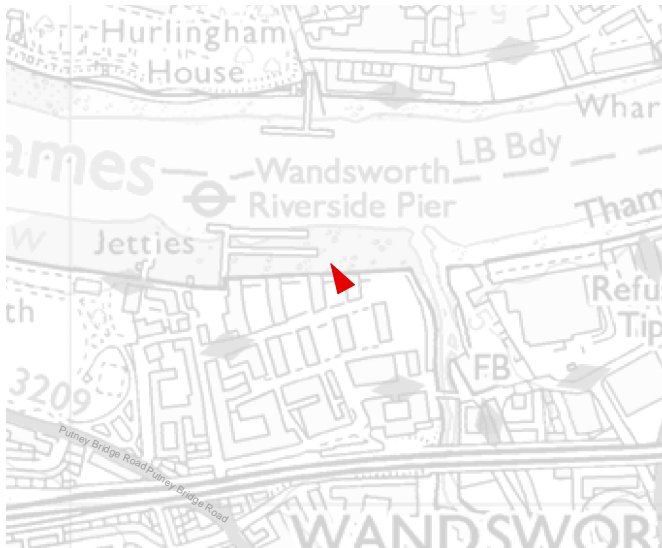
**I.2.21 G1 Wandsworth Town and Riverside**



**Fig. 665:** Thames - Bell Lane Creek looking west Map



**Fig. 667:** Thames - Bell Lane Creek looking west Photo



**Fig. 666:** Captain Apartments looking north Map



**Fig. 668:** Captain Apartments looking north Photo



**Fig. 669:** East Hill looking west Map



**Fig. 670:** East Hill looking west Photo

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Fig.671: Alma Road looking north Map



Fig.673: Alma Road looking north Photo



Fig.672: Tonsey Hill looking north Map



Fig.674: Tonsey Hill looking north Photo

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## I.2.22 G2 Wandsworth Common and Residential



Fig. 675: Spencer park looking east Map



Fig. 678: Spencer park looking east Photo  
© Google



Fig. 676: Earlsfield Road looking south Map



Fig. 679: Earlsfield Road looking south Photo  
© Google



Fig. 677: Trinity Road looking north east Map



Fig. 680: Trinity Road looking north east Photo  
© Google



Fig. 681: Bolingbroke Grove looking west Map



Fig. 684: Bolingbroke Grove looking west Photo  
© Google



Fig. 682: Trinity Road looking south east Map



Fig. 685: Trinity Road looking south east Photo  
© Google



Fig. 683: Belleview Road looking north Map



Fig. 686: Belleview Road looking north Photo  
© Google



**Fig. 687:** Burntwood Lane looking west Map



**Fig.688:** Burntwood Lane looking west Photo

© Google





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